

**Herb Durfee**

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**From:** Todd G. Thompson <Todd.G.Thompson@dartmouth.edu>  
**Sent:** Monday, January 25, 2021 6:21 PM  
**To:** cbrochu30@gmail.com; johnlanghus@gmail.com; marydlayton@gmail.com;  
 rogerarnoldvt@gmail.com  
**Cc:** Herb Durfee  
**Subject:** Beaver Meadow Sidewalk Study

Greetings All,

I wanted to let you know that the Thompson/Dustin Household (31 Dutton Hill Rd) are strong supporters of the extension to the existing Beaver Meadow Rd sidewalk.

I was impressed with Dubois & King’s work on the Study and agree that a sidewalk located on the western shoulder of the road is the most appropriate option. I was encouraged by the possibility of a State Grant and urge the Town to take the necessary steps to complete the application process. It seems that this sort of state aid might be available perennially and could be a source of assistance to the Town for other areas that would benefit from a sidewalk (ie. Church St, Elm St & Hopson Rd).

I think it’s useful to remember that a sidewalk project is especially well suited to a phased approach to the construction. It would be very reasonable from a construction logistics perspective to stretch this work over two or even three seasons and contracts. This would provide the benefit of: some immediate relief for the safety concern; a larger more diverse mix of sitework contractors; multiple bidding opportunities and a smaller budget line item. It even seems possible that the DPW would be interested in self-performing the first leg from the Condos to Bragg Hill?

Thanks for all the work you do for this Town!

My Best,  
Todd

Todd Thompson  
Project Manager  
Facilities, Operations & Management  
Dartmouth College  
6 Vox Lane  
Hanover, NH 03755  
O: 603 646-2842  
C: 603 359-0400

## Herb Durfee

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**From:** Stuart Richards <stuartrichards50@gmail.com>  
**Sent:** Saturday, January 30, 2021 3:24 PM  
**To:** norwich@lists.vitalcommunities.org  
**Cc:** Mary Layton; John Langhus; Claudette Brochu; Bob Gere; Roger Arnold; Herb Durfee; Miranda Bergmeier; Jennifer Frank  
**Subject:** CHIEF JENNIFER FRANK - A FOND FAREWELL

To the Norwich Selectboard:

As Chief Jennifer Frank departs for a new challenge in Windsor running a police force that is twice the size of the one she heads in Norwich I wanted her to know that she will be sorely missed and I wanted to extend a big thank you for all that she accomplished. During the two short years that she was Chief here in Norwich she showed us what true community policing was all about and what a difference it can make. At the same time that she leaves it is most unfortunate that our Norwich Selectboard with Selectman Roger Arnold in the lead has decided to defund the police budget by \$630.00. This small amount relative to the entire budget is nevertheless sending a most unfortunate message. Here's some of what's being cut:

- Operation Safety Cone – Kids who stop by the police office are given a certificate for a Dan & Whit's ice cream cone. Both my grandchildren make the police station a favorite stop on our frequent summer bike rides
- Halloween glo sticks – Over 400 handed out to keep kids safe over the trick or treat nights. Is making Halloween safer for kids a bad thing?
- Coffee with a cop – Being a part of the community and getting to know one another
- Touch a truck – an attempt to keep people safe on the road
- No new equipment or protective equipment
- Reduction in training time. Cops are being asked to do many things that they didn't have to do and that requires additional training.

Because I and Ned Redpath strongly disagree with defunding the police and the message that it sends we each are splitting the cost and sending our checks to restore the budget cut. By copy of this email I would appreciate it if you put this in Selectboard correspondence and let me know if Norwich will accept \$630 in order to specifically restore the budget cuts and the items listed above. How should the checks be made out? One other item. I understand that an anonymous donor has provided \$30,000 to equip the police with cameras. Is Norwich going to accept this donation and equip our police with cameras?

I look forward to a Selectboard response and to posting this in correspondence.

Stuart Richards  
Ned Redpath

Miranda Bergmeier

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**From:** charlotte metcalf <metcalfcharlotte738@gmail.com>  
**Sent:** Saturday, January 30, 2021 5:12 PM  
**To:** claudette brochu; Mary Layton; Roger Arnold; John Langhus; Douglas Wilberding; John Cushman; Chris Katucki; Linda Gray; Marcia Calloway; Pamela Thompson Smith; Herb Durfee; Miranda Bergmeier  
**Subject:** BURNED: Are Trees the New Coal? on Vimeo  
**Attachments:** WebPage.pdf; ATT00001.htm

This video was offered in our listserv this week.. It is heartbreaking to watch, but it is critical that we do before our NE forests are wiped out as have been those in the south and southeast. I have been following the discussion that has started on Article 10 of our town meeting and hope that all of you can see the similarity between what has happened here to what may happen to our forests if we allow wealthy out-of- towners, out of Staters, to buy patches of our forest to create solar energy. Do we really want to give them a tax break at the same time? I hope that as he steps down from the board, John Langhus will reconsider his determination to present this as an urgent item before we have had time to fully consider the consequences.

Please enter this appeal in the packet for the next board meeting.

Thank you.  
Charlotte Metcalf

I do not have Robert Gere’s email address and would appreciate your passing this to him and giving it to me for future reference.

<https://vimeo.com/286550378>

**Herb Durfee**

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**From:** Stuart Richards <srichards@globalrescue.com>  
**Sent:** Tuesday, February 02, 2021 12:19 PM  
**To:** norwich@lists.vitalcommunities.org  
**Cc:** Herb Durfee; Miranda Bergmeier; Mary Layton; John Langhus; Claudette Brochu; Bob Gere; Roger Arnold  
**Subject:** DEFUNDING/CUTTING THE POLICE BUDGET

Dear Readers

It is most unfortunate that there are those who want to ascribe a meaning other than what was intended to the words “defund the police.” What was intended is that “cutting” the police budget is hurtful. It’s hurtful not only with respect to the message it sends but also to the programs that might be cut as a result of the Selectboard’s ill advised cut in the budget of \$630. Quite a few individuals have stepped forward to say they will make up the cut in the police budget or add to the budget so that no programs will be cut if the Selectboard will allow the generously contributed money to be allowed. Norwich has been most fortunate to have had a wonderful police chief for two years, Jennifer Frank, who has provided terrific community policing in the best sense of the word. The programs below demonstrate how community policing should work. It is unknown why the Selectboard has chosen to cut any of the budget items below just as Chief Frank departs. Perhaps instead of a budget cut there should be a going away event as a huge thank you for all that she accomplished and let’s hope that whoever is the next police chief will continue or even enhance what she has done.

Community policing is one of the six identified topics of the President’s Task Force on 21st century policing and a primary element of the Norwich Police Department strategic plan. This year, the police department sponsored and organized a series of community policing activities, a few of which are highlighted below:  
**Bike Safety Day:** More than 75 youth participated in Bike Safety Day which included road safety instruction programs, agility courses, and bike helmet safety fitting sessions. Examples of expenditures: Traffic cones, helmets, bicycles, bicycle registration stickers, high-visibility arm bands and pull tags, water bottles  
**Coffee with a Cop:** More than 75 residents attended one of the four Coffee with A Cop events hosted this year by NPD. This program provided a casual atmosphere designed to encourage communication and positive interactions among law enforcement officers and the public. Community members are provided with a unique opportunity to sit down with officers and ask questions, share their local concerns, and have genuine discussions which lead to an increase in trust and communication.

**Pen-Pal program:** 32 local youth participated in the NPD Pen-Pal program in which students and officers connected through hand written letters to ease the burdens of social distancing during COVID-19 mitigation efforts. Students had the opportunity to practice their penmanship and writing skills while the schools were closed, building connections with officers along the way. Examples of expenditures: Envelopes, stamps  
**Multiple speaking engagements and training programs:** A few of the organizations with which NPD provided programs for include: Cub Scouts – fingerprinting lab, MCS 1<sup>st</sup> grade – Community helper program, Drivers Ed – Laws & Traffic stops, Health class – The impacts of impaired driving, Rec Programs – Babysitter training courses. Examples of expenditures: Fingerprint ink, print cards, impairment goggles, CPR disposables, babysitter tool kits

**Halloween Safety:** Distributed more than 400 glow sticks to kids to keep them safe

**Operation Safety Cone:** Issued certificates for kids to get ice cream cones at Dan & Whit’s

**Care of Stray Pets:** \$1,930 expended

**Touch a truck:** An attempt to keep people safe on the road

**Training time:** Cops are being asked to do many things that they didn't used to have to do and that requires additional training.

I hope that the Selectboard reverses the budget cut and instead sends Chief Jennifer Frank a big well deserved thank you. Her last day in Norwich will be February 28, 2021.

I would appreciate this email being made a part of Selectboard correspondence and would further appreciate a reply as to whether the Selectboard will restore the \$630 to the budget, whether donations will be accepted to supplement the police budget and also whether the \$30,000 will be put toward buying cameras for Norwich Police.

Thank you for your prompt attention,

Stuart Richards

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**Herb Durfee**

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**From:** Cheryl Lindberg  
**Sent:** Thursday, February 04, 2021 10:30 AM  
**To:** Herb Durfee  
**Cc:** Becky Grammer; Cynthia Stoddard; Seth Robertson; Miranda Bergmeier; Claudette Brochu; John Langhus; Mary Layton; rgere@mac.com; Roger Arnold  
**Subject:** RE: ~\$1.5M Need to Transfer

Selectboard,

This is a request to move the funds and close the account, which is an FDIC collateralized savings account.

Thank you,  
Cheryl A. Lindberg, Treasurer  
Town of Norwich

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**From:** Herb Durfee  
**Sent:** Thursday, February 04, 2021 10:14 AM  
**To:** Cheryl Lindberg  
**Cc:** Becky Grammer; Cynthia Stoddard; Seth Robertson; Miranda Bergmeier; Claudette Brochu; John Langhus; Mary Layton; rgere@mac.com; Roger Arnold  
**Subject:** ~\$1.5M Need to Transfer

Cheryl,

Having just learned and as I understand, there remains ~\$1.5M in the sweep account you created last Sept 2020 to help with collateralizing funds. Would you please request approval of the Selectboard to move that amount back into the Town's General Fund Account since it's tax season and school payments are forthcoming. Thx in advance.

P.S. I'll need that request before the close of business today, since the agenda/packet is being prepared and will go out the door tomorrow. I think you could simply "reply to all" on this e-mail making the request, and that would be sufficient information for the packet.

Claudette/Roger,

FYI, I'll be adding this item to the 2/10/21 Consent Agenda to keep it as simple as possible.

Herb  
Herbert A. Durfee, III  
Town Manager  
Town of Norwich  
PO Box 376  
Norwich, VT 05055  
802-649-1419 ext. 102  
802-698-3000 (cell)  
802-649-0123 (fax)

To: Norwich Selectboard  
From: John Langhus, Member

February 4, 2021

Re: Solar Energy in Norwich

Fellow Members,

The past two weeks has seen a flurry of statements in our meetings and on the Town Listserv about solar energy in Norwich. I work in solar energy. I promote solar energy at every opportunity. I hope that this can serve as a basis for detailed productive discussions about how we do and do not want to support solar energy here. Unfortunately, many of these statements have been very pointed, very personal in some respects, and misleading and misinformed in many respects.

What animates my thinking, whether as a father, or business leader, or elected official, is the simple fact that our entire society needs to set a path to completely change the way we derive and consume energy. We need to do that in about 10 years. We need to figure out how to fund, locate, permit and build over 60 TerraWatts of electricity (or its equivalent in other energy forms) across the world. That number is pretty close to inconceivable. 60 million Megawatts. And yet, that is the task before us globally. This will be a struggle at every level - to make it affordable, to make it equitable, to make it effective, to ensure that the cure is not worse than the disease (recognizing that at present the disease is tending toward terminal).

We (Americans and Northeasterners especially) have always preferred to get our energy from other places - coal from the deeply scarred plains of Wyoming and the desecrated mountaintops of Appalachia, oil and gas from the petroleum sacrifice zones of west Texas, Northern Alberta, the Orinoco, Niger and Mississippi deltas, and the Middle East. Environmental obscenities that can be seen from space! But now energy is becoming local, and we need to do some work to provide some of our own energy by ourselves. This will involve hard choices and new sacrifices that will need to be taken seriously. Building wind on the ridgelines, where it is effective, to some people mars the viewshed of a beautiful landscape (although some people view these turbines as beautiful symbols of hope and resilience). Solar and wind on prime agricultural land risks competing with local farms for quality land (although it also often gives farmland a needed rest from chemical fertilizers and overgrazing). And building in wooded areas involves cutting trees which provide beauty, habitat and would otherwise be helping with climate change by absorbing CO2 (although young trees are far less effective at this than older mature trees and there are many areas that are wooded with unwelcome invasive species that could be more surgically targeted). If this transition were easy, it would have been made 40 years ago when we first realized it must be done. Unfortunately, we are running out of excuses not to make it, and it is still going to be hard, harder when you consider how fast we must do it now. But I believe that the healing that will come from this change will vastly outweigh the costs. That we will find a path that treads lighter on the earth, that does not require sacrificing people or animals or habitat to succeed.

## **Article 11 (formerly Article 10)**

Much talk has centered around Article 11 on this year's Town Ballot (it was Article 10 in the draft we approved so it has been referred to in both ways). This Article reads as follows:

"Article 11: Shall the voters of the Town of Norwich authorize the Selectboard, pursuant to 24 VSA 2741, to enter into contracts, as application is made, for alternate energy generating plants: by fixing and maintaining the rate of tax applicable to such property, with the intent to ensure, among other things and as applicable, that the municipal tax treatment of off-site solar installations is equal to the municipal tax treatment of on-site solar installations?"

**What it does:** Provides authority for the SB to enter into tax stabilization agreements with some, all or none of the solar installations in Town not covered by the current tax exemption for onsite solar.

**What it doesn't:** Cut anyone's taxes. Exempt any solar from tax. Prefer one form of solar over another. Take any decision away from voters.

Article 11 doesn't define which solar projects should receive tax-stabilization contracts, the level of tax stabilization, or how long the contract should run. Those are the details the Selectboard will consider and settle, in a public process, *if the article passes*. A stabilization might reduce taxes paid, but it might instead just make them predictable for a project owner who wants certainty (i.e., you pay this year's rate this year, and 2% more each year going forward, regardless of how the Town's rate changes). That predictability is very helpful even if it is not a reduction. The Selectboard could choose different paths for different types of owners. The Norwich Energy Committee has historically supported the idea that offsite community solar, where the power is going to residential customers, municipal entities or nonprofits, should be tax-exempt just like residents are currently for onsite solar. Perhaps purely commercial solar, where the owner and the customer are both commercial parties, should be treated differently, although frankly I see no reason why.

The fact of the matter is that the federal government provides a big support to renewable energy investments. The State of Vermont provides another significant benefit. This is simply an opportunity for the Town to do its share and say that we also support this sort of investment in our Town. It is nothing more or less complicated than that. And I fully support it.

### **Solaflect Contract with the Town**

Questions have been raised for months about the Solaflect contract. These are not complicated matters. The contract is a fixed discount contract. It is a contract for dollars bought at 93 cents. The only way there could be a problem is if the Town was billed for net metering credits that it did not receive on its bills. That is a matter of comparing 12 bills per year from GMP with 12 bills per year from Solaflect, plus a 13<sup>th</sup> bill (or refund) provided each year for the purpose of reconciling at year end. That should take an hour per year to review and any employee of our Town should possess the skills necessary to do that comparison. This does not require a forensic audit from PriceWaterhouse. The Town needs to prioritize a full reporting of the following items:

1. What are the back charges Solaflect charged, when were they billed and paid and when should they have been billed? Some have suggested that these amounts maybe shouldn't have been paid if they were billed too late. Why we would choose not to pay a bill for a service we took, simply

because a clause in a contract may allow that, is beyond me. If we received the benefit of the net metering credits, I believe we should rightly have paid the bill. But we need to know the amount and when paid to understand the rest of these points.

2. How much has electricity demand increased since the net metering agreement was signed and especially in the past two fiscal years? I asked for an investigation of our electricity charges in three separate budget meetings and have received no answer. The increases in the Town's electricity charges in each of the last two years seem over and above what would have been driven by increased demand and GMP price increases. We need to have a reporting of that to understand where we sit and how we got here.

3. We need to vote on the proposed amendment to the Solaflect contract. It has been discussed in great detail and again is not complicated. The contract contained two mistakes. First, it stated that the solar array was to be built in Norwich. In the end it was built in Lunenburg, in part because of the difficulty of siting solar in Norwich at an affordable price. Second, it provided a purchase option to the Town in the event that we wanted to buy the solar field. So the decision is really whether either of these mistakes matter to the Town. I have posited that they do not. I personally do not care where our solar energy comes from. I care that it is solar energy and that we get the benefit of that. And I certainly see no benefit to taking on ownership of a solar power plant in a Town that seems to have its hands full with the existing responsibilities. No one in our Town government is qualified to maintain such a facility and it is just one more thing for the Town to keep track of. Like the deteriorating sidewalks, or the Tracy Hall in need of overhaul, or the 75% of Town energy use that has not yet been transformed from fossil fuel dependence. I would leave this one alone. I will not vote on this given my close friendship and business relationship with Rob Adams, Solaflect's COO.

4. Have you guys prepared for making any concessions for these changes? Some have pushed for that. I've argued that the mistakes have no value because we are not invested in the idea of the array being in Norwich and we would never want to buy the array. But others differ on this and with new members in march they may push you for something.

### **Route 5 Solar Project**

This is a private project, so I do not believe that it is an appropriate subject of Town business. However, since another elected town officer has chosen to publicly excoriate a private investor in this project, I will offer the following thoughts. The Fire District entered into a lease for this project. The suggestion is that the \$80,000 lease payment for an abandoned sand and gravel pit was too low. I would suggest raising that with the members of the Fire District, who presumably thought that it made sense. This is not prime agriculture. It is not forested woodland. It is an old sand and gravel pit. The type of site that the State of Vermont has specifically prioritized for solar development, so that a brownfield can be put to productive use without sacrificing more favorable types of land. This project benefits King Arthur Flour, helping them to be more sustainable and save money at the same time. It was developed by Norwich Technologies, another great local business that provides great local jobs, especially good-paying jobs for young Vermonters who all too frequently are choosing to start their careers in other States. Yes, I used to work for them. Yes, they are all my friends. Does this make them valid targets for insults and lies about their projects and their business? The vast majority of Norwich residents favor these sorts of projects. Like Article 36 two years ago they support aggressive action to reduce our use of fossil energy.

According to our own Tim Briglin, "Vermont has the highest level of greenhouse gas emissions per capita in the Northeast, and we're the only state whose emissions have increased in the last 30 years." Our 3300 citizens in Norwich will need between 10-20MW of solar-equivalent clean energy to do our part in this transition. With plenty of buffered space for screening and greenbelts, that could be accommodated with no more than 200 acres of land. Norwich is comprised of about 28,600 acres of land. So 1% of our land in Norwich. And as these and other solutions improve with technological improvements, this footprint will shrink over time.

\* \* \*

Thank you for your consideration of these thoughts.

John Langhus

cc: Herb Durfee  
Miranda Bergmeier

02/04/21  
02:31 pm

Town of Norwich Accounts Payable  
Check Warrant Report # 21-17 Current Prior Next FY Invoices For Fund (General)  
All Invoices For Check Acct 03(General) 02/10/21 To 02/10/21

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Vendor	Invoice Description	Invoice Date	Invoice Number	Account	Amount Paid	Check Number	Check Date
ATG	ADVANTAGE TRUCK GROUP	01/21/21	2019 Frtliner brake repai R701000222	01-5-703401.00 OUTSIDE REPAIRS	649.04	9854	02/10/21
ATG	ADVANTAGE TRUCK GROUP	01/25/21	2010 Frtliner engine repa R701000240	01-5-703401.00 OUTSIDE REPAIRS	250.18	9854	02/10/21
ATG	ADVANTAGE TRUCK GROUP	01/21/21	Plugs - restock X701001547	01-5-703403.00 PARTS & SUPPLIES	23.22	9854	02/10/21
ANYTIME	ANYTIME CARPET CARE & CLE	01/15/21	PSF - wkly clean Jan 6&15 969522	01-5-485304.00 CLEANING	369.47	9855	02/10/21
ANYTIME	ANYTIME CARPET CARE & CLE	01/28/21	Wkly cleaning Jan 21 & 28 969527	01-5-485304.00 CLEANING	360.00	9855	02/10/21
BCBS	BLUE CROSS/BLUE SHIELD OF	01/26/21	Feb medical ins bill 116918035	01-5-703123.00 HEALTH INSUR	7679.97	9856	02/10/21
BCBS	BLUE CROSS/BLUE SHIELD OF	01/26/21	Feb medical ins bill 116918035	01-5-704123.00 HEALTH INSURANCE	2228.24	9856	02/10/21
BCBS	BLUE CROSS/BLUE SHIELD OF	01/26/21	Feb medical ins bill 116918035	01-5-500123.00 HEALTH INS	6919.69	9856	02/10/21
BCBS	BLUE CROSS/BLUE SHIELD OF	01/26/21	Feb medical ins bill 116918035	01-5-555123.00 HEALTH INSURANCE	1852.18	9856	02/10/21
BCBS	BLUE CROSS/BLUE SHIELD OF	01/26/21	Feb medical ins bill 116918035	01-5-425123.00 HEALTH INS	2641.32	9856	02/10/21
BCBS	BLUE CROSS/BLUE SHIELD OF	01/26/21	Feb medical ins bill 116918035	01-5-005123.00 HEALTH INSUR	4476.00	9856	02/10/21
BCBS	BLUE CROSS/BLUE SHIELD OF	01/26/21	Feb medical ins bill 116918035	01-5-350123.00 HEALTH INS	1592.88	9856	02/10/21
BCBS	BLUE CROSS/BLUE SHIELD OF	01/26/21	Feb medical ins bill 116918035	01-5-100123.00 HEALTH INS	2795.40	9856	02/10/21
CASELLA	CASELLA WASTE SERVICES	01/01/21	TS - trash 61481	01-5-705306.00 C & D WASTE DISPOSAL	722.52	-----	---/---/---
CASELLA	CASELLA WASTE SERVICES	01/01/21	TS - trash 61481	01-5-705303.00 MUNICIPAL SOLID WASTE	4346.19	-----	---/---/---
CASELLA	CASELLA WASTE SERVICES	01/01/21	TS - recycle/compost 614817	01-5-705305.00 RECYCLING	3602.43	-----	---/---/---
CASELLA	CASELLA WASTE SERVICES	01/01/21	TS - recycle/compost 614817	01-5-705308.00 FOOD WASTE DISPOSAL	1606.43	-----	---/---/---
EYEMED	COMBINED INSURANCE CO OF	01/22/21	Vision ins for Feb 2021 164657994	01-2-001126.00 VISION SERV PLAN-PAYROLL	161.06	9857	02/10/21
COMCAST	COMCAST	01/20/21	26 New Boston - phone JAN-FEB 2021	01-5-703505.00 TELEPHONE	204.25	9858	02/10/21
CLAY	DANIEL S CLAY	01/27/21	Plowing & sanding 6414	01-5-703301.00 PLOWING & SANDING	6457.00	9859	02/10/21
DEADRIVER	DEAD RIVER COMPANY	01/31/21	TH - htg oil 333.1/gal 82693	01-5-706103.00 HEATING	679.15	9860	02/10/21
EVANSMOTO	EVANS GROUP, INC.	01/13/21	DPW Diesel 201/gal 14664	01-5-703405.00 PETROLEUM PRODUCTS	408.43	-----	---/---/---
EVANSMOTO	EVANS GROUP, INC.	01/20/21	Diesel 602/gal 14845	01-5-703405.00 PETROLEUM PRODUCTS	1222.30	-----	---/---/---
EVANSMOTO	EVANS GROUP, INC.	01/27/21	Diesel 300/gal 15040	01-5-703405.00 PETROLEUM PRODUCTS	612.60	-----	---/---/---
FERGUSON	FERGUSON WATERWORKS	01/21/21	DPW - deicer tank repair 1002464	01-5-703511.00 REPAIRS & MAINTENANCE	20.14	-----	---/---/---

Vendor	Invoice Date	Invoice Description Invoice Number	Account	Amount Paid	Check Number	Check Date
GMPC	01/22/21	GREEN MOUNTAIN POWER CORP Main St Tower JAN 2	01-5-575233.00 TOWER POWER	50.51	9861	02/10/21
GMPC	01/22/21	GREEN MOUNTAIN POWER CORP Beaver Mdw speed sign JAN 3	01-5-500204.00 SPEED SIGNS	17.01	9861	02/10/21
GMPC	01/22/21	GREEN MOUNTAIN POWER CORP 24 New Boston JAN 4	01-5-705501.00 ELECTRICITY	61.81	9861	02/10/21
GMPC	01/22/21	GREEN MOUNTAIN POWER CORP 300 Main St Bandstand JAN 5	01-5-706115.00 BNDSTND/SIGN/EVCH ELECTRI	12.58	9861	02/10/21
GMPC	01/22/21	GREEN MOUNTAIN POWER CORP Rt 10A speed sign JAN 6	01-5-500204.00 SPEED SIGNS	9.14	9861	02/10/21
GMPC	01/28/21	GREEN MOUNTAIN POWER CORP Street Lights JAN 7	01-5-703307.00 STREETLIGHTS	1033.72	9861	02/10/21
GMPC	01/21/21	GREEN MOUNTAIN POWER CORP Main St P7 Sign JAN 8	01-5-706115.00 BNDSTND/SIGN/EVCH ELECTRI	27.84	9861	02/10/21
FAIRFIELD	12/29/20	HOWARD P FAIRFIELD, LLC DPW - 7350141	01-5-704403.00 PARTS & SUPPLIES	4473.84	9862	02/10/21
IRVINGOIL	01/20/21	IRVING ENERGY DISTRIB. & Propane 377.2/gla 432945	01 5 703503.00 PROPANE	539.40	-----	--/--/--
JORDAN	01/11/21	JORDAN EQUIPMENT CO. Loader bucket P51219	01-5-703403.00 PARTS & SUPPLIES	875.00	9863	02/10/21
LEAF	01/31/21	LEAF CAPITAL FUNDING, LLC PD copier 11521660	01-5-500501.00 ADMINISTRATION	82.00	-----	--/--/--
JUDD	01/11/21	LISA JUDD parcel 05-096-000 overpmy TAX OVERPMY	01-2-001149.00 TAX CLEARING ACCOUNT	618.10	9864	02/10/21
PAPAZIAN	11/25/20	LYSSA PAPAZIAN CLG grant - Norwich Barn 11/25/20 #1	01-5-350416.00 HIST PRES GRANT	5054.13	9865	02/10/21
MAYER	01/31/21	MAYER & MAYER Jan payroll deduction JAN 2021	01-2-001120.00 EMPLOYEE JUDGEMENT ORDER	50.00	9866	02/10/21
NEMRC	01/27/21	NEW ENGLAND MUNI RESOURCE Finance assistance 47321	01-5-200112.10 FINANCE OFFICER WAGE	797.50	9867	02/10/21
PBA	01/31/21	NEW ENGLAND PBA, INC Union dues for Jan 2021 JAN 2021	01-2-001117.00 UNION DUES PAYABLE	460.00	-----	--/--/--
NORFARM	12/16/20	NORWICH FARM CREAMERY 1/2 gallons of milk 2217	01-5-425211.00 EQUIPMENT	16.50	9868	02/10/21
NORFIREDI	01/15/21	NORWICH FIRE DISTRICT 111 Turnpike Rd OCT-DEC 4THQ	01-5-425332.00 WATER USAGE	136.10	9869	02/10/21
NORFIREDI	01/15/21	NORWICH FIRE DISTRICT PSF - water bill OCT-DEC PSF	01-5-485232.00 WATER USAGE	203.60	9869	02/10/21
NORFIREDI	01/15/21	NORWICH FIRE DISTRICT TH water bill 4th qtr OCT-DEC TH	01-5-706100.00 WATER USAGE	146.10	9869	02/10/21
NORSCHOOL	01/31/21	NORWICH SCHOOL DISTRICT FY 20-21 #5 tax pmy TAX PMY #5	01-2-001123.00 SCHOOL DISTRICT TAX	1000000.00	9870	02/10/21
ROYCO	01/19/21	ROYCO DISTRIBUTORS INC DPW - Oil supply 152577	01-5-703405.00 PETROLEUM PRODUCTS	817.50	9871	02/10/21
SAFETYKLE	01/08/21	SAFETY-KLEEN SYSTEMS, INC DPW restock 85124444	01-5-703403.00 PARTS & SUPPLIES	818.00	-----	--/--/--
STANTEC	10/16/20	STANTEC CONSULTING SERVIC Rt 132 Cul/VT Trans grant 1714467	01-5-703715.00 VTRANS STRUCTURES GRNT	1809.39	9872	02/10/21
STANTEC	12/23/20	STANTEC CONSULTING SERVIC Rt 132 Culv/VT trans gran 1739840	01-5-703715.00 VTRANS STRUCTURES GRNT	421.09	9872	02/10/21

Vendor	Invoice Date	Invoice Description Invoice Number	Account	Amount Paid	Check Number	Check Date
TILDEN	01/07/21	TILDEN ELECTRIC, PC DPW/TS light repair 23914	01-5-703511.00 REPAIRS & MAINTENANCE	371.45	9873	02/10/21
TILDEN	01/07/21	TILDEN ELECTRIC, PC DPW/TS light repair 23914	01-5-705411.00 REPAIRS & MAINTENANCE	371.45	9873	02/10/21
TMDE	01/12/21	TMDE CALIBRATION LABS, IN PD EQ2 antenna repair 40772	01-5-500306.00 CRUISER MAINT	240.00	9874	02/10/21
HARTFORD	09/30/20	TOWN OF HARTFORD PD Vibrs Sept 2020 bill 11197	01-5-500535.00 VIBRS	115.42	-----	--/--/--
HARTFORD	12/31/20	TOWN OF HARTFORD PD - Vibrs Dec 2020 bill 11359	01-5-500535.00 VIBRS	116.19	-----	--/--/--
UNIFIRST	01/25/21	UNIFIRST CORPORATION B & G uniform 1070000120	01-5-704311.00 UNIFORMS	76.78	-----	--/--/--
UNIFIRST	01/18/21	UNIFIRST CORPORATION DPW uniforms 35 4554092	01-5-703507.00 SUPPLIES	81.57	-----	--/--/--
UNIFIRST	01/18/21	UNIFIRST CORPORATION DPW uniforms 35 4554092	01-5-703311.00 UNIFORMS	212.55	-----	--/--/--
UNIFIRST	01/18/21	UNIFIRST CORPORATION B & G uniforms 35 4554378	01-5-704311.00 UNIFORMS	76.78	-----	--/--/--
VALLEYNEW	01/16/21	VALLEY NEWS TC - notice to voters 401133	01-5-050540.00 ADVERTISING	193.80	9875	02/10/21
VMERS	01/13/21	VMERS DB Payroll Transfer PR-01/13/21	01-2-001111.00 VMERS GRP B PAYABLE	5013.48	9876	02/10/21
VMERS	01/13/21	VMERS DB Payroll Transfer PR-01/13/21	01-2-001113.00 VEMRS GRP C PAYABLE	2065.42	9876	02/10/21
VMERS	01/27/21	VMERS DB Payroll Transfer PR-01/27/21	01-2-001113.00 VEMRS GRP C PAYABLE	2036.79	9876	02/10/21
VMERS	01/27/21	VMERS DB Payroll Transfer PR-01/27/21	01-2-001111.00 VMERS GRP B PAYABLE	5935.93	9876	02/10/21
VTGFOA	01/24/21	VT GOVERNMENT FINANCE OFF VTGFOA 2021 workshop 2021 WK SHOP	01-5-005615.00 DUES/MTS/EDUC	20.00	9877	02/10/21
WBMASON	09/23/20	W.B. MASON CO., INC. DPW trash bags 214005226	01-5-703507.00 SUPPLIES	146.99	-----	--/--/--
WBMASON	09/24/20	W.B. MASON CO., INC. PD - DVD's for cruisers 214064262	01-5-500304.00 CRUISER VIDEO EQUIP	23.91	-----	--/--/--
WBMASON	09/24/20	W.B. MASON CO., INC. DPW - cybertrack 214064482-2	01-5-703513.00 TOOLS	69.99	-----	--/--/--
WBMASON	10/05/20	W.B. MASON CO., INC. PD office supplies- stamp 214361419	01-5-500501.00 ADMINISTRATION	42.99	-----	--/--/--
WBMASON	11/05/20	W.B. MASON CO., INC. DPW - time cards 215306751	01-5-703507.00 SUPPLIES	34.99	-----	--/--/--
WBMASON	11/20/20	W.B. MASON CO., INC. DPW ofc supply 215738013	01-5-703507.00 SUPPLIES	19.99	-----	--/--/--
WBMASON	12/14/20	W.B. MASON CO., INC. DPW - back up drive 216332231	01-5-703507.00 SUPPLIES	53.99	-----	--/--/--
WBMASON	12/14/20	W.B. MASON CO., INC. Rec - supplies 216339508	01-5-485301.00 BUILDING SUPPLIES	104.05	-----	--/--/--
WBMASON	12/22/20	W.B. MASON CO., INC. Plan - toner 216559001	01-5-350610.00 OFFICE SUPPLIES	160.99	-----	--/--/--
WBMASON	01/18/21	W.B. MASON CO., INC. DPW - ofc supplies 217182106	01-5-703507.00 SUPPLIES	55.52	-----	--/--/--

All Invoices For Check Acct 03(General) 02/10/21 To 02/10/21

Vendor	Invoice Date	Invoice Description Invoice Number	Account	Amount Paid	Check Number	Check Date
WBMASON	01/12/21	DPW - phone return CR8662872	01-5-703507.00 SUPPLIES	-173.99	-----	--/--/--
WBMASON	01/26/21	Plan - toner credit CR8704702	01-5-350610.00 OFFICE SUPPLIES	-160.99	-----	--/--/--
WORKSAFE	01/25/21	Rd closed signs 24823	01-5-703217.00 SIGNS	232.17	9878	02/10/21
Report Total				1087949.16	=====	=====

To the Treasurer of Town of Norwich, We hereby certify that there is due to the several persons whose names are listed hereon the sum against each name and that there are good and sufficient vouchers supporting the payments aggregating \$ \*1,087,949.16  
Let this be your order for the payments of these amounts.

FINANCE DIRECTOR Becky Grammer TOWN MANAGER: Herbert Durfee  
Becky Grammer Herbert Durfee, Town Manager

SELECTBOARD:

Claudette Brochu  
Chair

Roger Arnold  
Vice Chair

Robert Gere

John Langhus

Mary Layton

(3)

DRAFT Minutes of the Selectboard Meeting of  
Wednesday, January 27, 2021 at 6:30 pm

This meeting was conducted via teleconference using ZOOM, in order to maintain appropriate physical distance under COVID-19 precautions. Members present: Claudette Brochu, Chair; Roger Arnold, Vice Chair; Robert Gere; John Langhus; Mary Layton; Herb Durfee, Town Manager; Miranda Bergmeier, Assistant to the Town Manager. For the purposes of this meeting, Arnold acted as Chair.

There were about 24 people in the audience.

Also participating: Cheryl Lindberg, Stuart Richards, Ned Redpath, Demo Sofronas, Dayton Crites, Louise Hamlin, Peter Orner, Hilary Lynch, Pam Smith, Aaron Lamperti, Police Chief Jennifer Frank, Deputy Fire Chief Matt Swett, Linda Cook, Jack Cushman.

1. Approval of Agenda. Selectboard (SB) members agreed to delete item #11 (Climate Emergency Designated Fund) from the agenda.
2. Public Comment. Stuart Richards said he doesn't understand why the SB chose to appoint Finance Committee members if the SB wasn't going to consider their input. Richards also said he thinks the SB is shutting down public participation via their interpretation of Vermont's Open Meeting Law (OML). Ned Redpath offered comment on his letter protesting the SB's reduction of spending on police services. Stuart Richards spoke again, saying that he supports Redpath's comments. He doesn't want to cut spending on police services, especially community policing. Brochu said the budget has passed the SB and now it's up to the voters. She thinks the Finance Committee issue will be discussed at a future meeting. Demo Sofronas also said he thinks the SB should not have cut the Police Department budget, and he had said so at the SB's last budget meeting.
3. Consent Agenda. Layton moved (2<sup>nd</sup> Langhus) to approve the consent agenda, including the A/P warrants, as contained in the packet, and to include the proposed change to the minutes on page 77 of the packet. **Motion approved unanimously.**
4. Beaver Meadow Road Sidewalk Study Report. Dayton Crites, of Dubois & King, gave a synopsis of the Beaver Meadow Scoping Study, using the presentation as included in the SB meeting packet. SB members discussed design options for the sidewalk. Demo Sofronas gave some historical background. Stuart Richards wondered about funding not coming from the town; what costs have been incurred so far; and whether alternative #1 is the only one being considered; and whether this project will go before the town for a vote. Arnold made note of the questions. Louise Hamlin said she appreciates the modified sidewalk design on the west side of the road. She wonders what opportunity costs would result from sidewalk construction. Peter Orner spoke in support of the sidewalk project and said he appreciates the work of the SB and the input from all the citizens. Orner said we shouldn't wait for something bad to happen before we address the need for pedestrian safety. Hilary Lynch said she supports the sidewalk. Pam Smith said she wonders if a traffic study is going to be conducted, especially regarding truck traffic. Stuart Richards wonders what the situation with existing sidewalks is. He said he'd like to see the existing sidewalk better taken care of. Aaron Lamperti said that a mobility lane is an excellent option and should be kept as an option for the plan. Gere said he thinks the town needs to move forward with the next phase. Langhus said he agrees, as did Layton and Brochu. Arnold agreed and said he'd like to consider the mobility lanes, if possible. Layton moved (2<sup>nd</sup> Langhus) to schedule public hearings on Feb. 10 and Feb. 24, 2021 to solicit public input on the draft report and to gauge public support for moving forward with an application for a 2021 VTrans Bike/Ped Grant for phase II of the 3-phase project. **Motion approved unanimously.**

5. Layton **moved** (2<sup>nd</sup> Gere) to approve the Certificate of Adoption and Resolution Adopting the Norwich, Vermont 2020 Local Hazard Mitigation Plan, as included in the Selectboard meeting packet. **Motion approved unanimously.**

6. Stop/Yield Ordinance. Police Chief Jennifer Frank said that the existing ordinance only has 13 signs and was drafted in 1978. The new ordinance is updated to include all existing signs in the town and doesn't add any signs that aren't already in place in town. Layton **moved** (2<sup>nd</sup> Langhus) to accept the first reading of the Town of Norwich Stop/Yield Sign Ordinance, as contained in the Selectboard meeting packet. **Motion approved unanimously.**

7. FEMA Grant Request. Deputy Fire Chief Matt Swett spoke in support of the Fire Department's request to apply for a grant to purchase equipment. Layton **moved** (2<sup>nd</sup> Langhus) to authorize the Town Manager to apply for funding through the 2020 Assistance to Firefighters Grant, with the understanding that the Town of Norwich will be responsible for payment of matching funds equal to 5% of the grant amount total. **Motion approved unanimously.**

8. December 2020 Financial Reports. Town Manager Herb Durfee spoke about the financial reports, as contained in the SB packet, highlighting a few line items.

9. Ford Sayre Request to Reduce Usage Fees for Huntley Meadow. Durfee said the Ford Sayre program seeks further reduction beyond their 50% discount off the non-profit usage fee. Because the fee structure was approved by the SB, it is up to the SB to decide on any further discount. Langhus said he thinks we should discount the rate because of the nature of the use. Brochu said she found Langhus's arguments persuasive and she thinks we should waive the fee altogether. Gere agrees we should waive the fee. Layton said she is not comfortable with the precedent being set. Langhus **moved** (2<sup>nd</sup> Brochu) to waive the usage fees for Huntley Meadow for the Ford Sayre program according to their request, as outlined in their letter dated January 11, 2021 and contained in the Selectboard meeting packet. **Motion approved (4 yes; Layton abstained).**

10. Proposed Task Force to Address Article 36. Layton said she is inclined to approach this as a first reading; also she wonders if the SB should be creating yet another task force. Langhus said Layton's points are well-made; Langhus said he's inclined to shrink the size of the task force down from 9. Langhus thinks the charge is a good start. Brochu questioned the proposed membership – the make-up of the group. Durfee suggested the SB consider asking the Energy Committee to form the task force. Langhus said he thinks the task force needs to be led by the SB. SB members discussed options for approaches to Article 36. Linda Gray encouraged the SB to do something like the proposed task force in a timely manner. Pam Smith said she thinks the SB should set up a capital budget committee, instead. Aaron Lamperti said he supports having the SB form the task force so they can gather information. Jack Cushman said he supports this effort. He wants the SB to include a broadening phrase to allow the task force some flexibility to explore related objective and approaches. Brochu **moved** (2<sup>nd</sup> Arnold) to accept the Article 36 Task Force draft as a first reading and revisit and possibly adopt at a future SB meeting. **Motion approved unanimously.**

11. Climate Emergency Designated Fund. SB members agreed to skip this item, per earlier discussion.

12. Listers RFP for Re-Appraisal Services. Cheryl Lindberg said the Listers want to keep everyone informed so they can get input and suggestions. Langhus suggested the Listers include a proposed contract along with the RFP. Arnold said he thought a 30-page RFP might be too detailed; most other RFPs he saw were 10-11 pages.

13. Town Meeting Presentation. Brochu said she wants the SB members to start thinking about what type of a presentation they want, and would the SB like to hold earlier meetings, prior to the March 1<sup>st</sup> informational meeting. SB members discussed options. SB members agreed that Langhus will deliver the presentation and Brochu and Arnold will draft the presentation to share with the SB for approval.

14. Adjournment. Layton **moved** (2<sup>nd</sup> Brochu) to adjourn. **Motion approved unanimously.**

Meeting adjourned at 9:19 pm.

By Miranda Bergmeier

Approved by the Selectboard on \_\_\_\_\_, 2021

\_\_\_\_\_  
Claudette Brochu  
Selectboard Chair

Next Meeting – February 10, 2021 – Meeting at 6:30

PLEASE NOTE THAT CATV POSTS RECORDINGS OF ALL REGULAR MEETINGS OF THE NORWICH SELECTBOARD.



# BEAVER MEADOW ROAD SIDEWALK SCOPING STUDY

Norwich, Vermont

DRAFT January 2021



DuBois  
& King  
INC.

## **Project Team**

### **Town of Norwich**

Herb Durfee, Town Manager

Larry Wiggins, Public Works Director

Nydia Lugo, VTrans

Rita Seto, TRORC

### **Dubois & King**

Dayton Crites

Chris Sargent

Julia Ursaki

### **Hartgen Archaeological Associates, Inc**

## **Project Funding**

This project is funded by the Vermont Agency of Transportation (VTrans) Bicycle and Pedestrian Program with support from the Two Rivers-Otauquechee Regional Commission (TRORC).

## **Special Thanks**

To the Norwich Community, for robust participation and valuable contributions to this study of a roadway in the heart of their town.

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Need	

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# Introduction

## Introduction

- Existing Conditions
- Purpose and Need
- Alternatives
- Implementation
- Appendices

The Town of Norwich (the Town) is a vibrant and engaged community in eastern Vermont bordering Hanover, New Hampshire. The Town has elected to investigate the feasibility and value of building a sidewalk along Beaver Meadow Road. Beaver Meadow Road connects the town's Main Street with a Church, numerous single family homes, the Norwich Public Pizza Oven, and Huntley Meadows park.

This Scoping Study investigates the concept of the sidewalk through community preferences, its feasibility, alternative sidewalk designs, and planning level cost estimates.

## Methodology

This study began in late summer of 2020, with project team members and representatives of the Town meeting to walk the Beaver Meadow Road corridor and review existing conditions and constraints.

Due to social distancing requirements and the ongoing COVID 19 pandemic, public engagement took place outside of a traditional public meeting format. On August 30th, a public website was launched as [www.tinyurl.com/beavermeadowsidewalk](http://www.tinyurl.com/beavermeadowsidewalk) That project website contained existing conditions information, a link to a public survey, and public meeting information throughout the project.

The first online survey gathered 422 unique responses, and the public video-conference was attended by 38 members of the public. This initial effort allowed the design team to generate multiple design alternatives that were reviewed via online survey and a second public meeting. The second public survey received 46 responses, and 25 members of the public attended the second public video-conference. A summary of these voices can be found in the public engagement chapter of this document.

Following these meetings, a project team meeting was held to review overall public input and select a preferred alternative. That alternative was further refined in both design and detailed planning level cost estimates.



Where the sidewalk ends. Beaver Meadow Road at Huntley Road - facing south.

## Project Area and Background

Beaver Meadow Road is part of a popular route in Norwich used by people driving, walking, bicycling, running, and more. This "loop" route connects Norwich's Village on Main Street, Beaver Meadow Road, Moore Lane, Turnpike Road, and then back down Main Street into the Village.

Along this route, Main Street, Turnpike Road, and the southern quarter mile of Beaver Meadow Road have sidewalks, while the northern half mile of Beaver Meadow Road and Moore Lane do not, leaving a gap in the sidewalk network.

This route provides access to several community destinations, including:

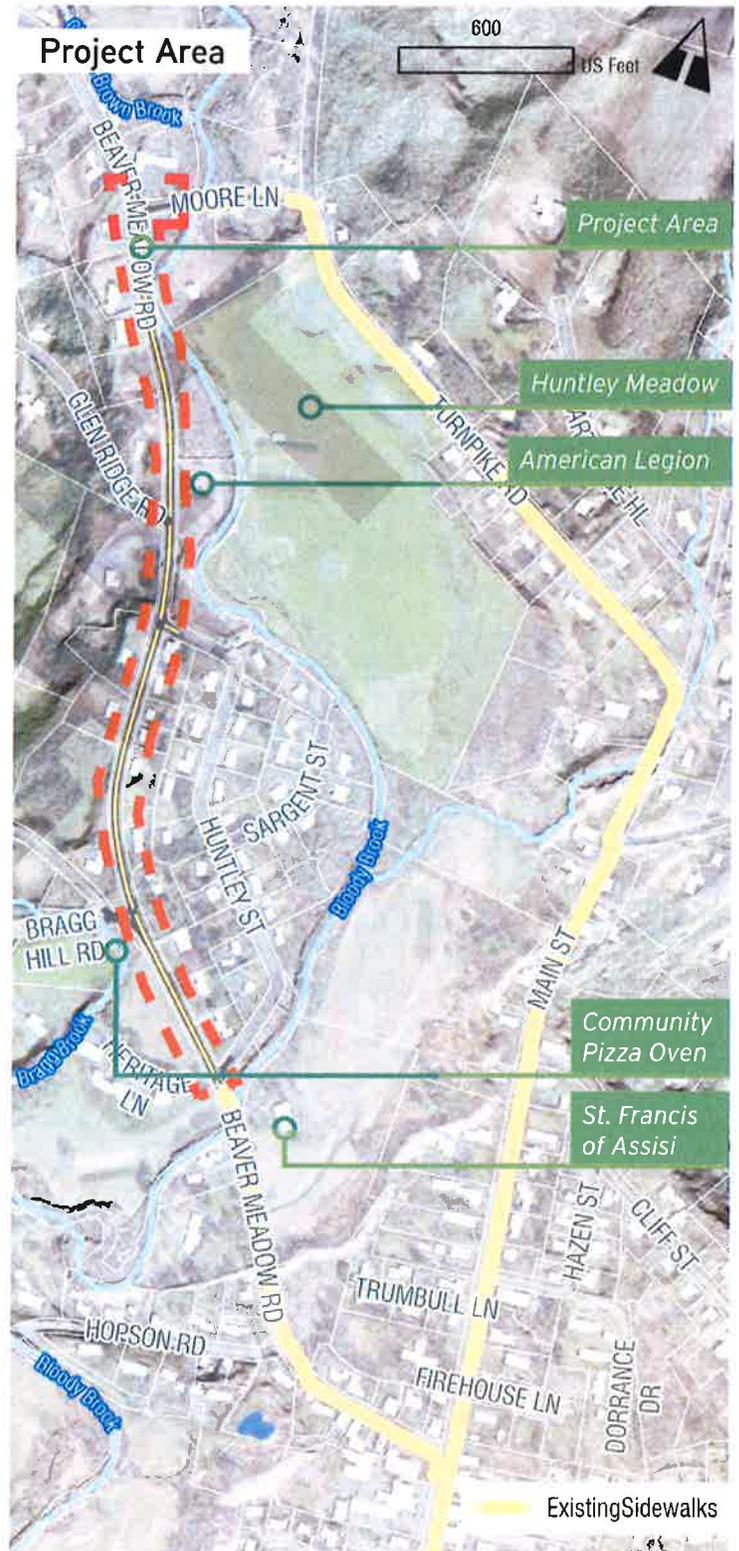
- Huntley Meadows community park
- Norwich Community Pizza Oven
- American Legion
- Saint Francis of Assisi Church
- Bill Ballard Trail
- Norwich's Village Center and elementary school

## Land Use Context

Land uses within the project area are primarily village-scale residential in nature. A few of these residential homes function as home offices. Exceptions to this pattern are the American Legion building, St. Francis of Assisi Church, and parks along this corridor.

To the eastern side of the corridor there is an approximately 25 home subdivision surrounding Huntley and Sargent streets. To the east, development is less dense with more open space.

The zoning district along Beaver Meadow Road in the study area is Village Residential I. This district provides a medium density residential neighborhood setting that is close to municipal services.



# Existing Conditions

- Introduction
- Existing Conditions
- Purpose and Need
- Alternatives
- Implementation
- Appendices

## Roadway Characteristics & Safety

### Traffic Volumes

Average annual daily traffic on Beaver Meadow Road was last reported in 2019 as approximately 1805.

### Speeds

The speed limit of Beaver Meadow Road is posted at 25 miles per hour. However, many members of the public indicated that they perceive traffic to be speeding faster through this road. Trucks driving on this road in particular have a strong presence as their engines are louder and vehicles are larger. A speed study should be requested of the TRORC to verify actual speeds traveled by the general public. This information would support additional design decisions and implementation of a preferred alternative.

### Sight Distance

There are a number of locations along Beaver Meadow Road that have limited sight distances due to curves

or hills in the road. Additionally, in some places, rock ledge or other roadside objects block the amount sight distance available. This is discussed further in the Constraints section on page 10.

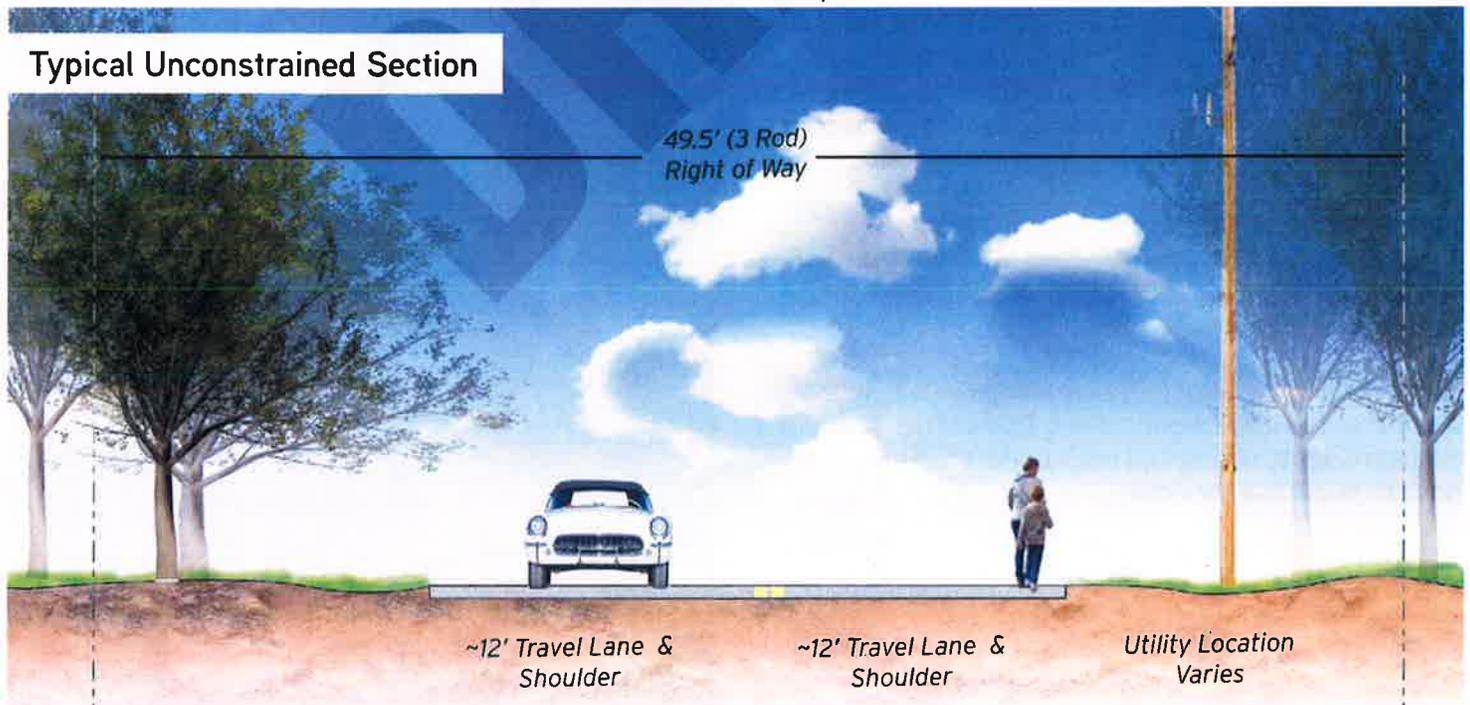
### Right-of-Way

Based on parcel data from VCGI, the right-of-way (ROW) of Beaver Meadow Road is assumed to be generally 3 rods or 49.5 feet. Road ROW is generally centered on the road's centerline. Precise measurements of this right of way will require a full survey to determine variance throughout the corridor.

### Pavement Width

The road or pavement width along Beaver Meadow Road is approximately 24 feet throughout the corridor. There is no edge line striping, so each travel lane is 12 feet wide. Beaver Meadow Road was repaved in September 2020 by the Town, and the pavement is currently in good condition, though additional repair needs are anticipated.

### Typical Unconstrained Section



### Crash History

Based on crash data the VTrans Crash Query Tool, there has been one reported crash in the project area in the past five years (2014 - 2019). It was of low severity, resulting in property damage only. This data only includes reported crashes, and it should be noted that near misses are unreported.

### Sidewalk Maintenance

Currently, a sidewalk network exists between southern Beaver Meadow Road, Main Street, and Turnpike Road. This route is already maintained in winter by snowplow. As a dead end network, plow drivers must return the way they came and retrace their path. Should a sidewalk be completed on Beaver Meadow Road, this completion of the loop will offer some efficiency to maintenance crews being able to maintain the whole loop without doubling back as is currently the case.



## Utilities

### Overhead Utilities

Power lines and other overhead utilities weave through the study corridor. There are utility poles on both sides of the road. Offset varies, but poles tend to be located 6 feet or more from the pavement edge.

### Water Lines

Municipal water lines run under Beaver Meadow Road. They service the buildings along this corridor as well as hydrants along Beaver Meadow Road. The waterlines are currently not digitally mapped. The project team located hydrants during a site visit, they are shown in the constraints section on page 10.

### Septic

The Study team reviewed the Vermont Agency of Natural Resources Department of Environmental Conservation's permit records at <https://dec.vermont.gov/water/forms/ww-systems-permits>. 48 records and site plans for subdivisions and wastewater systems along Beaver Meadow Road were reviewed. All but one septic system reviewed were located 50' or more from the public right of way. One system located at the NW corner of Bragg Hill and Beaver Meadow Road intersection has units constructed approximately 25' from the public right of way.

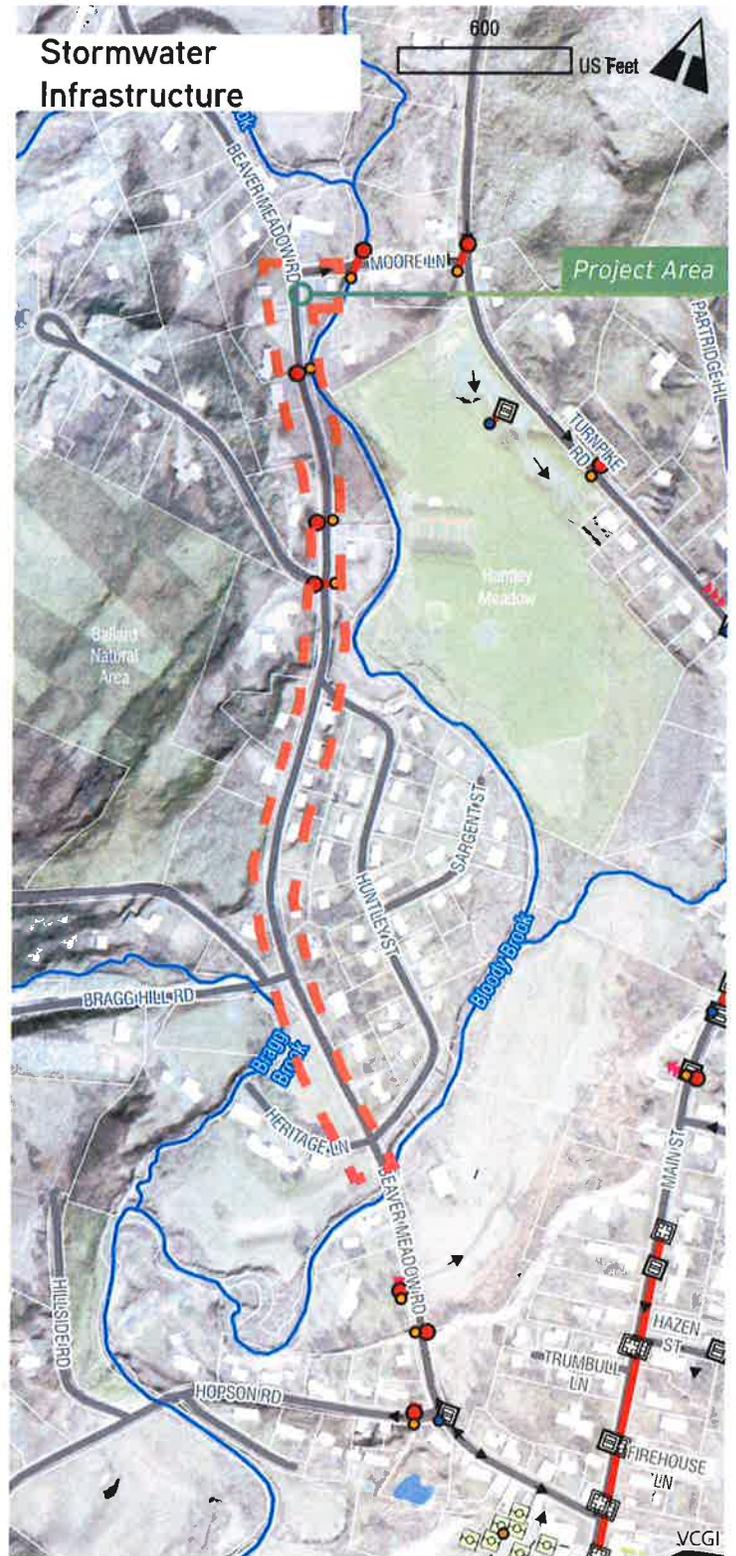
However, any construction proposed in this study would take place within the public right of way for Beaver Meadow road, or within 10' of that boundary. This would provide more than the 10' clearance required from the edge of leachfields, septic tank, or other sub-surface wastewater structures as required by the [State of Vermont's Wastewater System and Potable Water Supply Rules \(April, 2019\)](#).



## Stormwater

Stormwater that falls along the Beaver Meadow Road corridor primarily flows into Bloody Brook. Currently, there are no catch basins or storm lines in the project area, and stormwater runs overland into Bloody Brook. Most stormwater flows over Beaver Meadow Road from west to east.

There are three culverts in the northern section of the project area which bring stormwater from the west side of Beaver Meadow Road to the east side, where the water then flows into Bloody Brook.



# Specific Constraints

This section of the Sidewalk Scoping Study reviews the entire project corridor and identifies specific constraints on either side of the road. Utilities, terrain, and safety concerns have been identified as constraints when they occur within or adjacent to the publicly owned right of way and would impact the feasibility and/or cost of sidewalk or roadway construction along Beaver Meadow Road.

## Constraints along Beaver Meadow Road (1 of 5)



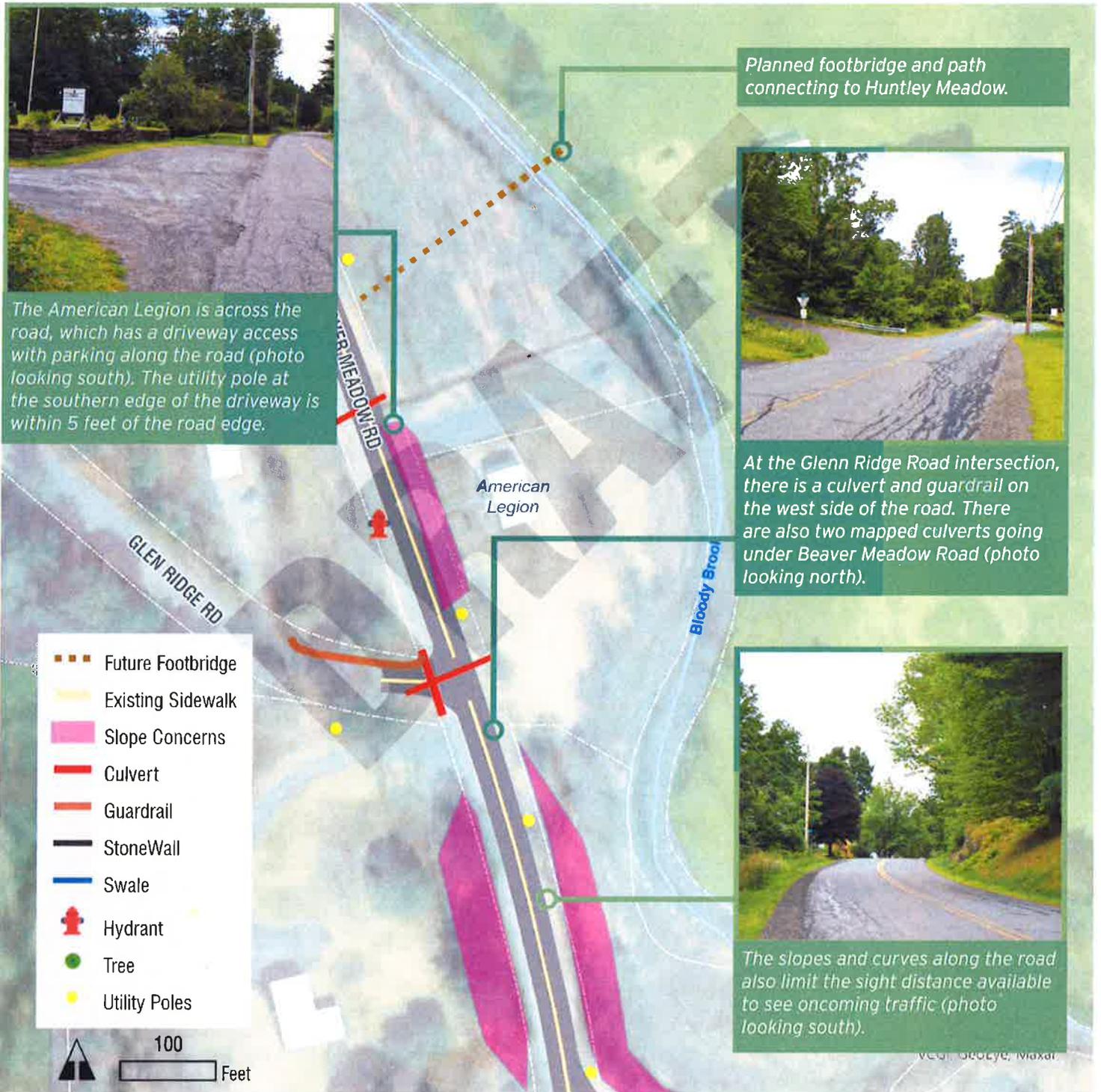
## Constraints along Beaver Meadow Road (2 of 5)



Constraints along Beaver Meadow Road (3 of 5)



## Constraints along Beaver Meadow Road (4 of 5)



Constraints along Beaver Meadow Road (5 of 5)



## Natural Resources

### Surface Water

Bloody Brook runs adjacent to Beaver Meadow Road in the northern section of the study corridor. Bloody Brook drains into the Connecticut River southeast of the project area.

Between Moore Lane and the northern Huntley Street intersection, portions of Beaver Meadow Road are in the Department of Environmental Conservation's (DEC) designated River Corridor. The river corridor includes the area surrounding a stream that provides the necessary land for river meandering, floodplains, and riparian functions for a naturally stable river, or a river in its least erosive form.

Bloody Brook crosses Beaver Meadow Road south of the project corridor, near Heritage Lane/Huntley Street. There is a bridge for this stream crossing with a sidewalk on the east side of the road.

Based on the Norwich Zoning Regulations (amended in 2009), Bloody Brook has a 100 foot setback for Primary Shoreland Protection Area. However, the same portions of Beaver Meadow Road that are in the river corridor are also already within the 100 foot setback.

Because of the adjacencies of river corridor (and flood hazard) areas in this corridor, any application for a flood hazard bylaw permit will also need to be submitted to the local DEC Floodplain Manager for review and comment.



## Flooding

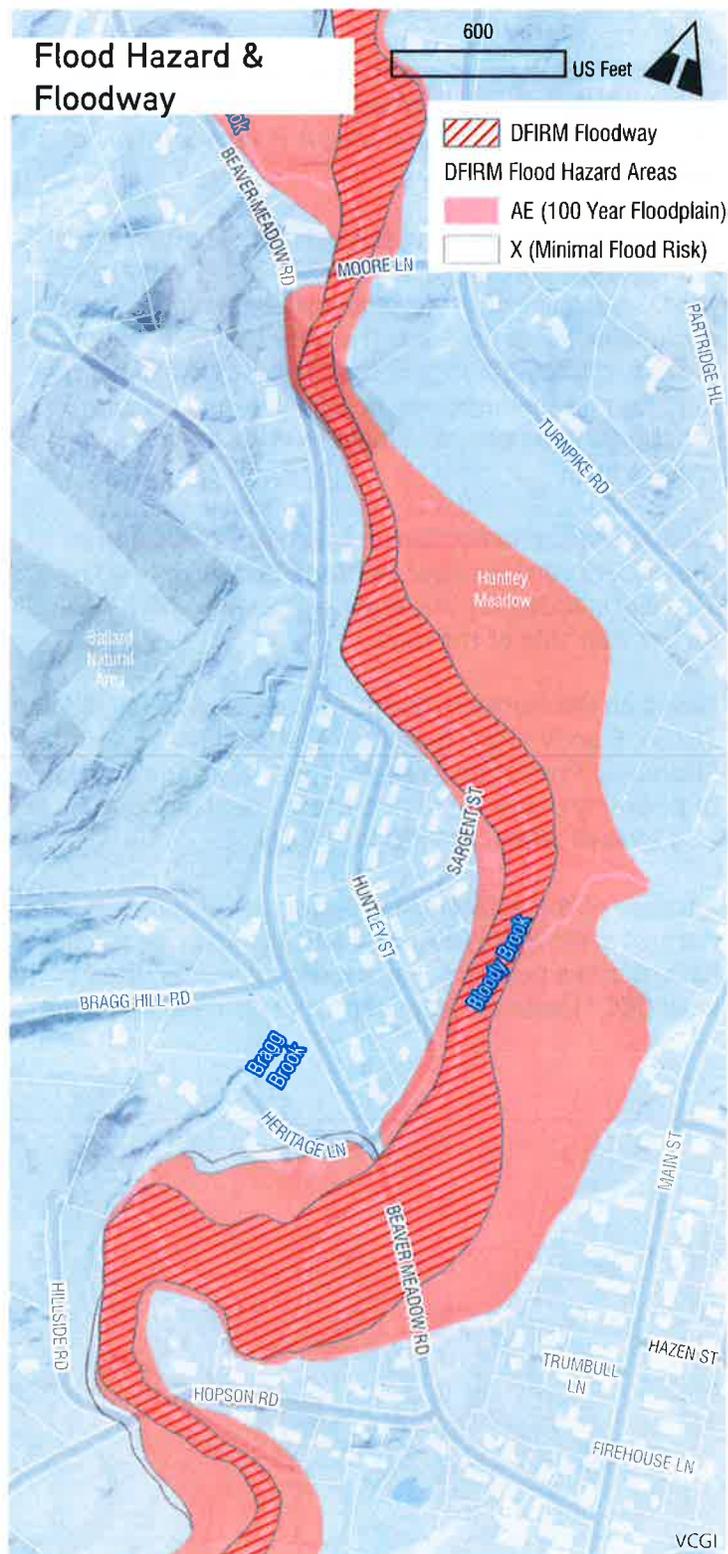
The project team reviewed the Federal Emergency Management Agency (FEMA) Digital Flood Insurance Rate Maps (DFIRM) to identify any flood hazard areas. A portion of this Beaver Meadow Road is in the 100-year floodplain. This area, south of Moore Lane, is shown in the map to the right. The 100-year floodplain is the area that has a 1% annual chance of flooding due to storm events. In addition, a piece of this road is also in the floodway. The floodway is the portion of the floodplain that contains the fastest moving (and most dangerous) portion of a 100-year flood event.

The floodway complicates building additional infrastructure on the east side of the road in the northern portion of the study area, since it would be prone to being washed out frequently. In addition, construction in the floodway is governed by Norwich's Zoning Regulations (amended in 2009). Section 5.05 (B) (1) states that:

*Development, except for minor improvements to existing structures or relating to bridges, culverts, roads or stabilization projects, within the regulatory floodway is prohibited. Such exceptions require conditional use approval prior to permitting and must comply with the other standards in Section 5.05(B) demonstrated through hydrologic and hydraulic analyses performed and certified in accordance with standard engineering practice by a registered professional engineer that the proposed development will result in no increase in flood levels during the occurrence of the base flood.*

Building within the 100-year floodplain is subject to conditional use by the Development Review Board and is subject to the following regulations from Section 5.05 (B) (2):

- a. designed (or modified) and adequately anchored to prevent flotation, collapse, or lateral movement of the structure during the occurrence of the base flood,
- b. constructed with materials resistant to flood damage,
- c. constructed by methods and practices that minimize flood damage, and
- d. constructed with electrical, heating, ventilation, plumbing and air conditioning equipment and other service facilities that are designed and/or located so as to prevent water from entering or accumulating within the components during conditions of flooding.



### Wetlands

There is a 6.7 acre wetland on the edge of Huntley Meadow. It lies outside of areas of potential impact along Beaver Meadow Road for this project. There are no other wetlands in the project area.

### Rare, Threatened, or Endangered Species

No rare, threatened, or endangered species were located in the project area. However, Long-Eared Bat is a threatened species whose habitat spans the entire state of Vermont.

### Other

The following resources were reviewed but not found in the project area:

- Hazardous Waste Sites
- Sole Source Aquifers



## Adjacent Landowners

This section identifies the owners of parcels adjacent to the project area.

ID	OWNER	OWNER2
1	ROJAS-SOTO DIANA M	RODRIGUEZ-PANIAGUA ALBERTO
2	MCDONALD LORI C	
3	ORNER PETER M	
4	FULD ALEXANDER D	HORT SHOSHANA J.
5	GATES CHARLENE J.	
6	HOISINGTON PAUL	
7	CROW REBECCA S	CROW PAUL VINCENT
8	MOISTER F.CORBIN JR	
9	VISION PROPERTIES VERMONT LLC	
10	SEYBOLD ERIC SCOTT	FOSTER TARYN L.
11	AMERICAN LEGION POST 8	
12	CANDON JOHN C	CANDON MARTHA P
13	BAKER NANCY E	
14	THOMPSON KEVIN	KELLY KATHRYN
15	RAMANATHAN CHANDRASEKHAR	BRIGGS PATRICIA
16	HAMLIN LOUISE E.	LENHART GARY A.
17	GRIBBLE GORDON W	
18	GARRIGUE ANNE M.	
19	FLANNERY ELISABETH L.	
20	No data	
21	NORWICH TOWN OF	
22	NORWICH TOWN OF	
23	LIVINGSTON BRIAN F.	LIVINGSTON ELAINE P.



## Cultural Resources

This study included both an archaeological resources assessment (ARA) and historic resource inventory (HRI). These reports are summarized here, and the full reports are included in the appendix of this document.

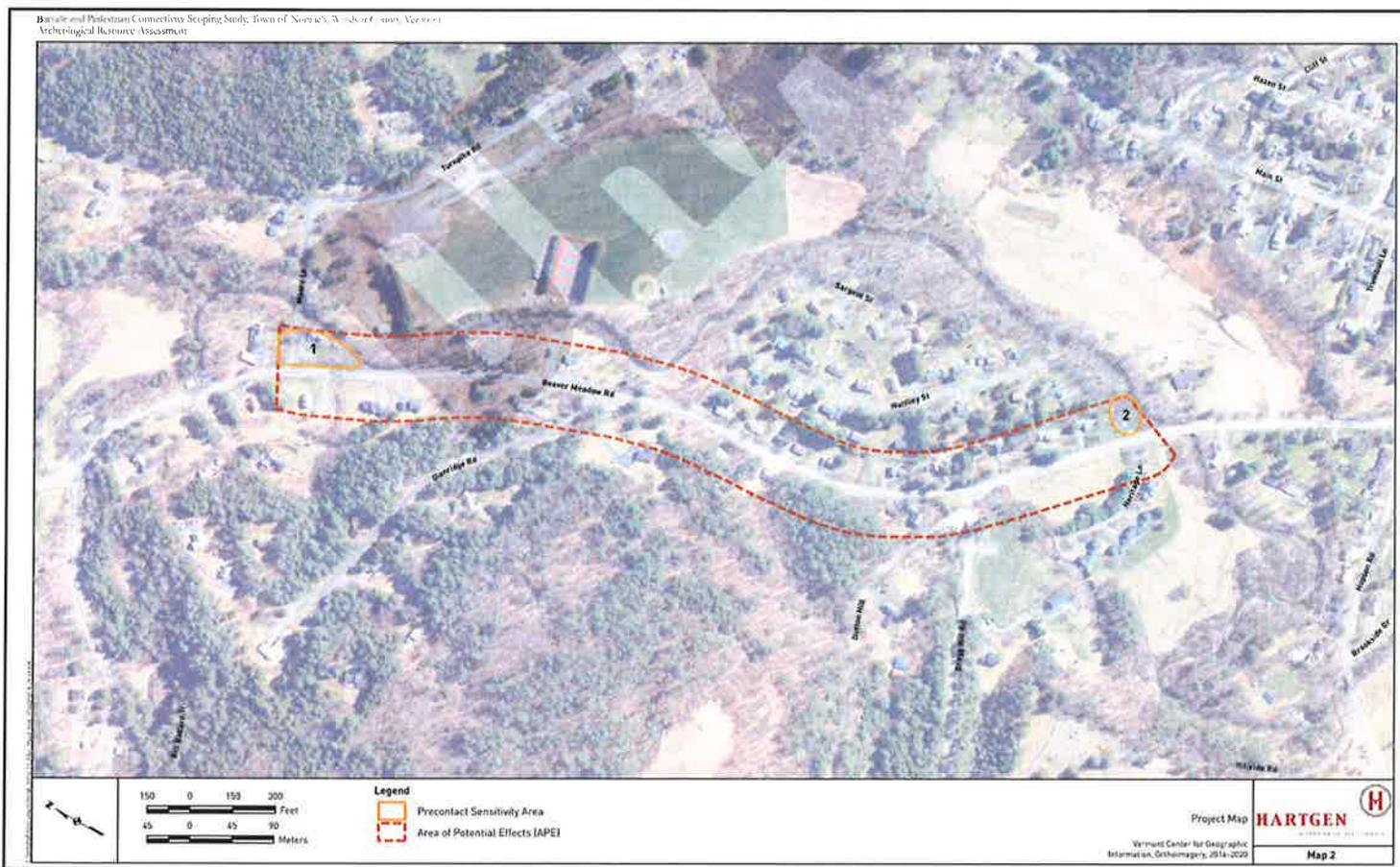
Both reports studied the project area of a 20' swath surrounding either side of Beaver Meadow Road's pavement profile to assess and document potential impacts to historical and cultural resources that any future construction might entail.

The ARA found no historic archaeological sites within or adjacent to the project area, but found four within a mile of the project area. The HRI and ARA did not identify national register listed or eligible properties. One state register listed property was identified, the circa 1780 Burton-Ballard house located at 149 Beaver Meadow Road. The HRI report recognizes this

structure as eligible for the National Register. The report recommends avoiding impacts to the stone wall associated with this residence wall and its surrounding historic plantings.

Both reports reviewed 19th century historic maps to gain an overview of the historic and environmental context of the project area.

The ARA report identified areas of pre-contact archaeological sensitivity at the eastern and western ends of the project alignment adjacent to Bloody Brook. Phase 1B testing is proposed in either of these areas, should a preferred alternative impact either.



## Public Engagement

Due to concerns related to the pandemic, this project's public engagement efforts were primarily online. A project website, (<http://tinyurl.com/beavermeadowsidewalk>) provided project information and public survey opportunities. This website served as primary public feedback mechanisms, along with a dedicated project email address and two in-person video-conference meetings. These meetings were the Local Concerns Meeting, to discuss the project in broad terms and understand local concerns and positions regarding the project, as well as the Alternatives meeting in which the public was given an opportunity to respond to proposed alternatives for future walking and bicycling improvements along Beaver Meadow Road.

The Alternatives meeting is discussed in greater detail in the Alternatives chapter of this document.

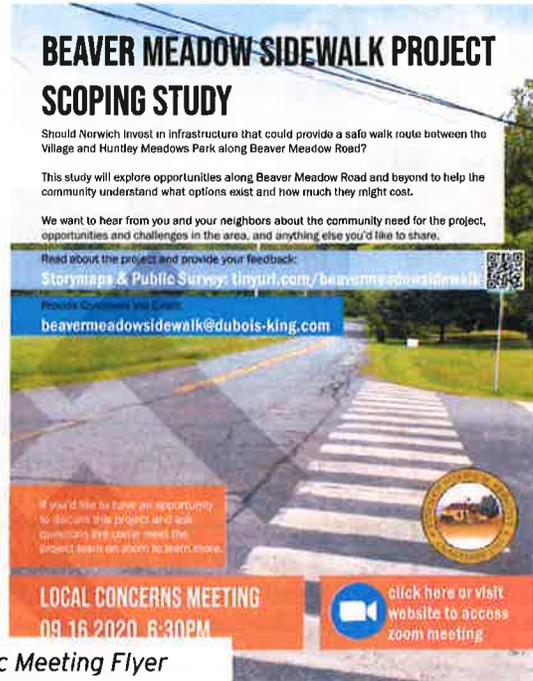
## Local Concerns Meeting

The Local Concerns meeting was preceded by promotion of the meeting and this study through digital and physical media. A project website outlined the study and provided an overview of existing conditions throughout the Beaver Meadow Road area.

The project website directed interested members of the public to fill out an online survey, attend the public local concerns meeting, or send in comments directly to the dedicated project email.

Project communications, such as the website and associated survey, were advertised in Norwich through local news stories, the Town website, and on the local list serv. Physical sandwich boards were placed along Beaver Meadow Road that further advertised this study's effort to gather public input. Between September 1st and September 30th, the official project survey received 422 responses.

The Local Concerns meeting, held at 6:30 pm on September 16th through online video conferencing, hosted 38 members of the public who offered feedback on the value, need, and potential impacts of a sidewalk along Beaver Meadow Road.



Public Meeting Flyer

## Initial Public Feedback

As evident by the large number of Norwich residents who took the time to fill out a survey, write an email, or attend a meeting on Zoom, there was significant passion for and against a project that could result in changes to Beaver Meadow Road.

Of the responses (online survey, emails, public meeting), 313 support the future development of a sidewalk along Beaver Meadow Road. 79 oppose the development of a sidewalk along Beaver Meadow Road.

Among those who support construction of a Beaver Meadow Road sidewalk, two major themes were present - overall safety, and safety for youth and children utilizing the roadway.

Among those who opposed future development of a sidewalk along Beaver Meadow Road, cost to the Town was the most often cited reason for opposition to the project. Additional project opponents questioned the true need for improvements to the roadway. There was also a sentiment that the Town should tackle other projects first before making investments in a sidewalk.

**Total Local Concerns Feedback** *(Online Survey, Emails, Public Meeting)*

Support Beaver Meadow Road Sidewalk Construction	313
Oppose Beaver Meadow Road Sidewalk Construction	79

**Total Local Concerns Feedback**

- Support Beaver Meadow Road Sidewalk Construction
- Oppose Beaver Meadow Road Sidewalk Construction



**Why Residents support construction of a Beaver Meadow Road Sidewalk**

Need for improved safety along corridor	16
Need for improved access for youth	8

**Why Residents would prefer no investment in Beaver Meadow Road Sidewalk**

Cost prohibitive	7
Not a needed safety improvement	5
Norwich must prioritize other projects first (Septic repairs)	4
Additional maintenance costs are significant	2

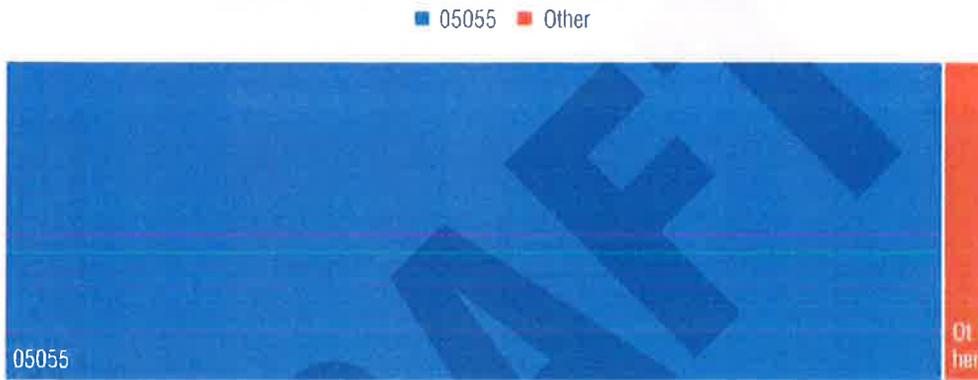
*Note: the "Why Residents" totals are less than the overall total Support / Oppose totals because only a small subset of feedback included rationales.*

## Local Concerns Survey Response

The response to the online survey was significant, the 422 responses representing nearly 13 percent of Norwich's 3,318 person population. 95 percent of responses identified their home zip code as 05055 (Town of Norwich). The charts that follow outline the results of this survey.

The full record of public input, including over 300 open ended responses was provided to the Town of Norwich for additional review and processing.

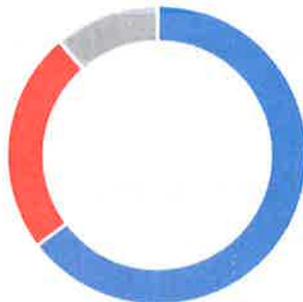
### Respondent Zip Code



### When it comes to town priorities - which of these two statements best align with your views?

Norwich should focus on completing the sidewalk network, even if that means that portions are in poor condition.	273
Norwich should focus on having better condition sidewalks, even if that means the network of sidewalks is incomplete.	101
No response provided	48

### Town Sidewalk Philosophy



- Norwich should focus on completing the sidewalk network, even if that means that portions are in poor condition.
- Norwich should focus on having better condition sidewalks, even if that means the network of sidewalks is incomplete.
- No response provided

**How do you use BMR?**

Bicycle	208
Walk	313
Run	164
Drive	356
Other	23

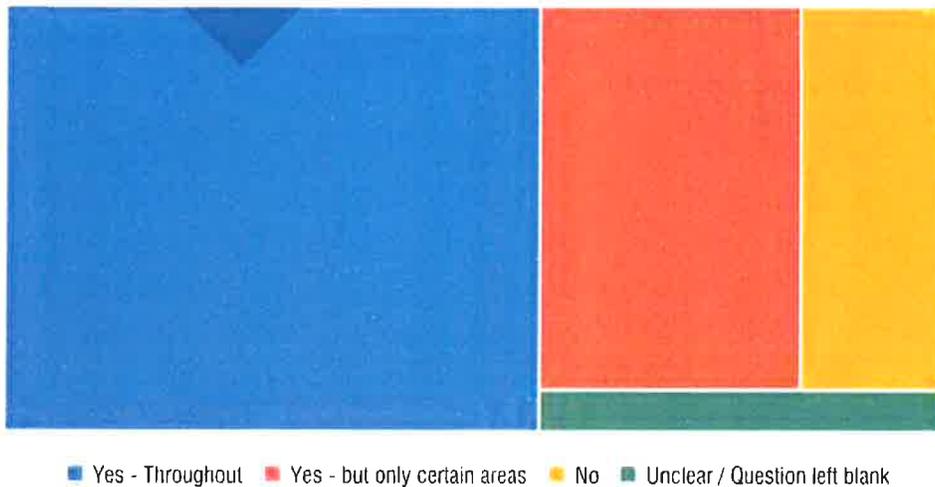
**Reported Modeshare**



**Do you feel safety is a concern for BMR users?**

Yes - Throughout	240
Yes - but only certain areas	105
No	59
Unclear / Question left blank	18

**Is Safety a Concern along Beaver Meadow Road?**



**Should a sidewalk be constructed along Beaver Meadow Road?**

Yes	291
Yes, but not at this time	7
Maybe	50
No	71
Unclear / Question left blank	3

**Should a Sidewalk Be Built?**

Yes
  Yes, but not at this time
  Maybe
  No
  Unclear / Question left blank



**Key Takeaways**

- The majority of residents support a sidewalk project on Beaver Meadow Road.
- Beaver Meadow Road is a multimodal corridor supporting drivers, walkers, runners, and cyclists alike.
- The safety of road users, especially children, is a high priority.
- The primary concern expressed regarding this project is its relative cost to implement.

# Purpose and Need

Introduction

Existing Conditions

Purpose and Need

Alternatives

Implementation

Appendices

## Purpose

The purpose of this study is to investigate the feasibility, value, form, and cost of potential sidewalks or other active transportation solutions along Beaver Meadow Road in Norwich, VT. By investigating and documenting design constraints and public response to a potential investment, the Town of Norwich can make wise investment of public funds. This study will investigate how dedicated infrastructure for non-vehicular traffic could improve safety and road user comfort, and create a continuous sidewalk loop linking the Norwich Village Center, Huntley Meadow, Moore Lane and Turnpike Road.

## Need

Beaver Meadow Road provides direct connection between the Norwich Village Center, multiple public parks, a church, and numerous residences. It is part of a popular walking, running and biking route that is used by many. Aside from the project corridor along Beaver Meadow Road, this loop route has sidewalks for its entire length, leaving a clear gap in the sidewalk network.

The rolling terrain, roadway curves, adjacent landforms and stream banks create constraints require pedestrians, cyclists, and motorists to share the same 12 foot travel lane, which in many places lacks sufficient sight distance to see around corners or above and below hills. This leads to significant safety concerns, despite Beaver Meadow Road's proximity to the Village Center and direct connections to parks and homes.



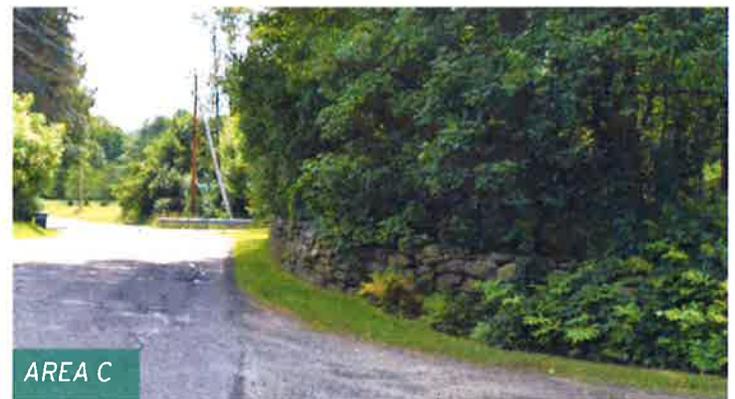
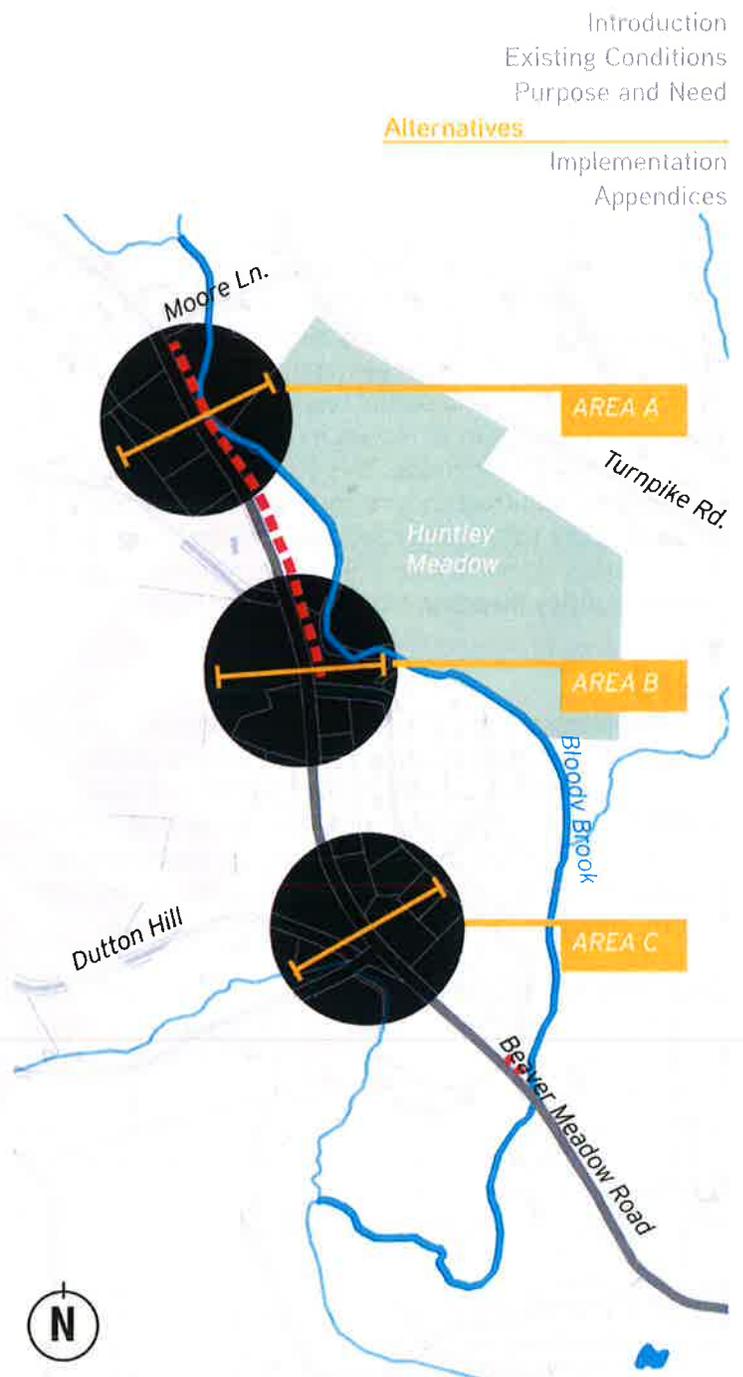
# Alternatives

Following the Local Concerns meeting, the project team developed four unique alternatives to address pedestrian safety and connectivity issues along Beaver Meadow Road.

## Areas of Constraint

Much of Beaver Meadow road would accommodate sidewalk construction in a straightforward manner. However, key constraints were identified in the existing conditions analysis segment of this study. The most significant of these constraints are identified in each alternative. These significant constraints include:

- Area A - the roadway's closest point to Bloody Brook, immediately south of Moore Lane.
- Area B - North of Huntley St intersection bounded by steep rock ledge to the west and downhill slopes towards bloody brook to the east.
- Area C - Immediately north of Bragg Hill Road, where historic stone walls within the Right of Way constrain the roadway.



## Alternatives

**Alternatives**

## Alternative 0: No Build

One alternative supported by a vocal minority of the community surveyed is to leave Beaver Meadow Road infrastructure in place and direct Norwich public works resources elsewhere.

This alternative would not change conditions related to traffic calming or safety through the roadway. Drivers and pedestrians / cyclists would continue to share the roadway as currently. Additional pedestrian and cyclist traffic may be added to portions of Beaver Meadow Road as plans move forward with a future bridge between Huntley Meadow park and Beaver Meadow Road just north of Glen Ridge and the American Legion.

The sections on the following page illustrate existing conditions on Beaver Meadow Road, and are replicated for the alternatives considered in this study.

### Impacts

This alternative would have minimal impact to existing conditions, but would do nothing to improve pedestrian safety or comfort along Beaver Meadow Road, and would not contribute to reduced automotive traffic or emissions throughout this corridor.

### Benefits

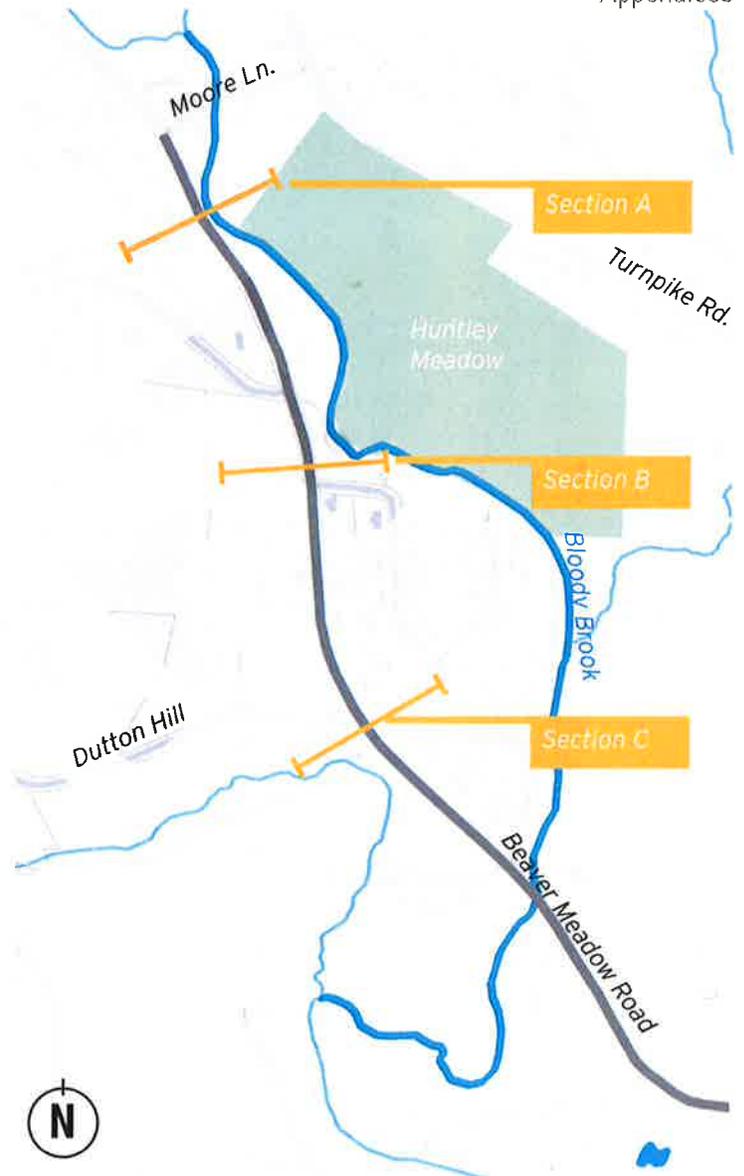
This alternative would allow the Town of Norwich to dedicate funding resources elsewhere in the community.

### Drawbacks

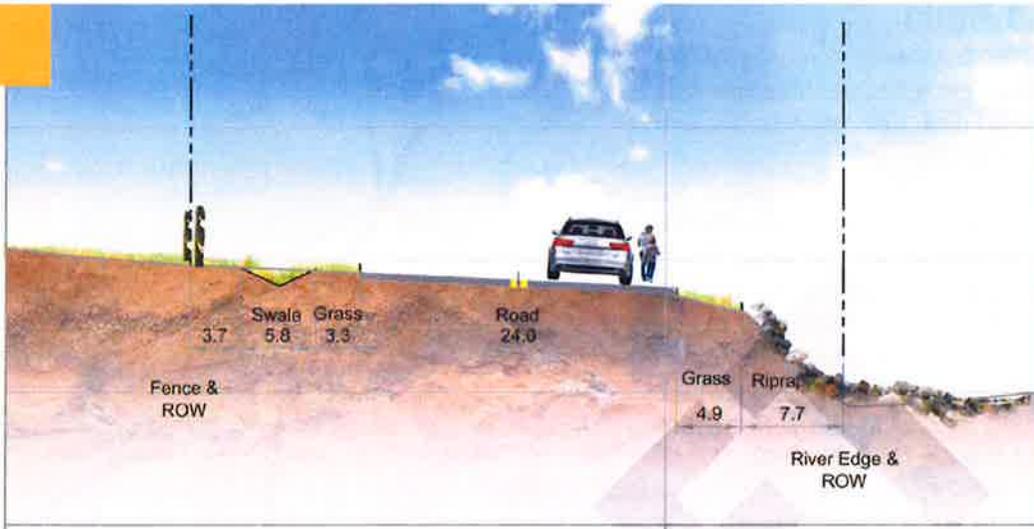
This alternative does nothing to calm traffic or provide a safe walking route along Beaver Meadow Road.

### Planning Level Cost Estimate

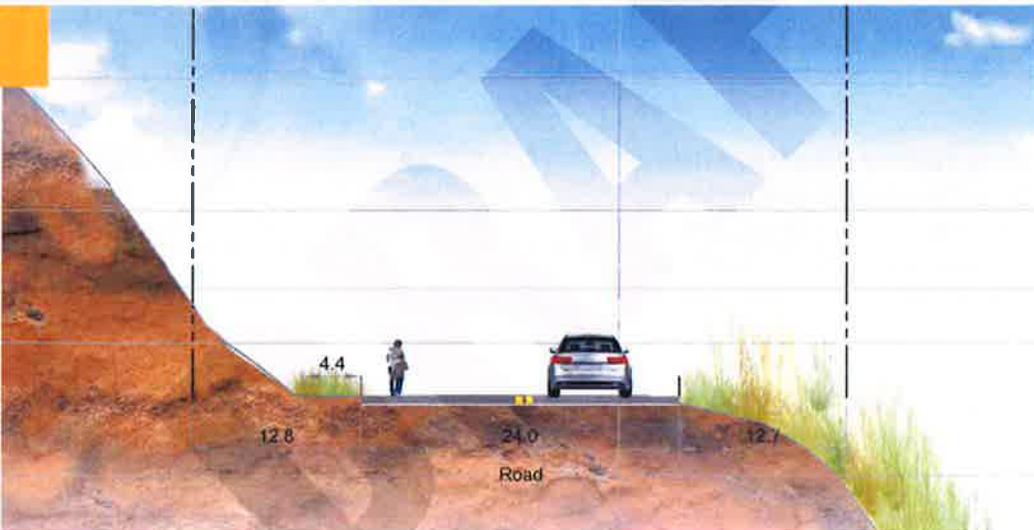
\$0



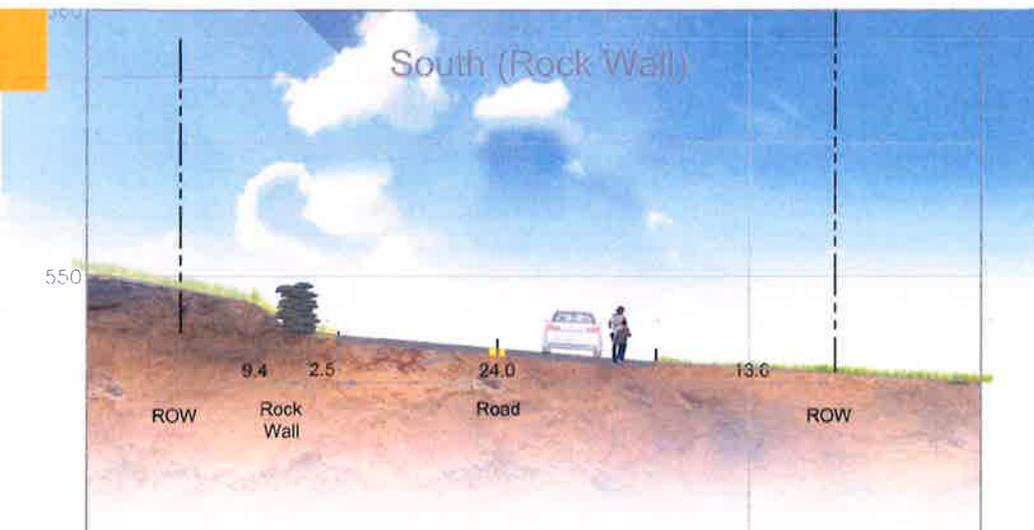
No Build -  
Section A



No Build -  
Section B



No Build  
Section C



## Alternative 1: Sidewalk West

This alternative envisions a sidewalk built along the west side of Beaver Meadow Road. This would require pedestrians to cross Beaver Meadow Road at both ends of the sidewalk to connect with existing safe routes and sidewalk. Future connections with Huntley Meadows or the American Legion would also require the crossing of Beaver Meadow Road.

### Impacts

This alternative will require the relocation of overhead utilities throughout the corridor. North of Huntley Street, it is anticipated that construction alongside existing visible ledge will also require a narrowing of the roadway. In front of the properties immediately south of Moore lane, the alignment will require movement of existing fences and drainage swales to accommodate the sidewalk.

In addition, existing stormwater infrastructure such as catch basins and culverts surrounding Bragg Hill will need to be preserved and/or replaced as part of the construction process.

Equally notable is the need for this alignment to avoid impact to historic stone walls north of the Bragg Hill intersection. The alignment will require removal of the existing guardrail at this intersection.

### Benefits

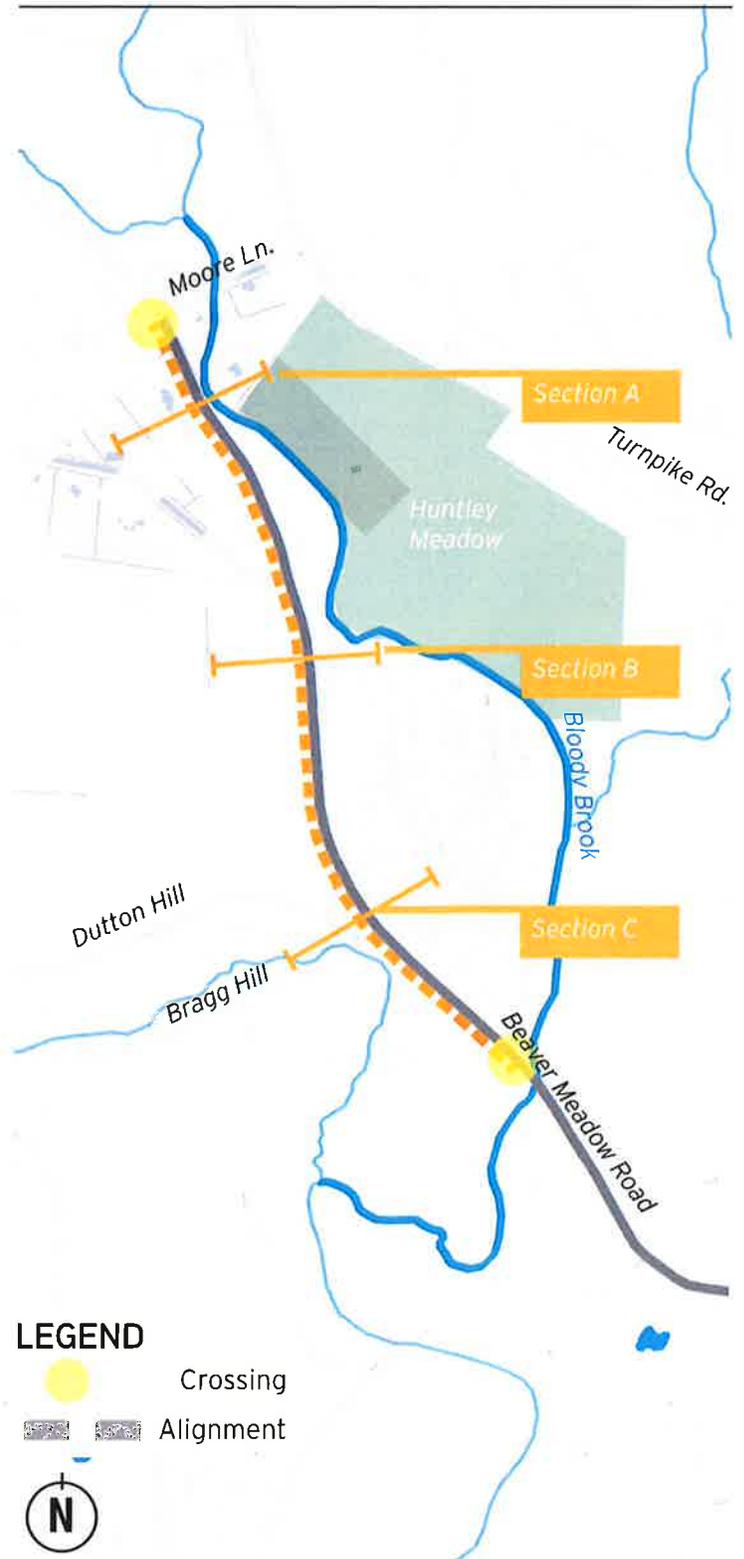
This alternative provides direct access to the small Pizza Oven park at the base of Dutton Hill and has a minimal number of adjacent landowners.

### Drawbacks

Sight distance concerns exist surrounding the Moore lane intersection. The area has limited sight distance to the north, and additional care should be taken to design an intersection that is visible from 150' or more in either direction. Additional pedestrian warning signalization should be considered.

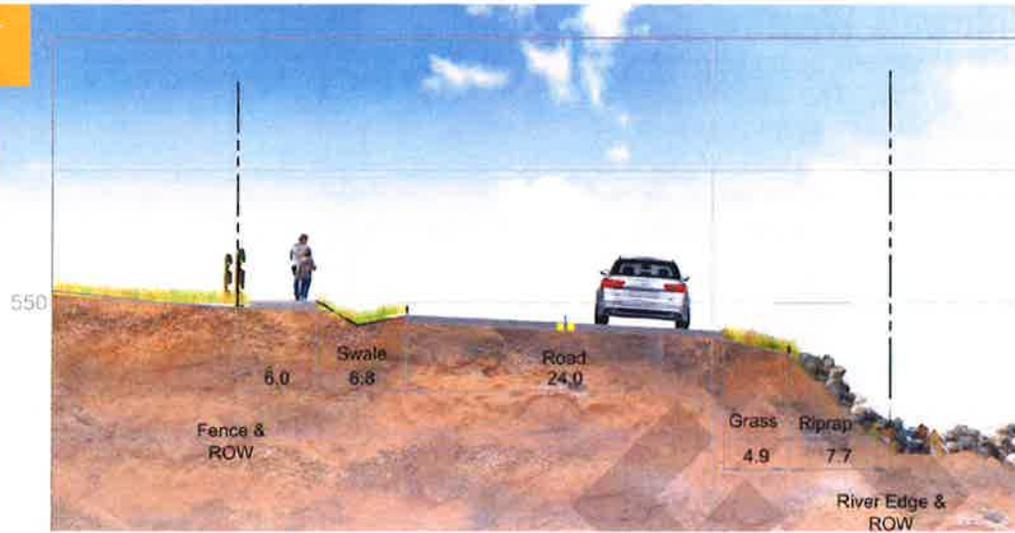
### Planning Level Cost Estimate

\$840,000

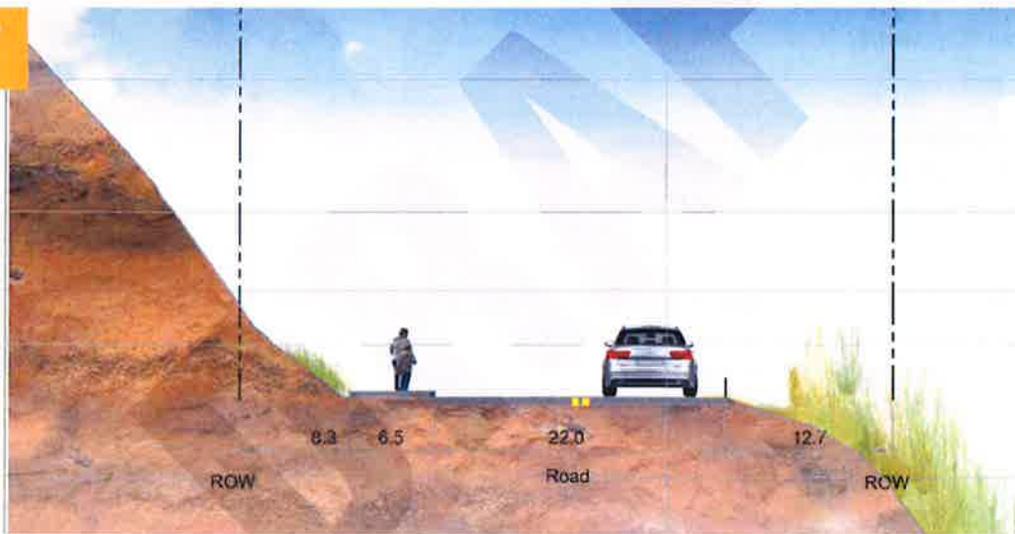


# Alternative 1 - West Side Sidewalk @ Areas of Constraint

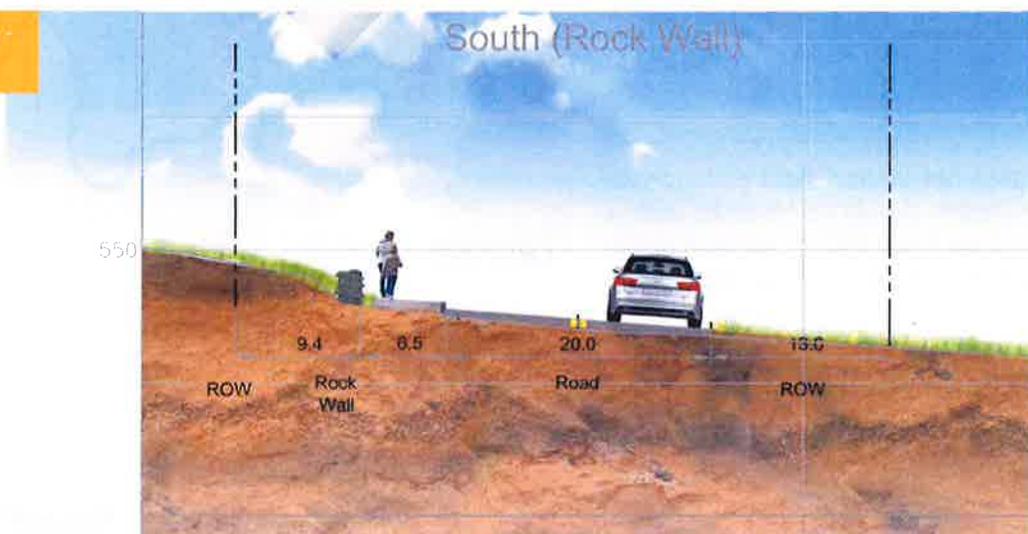
Alternative 1 -  
 Section A



Alternative 1 -  
 Section B



Alternative 1 -  
 Section C



## Alternative 2 - Sidewalk East

The second alternative envisions a sidewalk developed on the eastern side of Beaver Meadow road. This alignment would connect directly to existing sidewalk along Beaver Meadow Road and existing walkways on the bridge over Bloody Brook on Moore lane. Pedestrians would not need to cross Beaver Meadow Road unless accessing a residence on the western side.

### Impacts

This alternative would require relocation of overhead utilities throughout the corridor. The design would need to avoid impacting current parking and access to the American Legion located at 228 Beaver Meadow Road.

In order to cope with the adjacency to Bloody Brook, this design re-aligns Beaver Meadow Road between Moore Lane and the American Legion. Rebuilding the roadway to the west would accommodate a sidewalk east of the existing roadway edge along Bloody Brook. (See Section A) A small retaining wall would be required to support the sidewalk north of Huntley Street as it would be built over steep side slopes dropping towards Bloody Brook. (See Section B) This alignment would be constructed in the river corridor.

### Benefits

This alternative does not require construction of any new roadway crossings on Beaver Meadow Road, and offers direct access to Huntley Meadow and the American Legion.

### Drawbacks

Construction adjacent to Bloody Brook could prove costly and complex. Due to this factor, as well as need for retaining walls, this is the most costly and complex option. The public input process revealed strong opposition to any sidewalk project throughout the neighborhood between Huntley Street and Beaver Meadow Road.

### Planning Level Cost Estimate

\$880,000





## Alternative 3 - Sidewalk Crossing

A mix of alternatives 1 and 2, this option seeks to reduce cost and construction complexity by crossing Beaver Meadow Road north of Huntley Street. Impacts in constrained areas are identical to alternative 1 (area A and B) and alternative 2 (Area C)

### Impacts

This alternative requires relocation of overhead utility lines to accommodate the future sidewalk. It would also require minor adjustments to stormwater drainage and fence lines to the north.

The two street crossings would have to be designed carefully to ensure pedestrians are visible and are not exposed to additional risk by the crossing design or placement.

### Benefits

This alternative avoids the most costly and complex areas associated with Alternatives 1 or 2, and as such is the least expensive full sidewalk option considered in this report.

### Drawbacks

The road crossings present a safety concern, and the alignment is adjacent with the neighborhood most vocally in opposition to any sidewalk project on Beaver Meadow Road.

### Planning Level Cost Estimate

\$810,000

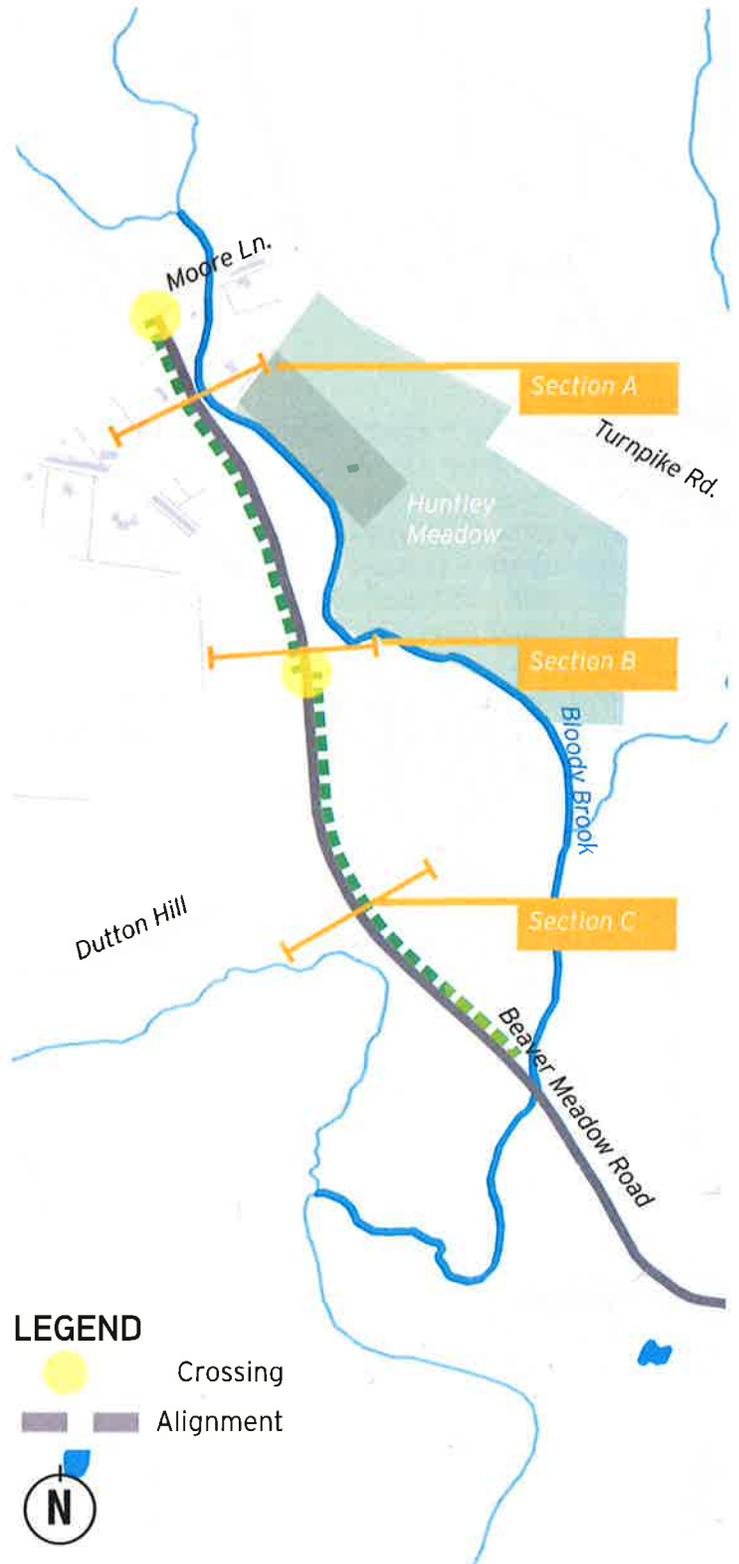
### Sections

The constraint area sections for this alternative would be a mix of west and east side alternatives discussed previously - specifically,

Constraint Area A - West Side Alternative Section (pg 30)

Constraint Area B - West Side Alternative Section (pg 30)

Constraint Area C - East Side Alternative Section (pg 32)



## Alternative 4 - Mobility Lanes

The Mobility Lanes alternative thinks outside the sidewalk to propose an expansion of the roadway pavement surface to allow for mobility lanes. Similar to a bike lane, but with expanded purpose, mobility lanes are envisioned as a 2-way, buffered bike and pedestrian travel lane. This alternative allows the town flexibility in future street layouts and designs. This alternative could also accommodate two one way mobility lanes on either side of the street.

### Impacts

This alternative would require careful design of the access points at either end of the mobility lane segment, or consider extending the lane southward to Main Street. Turning movements must be carefully considered when designing entry to a two way mobility lane. This alternative would also require accommodation of parking and access at the American Legion.

This alternative would also narrow travel lanes in multiple areas of constraint (A and B) to accommodate the additional use of the roadway.

### Benefits

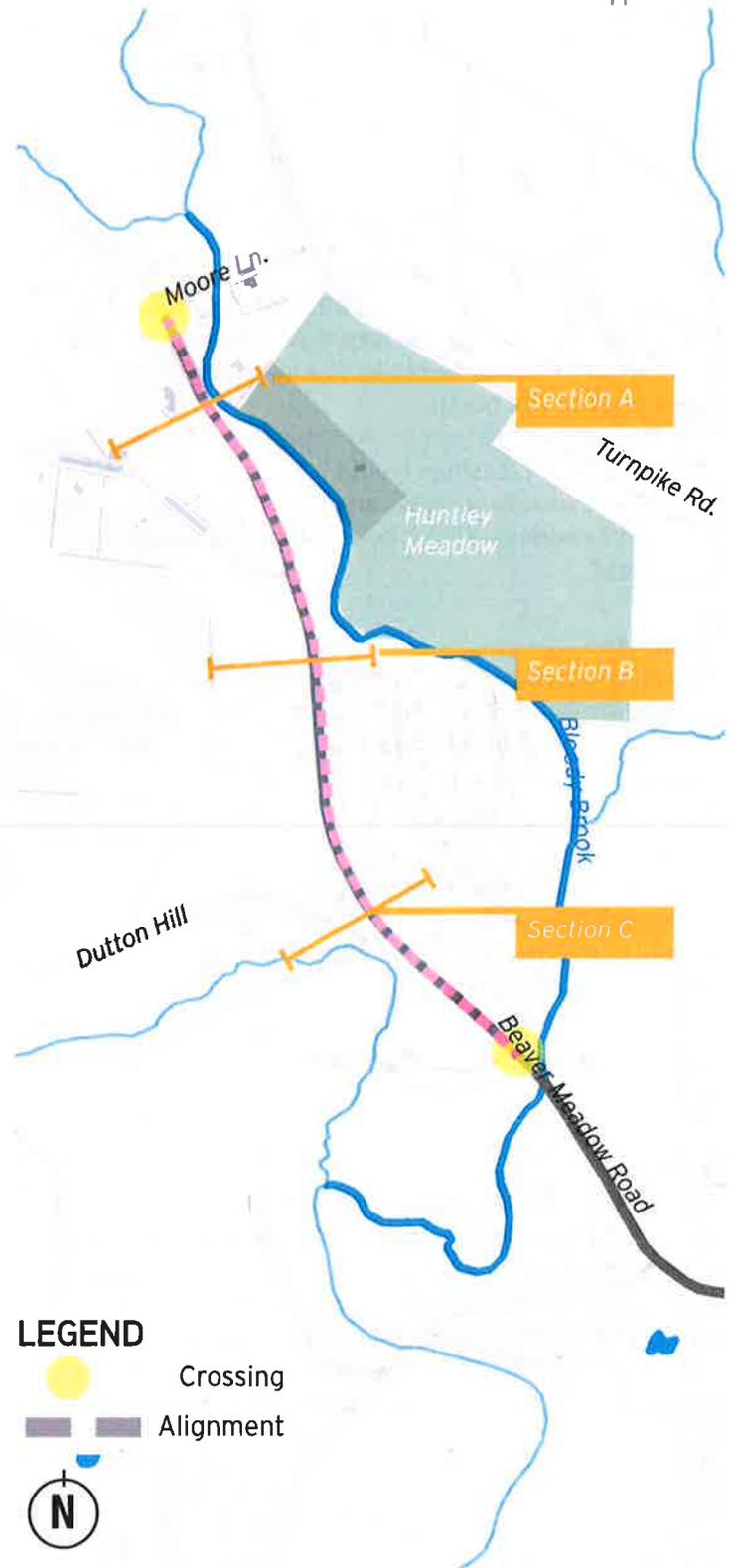
This alternative is the lowest cost alternative considered, and is the only one that could be designed to safely accommodate bicyclists and pedestrians within the same facility.

### Drawbacks

This alternative creates the largest amount of impervious area. In addition, the dedicated bicycle/pedestrian facility offers the least amount of separation and protection when compared to a grade-separated sidewalk.

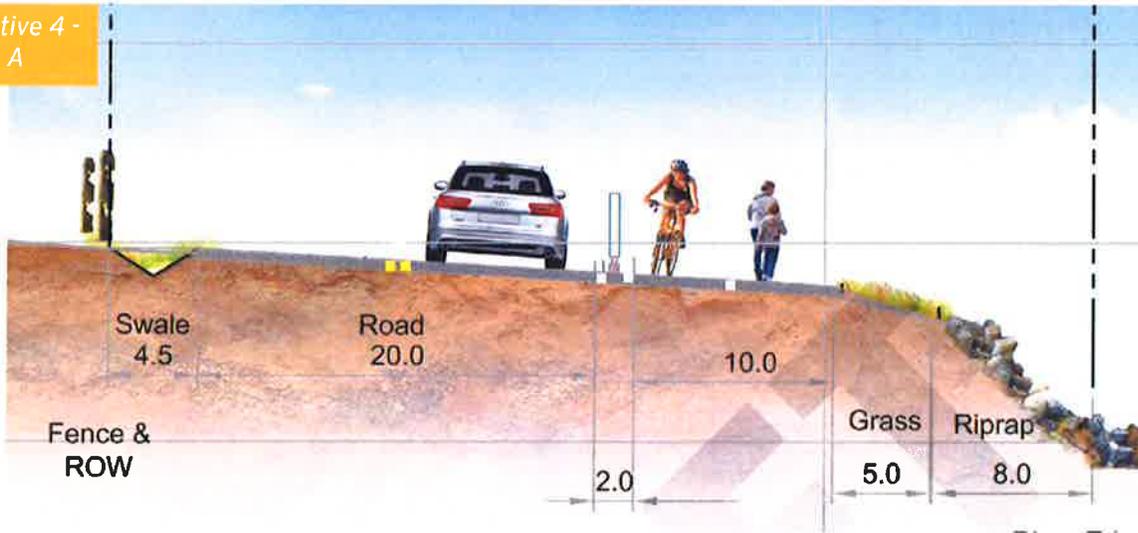
### Planning Level Cost Estimate

\$340,000

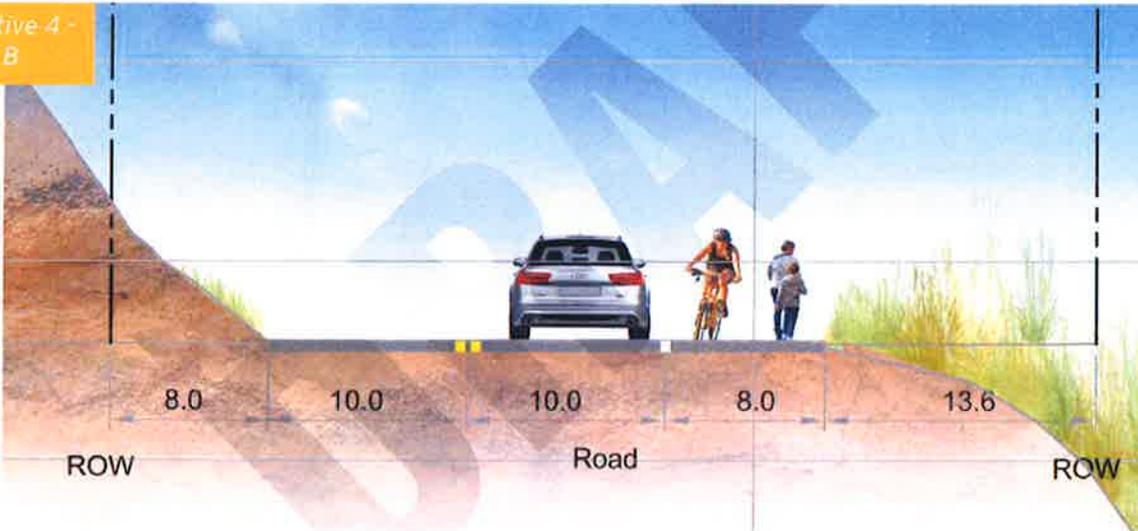


# Alternative 4 - Mobility Lanes @ Areas of Constraint

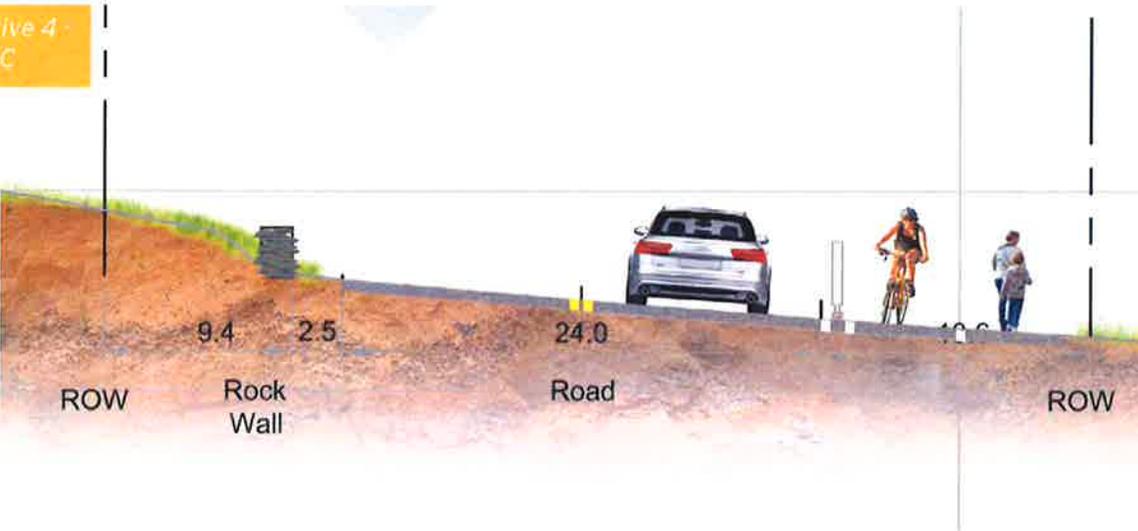
Alternative 4 -  
Section A



Alternative 4 -  
Section B



Alternative 4 -  
Section C



## Alternative 5 - Huntley Gap

This alternative was suggested by multiple members of the public during the alternatives meeting and in the associated survey. By building sidewalk outside of the Huntley Street Corridor, a safer - if longer - walking route could be developed which would route walkers and bikers to a Huntley Street detour.

In the alternatives public survey results discussed in this chapter, this alternative is referred to as "other alternative".

### Impacts

As with other options, this alternative would require relocation of overhead utilities throughout corridor. Impacts would be similar in constraint areas A and B to Alternative A.

This alternative would not offer full connectivity as it would encourage pedestrians and cyclists to utilize a slightly longer detour route around Huntley Street. Typical pedestrian transportation behavior, however, seeks the shortest distance between two points, and this alternative route might not be well utilized.

### Benefits

This alternative lowers cost compared to other sidewalk options, while providing a safe route through the corridor.

### Drawbacks

This alternative may not reduce overall safety concerns along a central portion of Beaver Meadow Road, due to the recommended detour around Huntley Street being less direct than a Beaver Meadow Road Route.

### Planning Level Cost Estimate

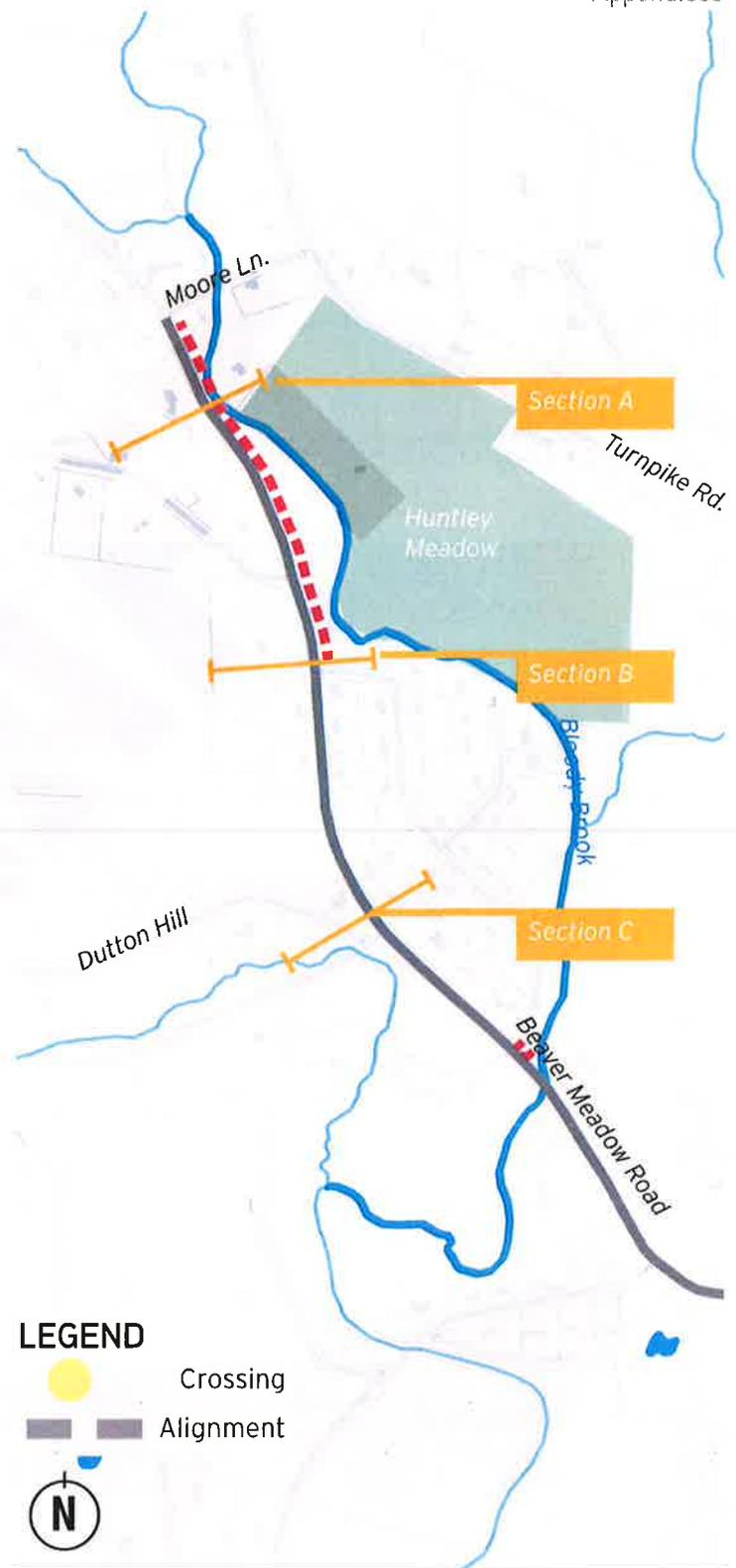
\$510,000

### Sections

Section A - East Side Alternative Section (pg 32)

Constraint Area B - East Side Alternative Section (pg 32)

Constraint Area C - No Build Alternative Section (pg 28)



## Alternatives Meeting

Following the local concerns meeting and review of input, an Alternatives meeting was held on November 12th, 2020. This meeting was accompanied by a slideshow description of the alternatives and survey posted to the project website a month in advance of the meeting. This meeting was held over video-conference to limit gatherings in accordance with COVID-19 guidelines.

The meeting was advertised through Norwich list serv announcements, physical sandwich boards along Beaver Meadow Road, and through the Town website. 25 members of the public gathered to review and discuss alternatives during this meeting, and 45 responses were collected as part of the survey.

During this meeting, numerous questions were raised regarding the exact location of right of way lines, and many residents were concerned that this study would not include survey to legally determine right of way bounds. There was strong support presented for a sidewalk along the roadway, with significant concern raised regarding

the cost and potential impacts to adjacent landowners.

Another outcome of the alternatives meeting was a community-driven suggestion to build a portion of the proposed sidewalk linking to Huntley Street, but then avoid impacts to that portion of Beaver Meadow Road, and encourage pedestrians to take the long route on Huntley Street due to its lower traffic volumes. This alternative is included as Alternative 5 in this study.

## Alternative Survey Results

45 people contributed to the survey whose results are illustrated in the graph on this page. Approximately 18% of responses did not feel Norwich should invest in sidewalk improvements along Beaver Meadow Road. 66% of responses felt that some form of a sidewalk should be constructed, but were evenly split between west, east, or alternating side options. The mobility lanes were the least popular alternative, receiving only 4% of the vote total, and a community suggested alternative that skips the sidewalk entirely received 11% of the vote.



Alternative 1 - Sidewalk West	11	24%
Alternative 2 - Sidewalk East	10	22%
Alternative 3 - Sidewalk Alternating Sides	9	20%
Alternative 4 - Mobility Lanes	2	4%
Other Alternative	5	11%

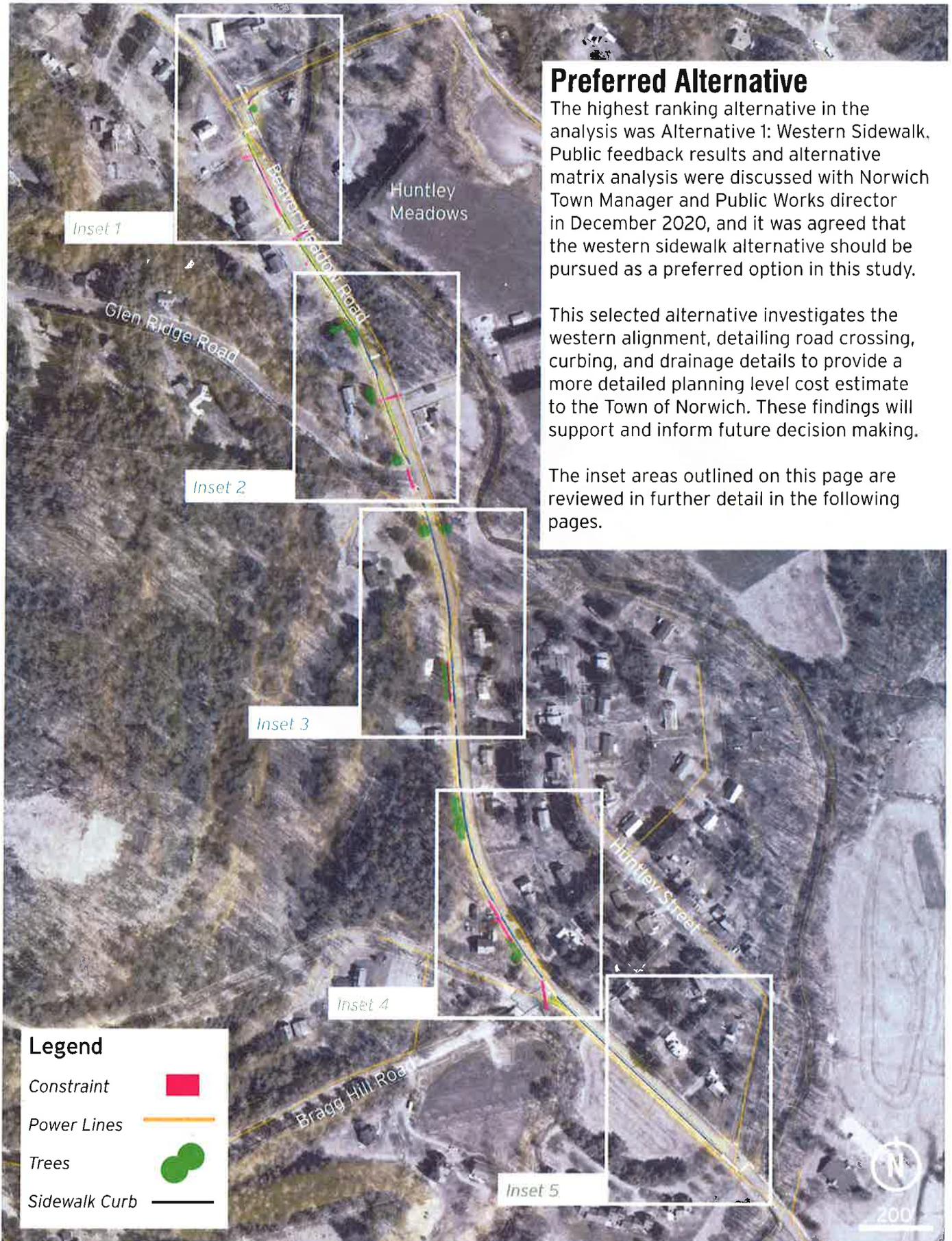
# Alternatives Evaluation

Based on public feedback and Town direction, all alternatives were analyzed in their ability to meet two overarching goals. Improve the safety and comfort of Beaver Meadow Road travelers and to find a solution that is implementable and cost effective.

An alternatives matrix was developed that weighed multiple elements of each goal. The average of these scores is indicated in green columns as safety average and effect average. The final score for each alternative is a sum of these two averages, in order to give even weight to the two project goals. The top two ranking alternatives in this methodology are Alternative 1, Sidewalk West, and Alternative 4, mobility lanes. However, Mobility lanes should be seen as a lesser alternative due to its lowest ranking in the public input process.

Alternative	Improve the safety & comfort of BMR residents & users				Implementable & Cost Effective Effort												Final Score	
	Pedestrian Safety	Bicycle Safety	Traffic Calming	SAFETY AVG	Property Impact Score		Cost Score		Natural Resource Score		Historic / Cultural Score		Construction Difficulty		Community Support			EFFECT AVG
					Adjacent Property Count		Concept Cost		Natural Resource Impacts		Historic/Cultural Resource Impacts				% of vote in favor			
0: No Build	0	0	0	0	0	3	\$0	3	None	3	None	3	None	3	18%	2	2.8	2.8
1: Sidewalk West	2.5	0	2	1.5	10	1	\$840,000	2	Ledge impacts & drainage changes	2	Plantings @ 1 home.	2	Moderate difficulty	2	24%	3	2	3.5
2: Sidewalk East	3	0	2	1.7	14	0	\$880,000	1	Stream impacts to Bloody Brook	1	Plantings @ 1 home. Arch. impacts	1	High difficulty	1	22%*	2	1	2.7
3: Sidewalk Alternate	2.5	0	2	1.5	8	2	\$810,000	2	Ledge impacts & drainage changes	2	Plantings @ 1 home.	2	Moderate difficulty	2	20%	1.5	1.9	3.4
4: Mobility Lanes	1	3	1	1.7	0	3	\$340,000	3	Highest amount of additional impervious	1	None	3	High difficulty	1	4%	0	1.8	3.5
5: Huntley Meadow Road Gap	2	0	2	1.3	4	2	\$510,000	2.5	Stream impacts to Bloody Brook	1	Arch. Impacts	1	High difficulty	1	11%	2	1.4	2.8

\*Alternative 2, Sidewalk East had the most vocal landowner opposition of any of the proposed alternatives.



## Preferred Alternative

The highest ranking alternative in the analysis was Alternative 1: Western Sidewalk. Public feedback results and alternative matrix analysis were discussed with Norwich Town Manager and Public Works director in December 2020, and it was agreed that the western sidewalk alternative should be pursued as a preferred option in this study.

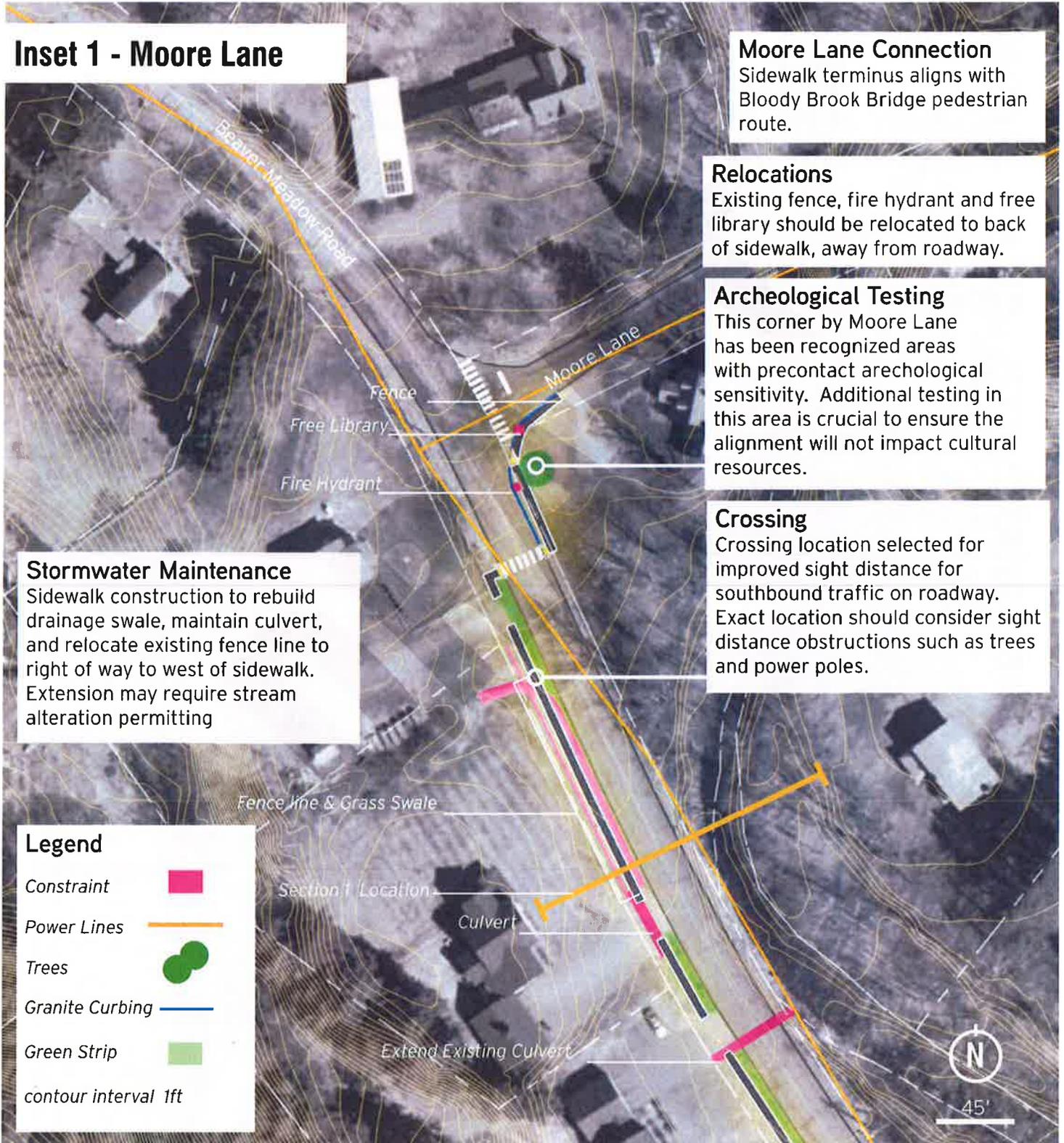
This selected alternative investigates the western alignment, detailing road crossing, curbing, and drainage details to provide a more detailed planning level cost estimate to the Town of Norwich. These findings will support and inform future decision making.

The inset areas outlined on this page are reviewed in further detail in the following pages.

### Legend

- Constraint 
- Power Lines 
- Trees 
- Sidewalk Curb 

## Inset 1 - Moore Lane



**Moore Lane Connection**  
Sidewalk terminus aligns with Bloody Brook Bridge pedestrian route.

**Relocations**  
Existing fence, fire hydrant and free library should be relocated to back of sidewalk, away from roadway.

**Archeological Testing**  
This corner by Moore Lane has been recognized areas with precontact archeological sensitivity. Additional testing in this area is crucial to ensure the alignment will not impact cultural resources.

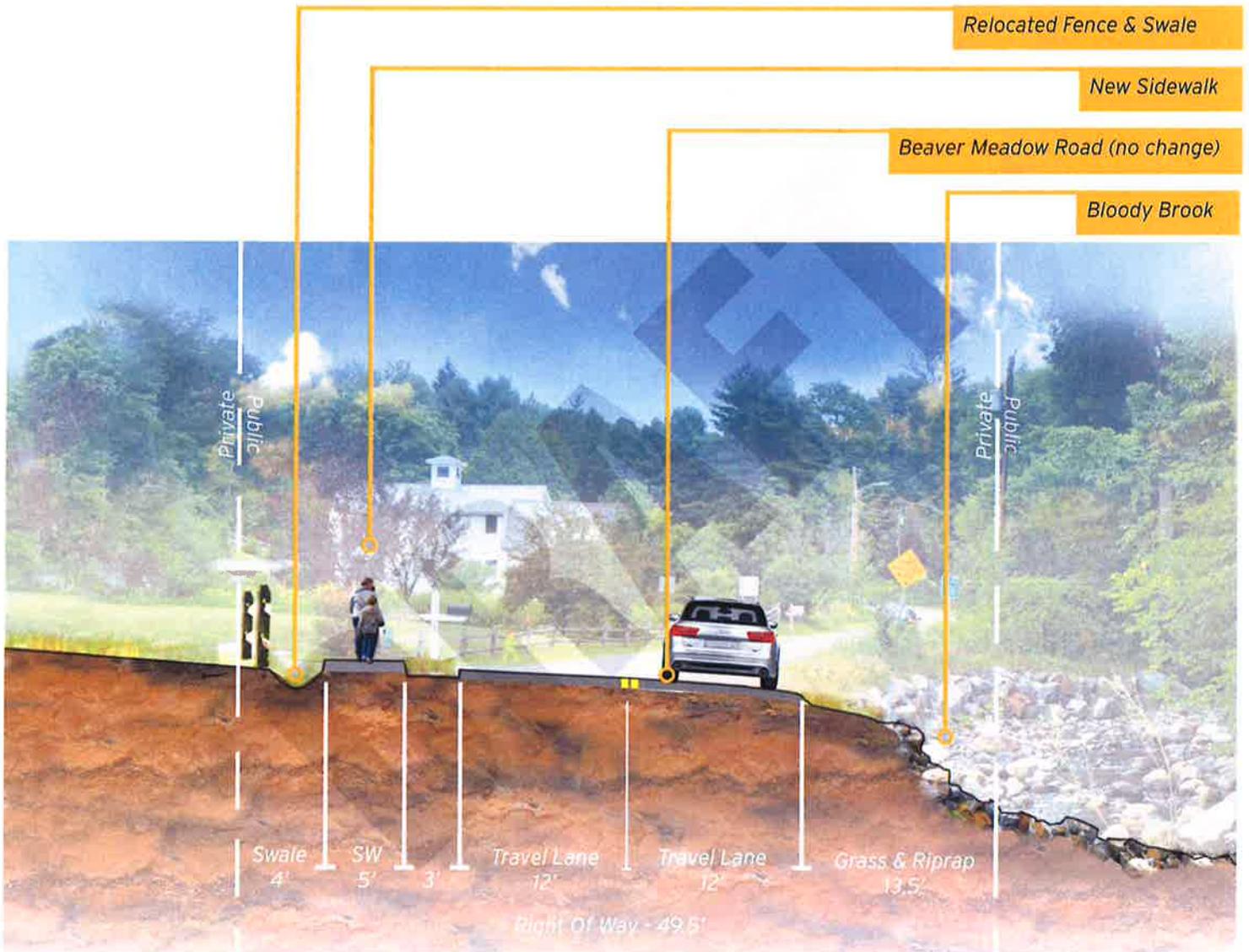
**Crossing**  
Crossing location selected for improved sight distance for southbound traffic on roadway. Exact location should consider sight distance obstructions such as trees and power poles.

**Stormwater Maintenance**  
Sidewalk construction to rebuild drainage swale, maintain culvert, and relocate existing fence line to right of way to west of sidewalk. Extension may require stream alteration permitting

**Legend**

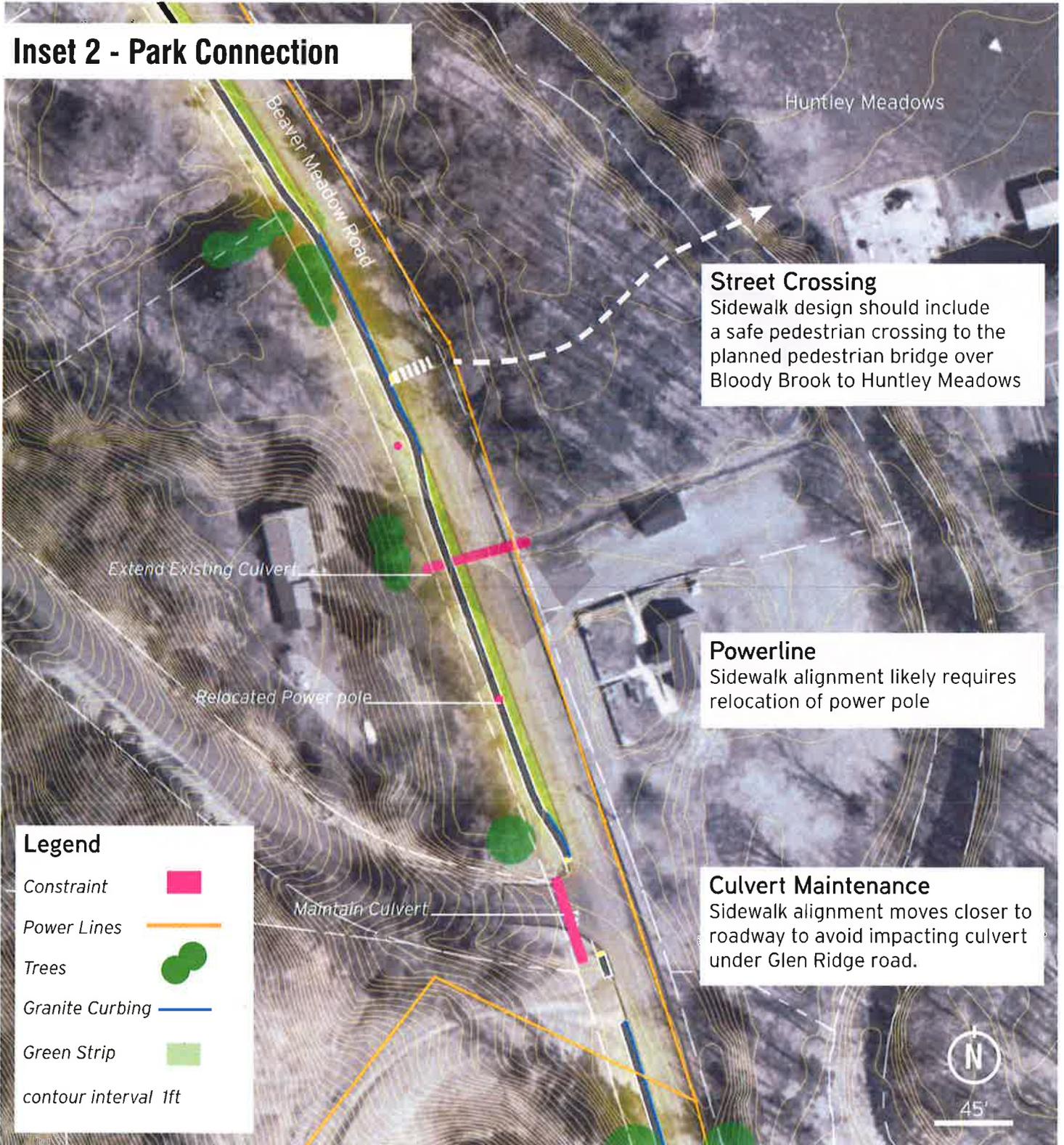
- Constraint 
- Power Lines 
- Trees 
- Granite Curbing 
- Green Strip 

contour interval 1ft

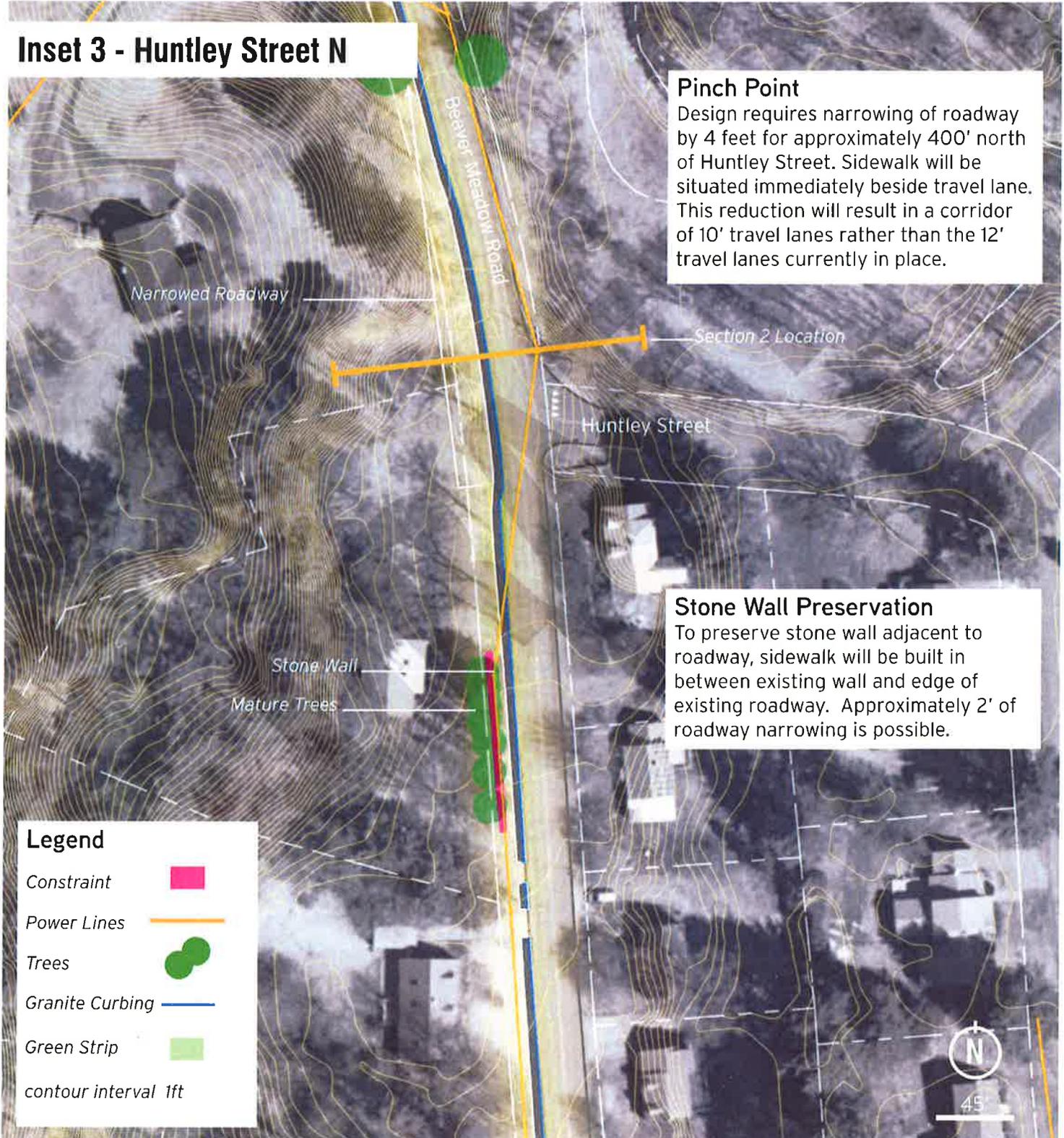


## Section 1 - Bloody Brook

## Inset 2 - Park Connection



### Inset 3 - Huntley Street N



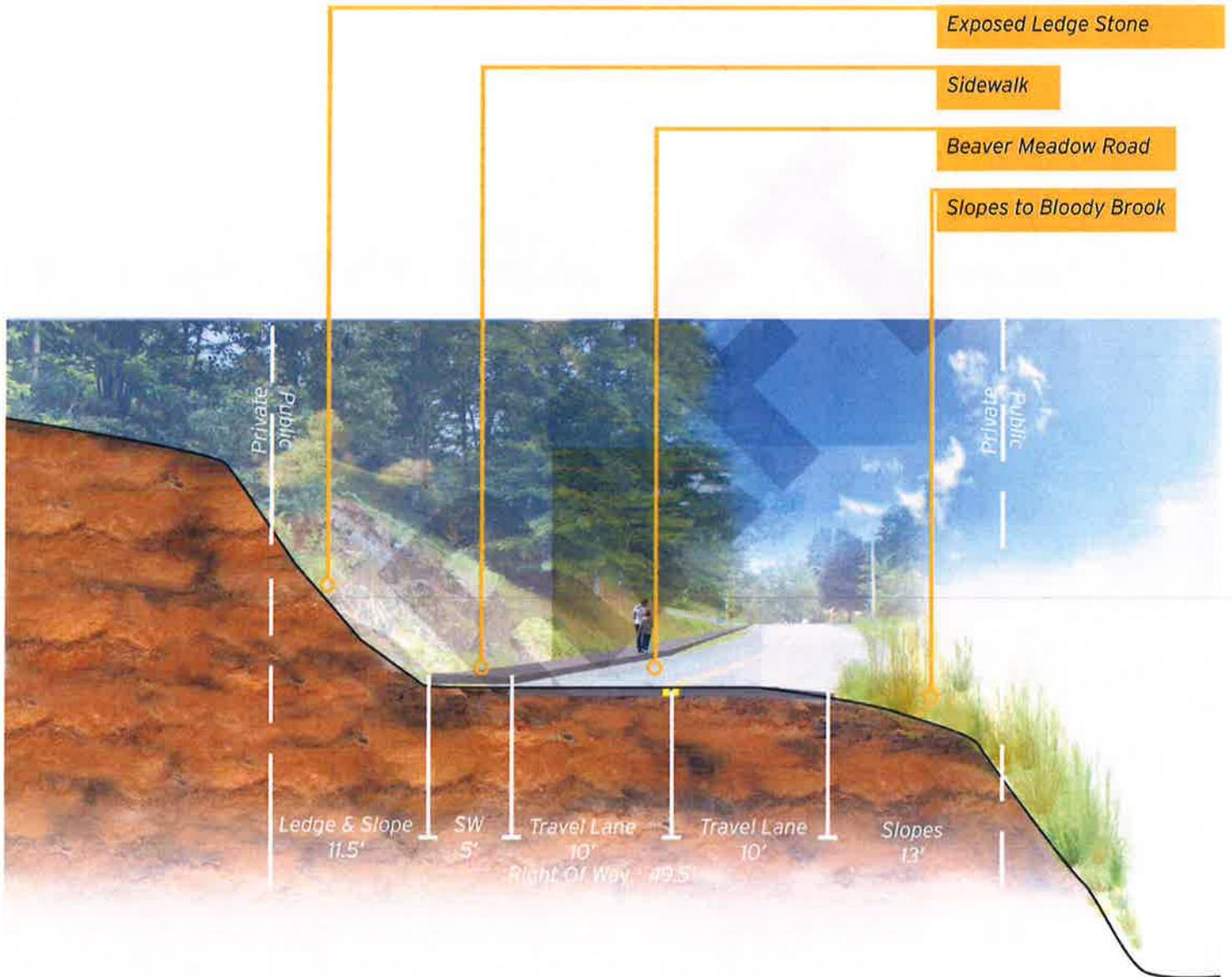
**Pinch Point**  
Design requires narrowing of roadway by 4 feet for approximately 400' north of Huntley Street. Sidewalk will be situated immediately beside travel lane. This reduction will result in a corridor of 10' travel lanes rather than the 12' travel lanes currently in place.

**Stone Wall Preservation**  
To preserve stone wall adjacent to roadway, sidewalk will be built in between existing wall and edge of existing roadway. Approximately 2' of roadway narrowing is possible.

**Legend**

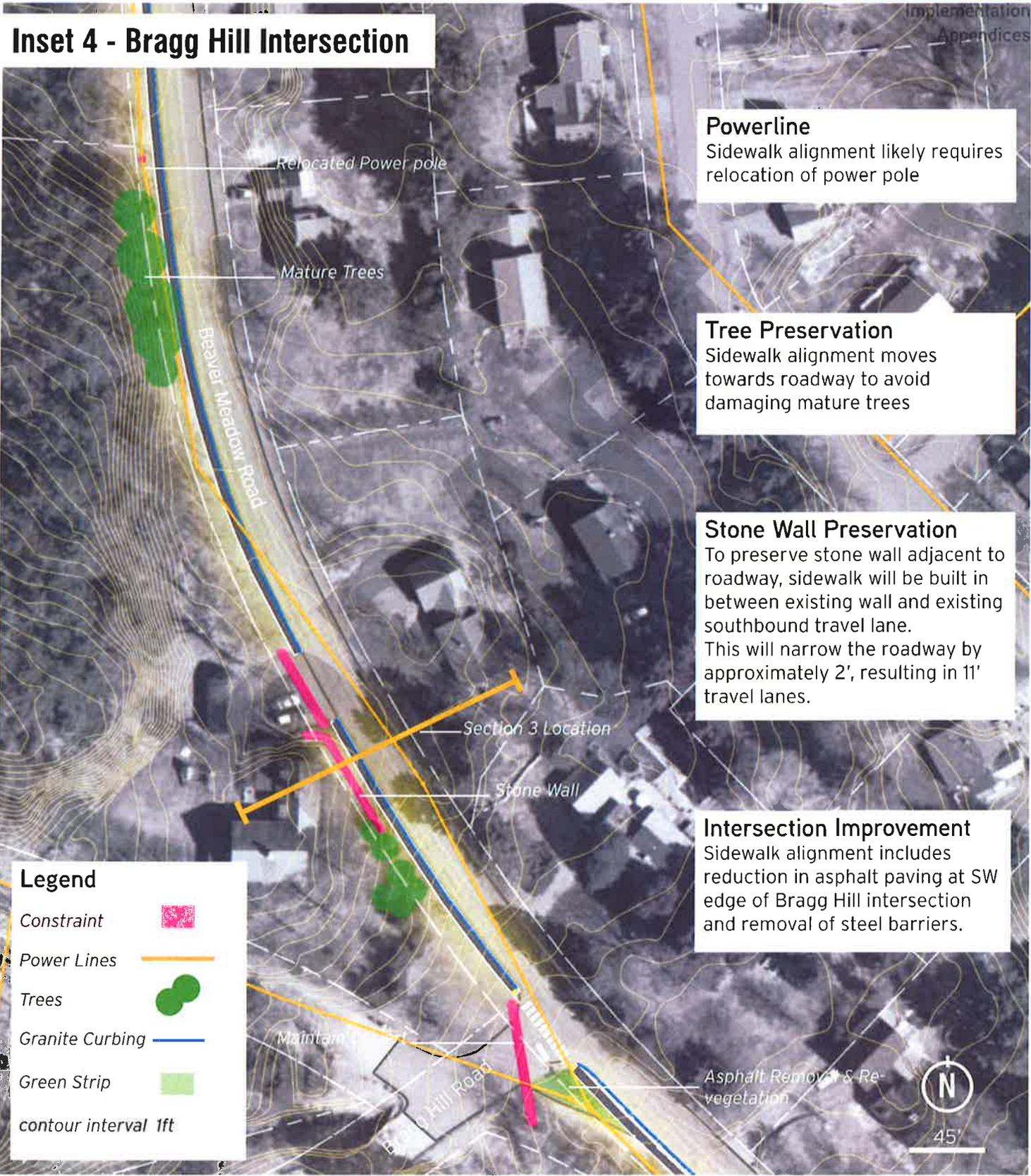
- Constraint 
- Power Lines 
- Trees 
- Granite Curbing 
- Green Strip 

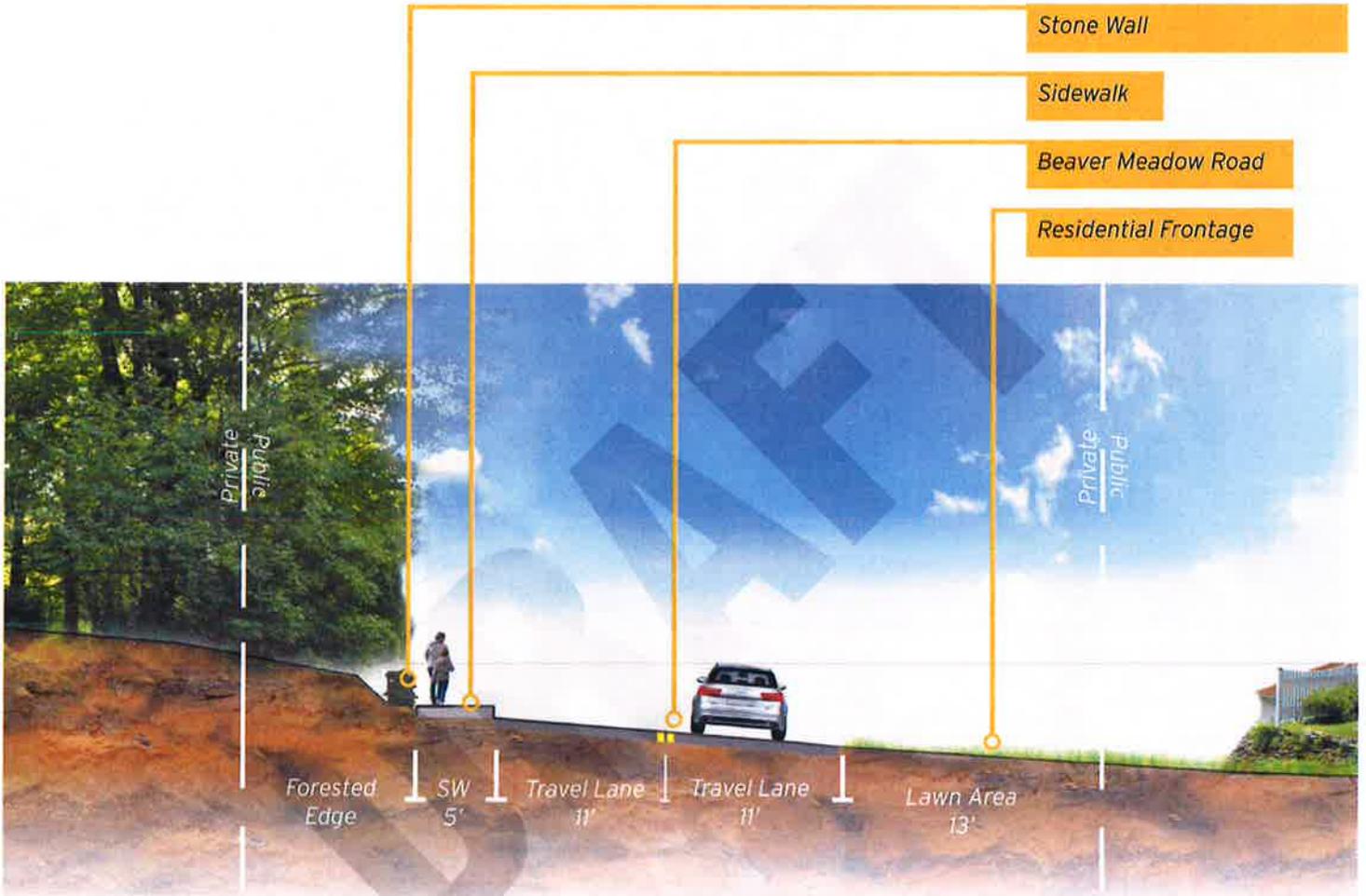
contour interval 1ft



## Section 2 - Huntley Street North

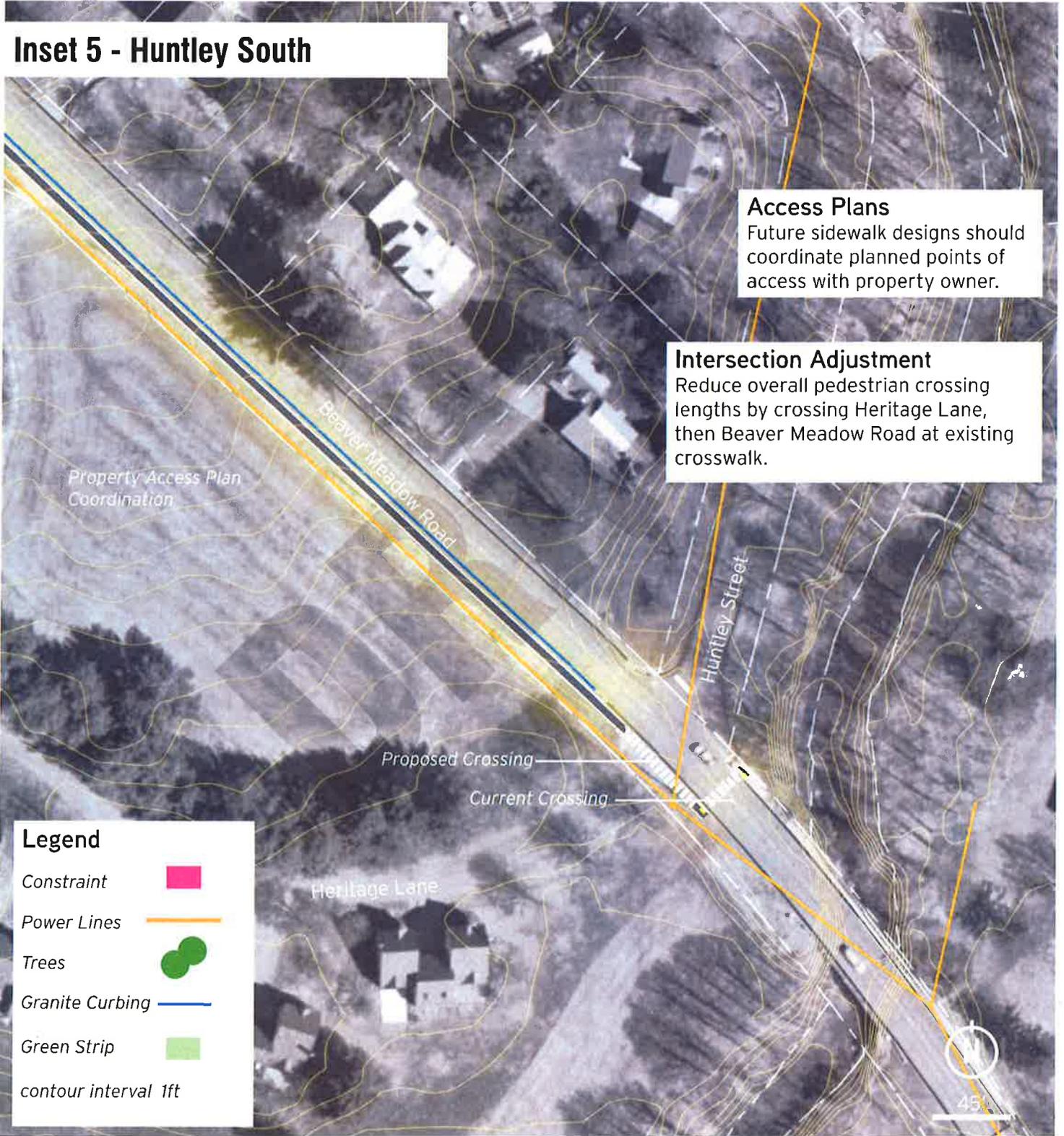
### Inset 4 - Bragg Hill Intersection





### Section 3 - Rock Wall

## Inset 5 - Huntley South



### Access Plans

Future sidewalk designs should coordinate planned points of access with property owner.

### Intersection Adjustment

Reduce overall pedestrian crossing lengths by crossing Heritage Lane, then Beaver Meadow Road at existing crosswalk.

## Preferred Alternative Design Considerations

### Cost

Based on updated planning-level design work and cost estimation based on VTrans 5 year construction cost averages, this alternative could cost up to \$975,500. This figure includes a 20% contingency for construction costs, as well as placeholder costs for requisite survey, design, project management and construction inspection.

This figure does not account for inflation and represents this project constructed with 2021 dollars. A detailed cost estimate can be found in the appendix of this document.

### General Design

This design proposes the construction of a 5' wide asphalt sidewalk located along the western edge of Beaver Meadow road. Where necessary, a 6" vertical granite curb is installed to separate the sidewalk from the western edge of Beaver Meadow Road.

### Stormwater Management

This design is intended to compliment existing stormwater management along Beaver Meadow Road through extension of existing culverts and ditches, and installation of additional culverts below driveways and along key segments of new sidewalk.

### Mid Block Crossing

The proposed mid block crossing of Beaver Meadow Road is designed to link to current parks projects that will provide pedestrian linkages to Huntley Meadows.

Though town highways such as Beaver Meadow Road are ultimately under Town Jurisdiction, VTrans provides clear guidance regarding mid block crossings. According to VTrans Pedestrian Crossing guidance (2019), the following criteria must be met for a mid block crossing:

- Speed Limit must be posted less than 40mph
  - » Beaver Meadow Road is set to 25mph
- There must be existing sidewalk to connect to
  - » There will be if this plan is implemented.

- There is not another crosswalk within 200 feet of crossing
  - » Per plan, the closest crosswalk is an additional 600' to the north.
- Adequate sight distance is available.
  - » Though this study does not constitute a specific study of sight distances, initial analysis in this area indicates that roadway and topography provides ample sight distance along the roadway at this crossing.

### Heritage Lane Crossing

This crossing is adjusted to move pedestrian traffic across Heritage Lane as opposed to Huntley Street. Through this adjustment, overall crossing lengths of Beaver Meadow Road and side streets can be reduced, and pedestrians are exposed to a Heritage Lane, a side street crossing with less use than Huntley Street.

### Selectboard Review

On January 7th, 2021, a draft of this report was provided to the Town of Norwich for preliminary review. The report was also provided to the public through a download link on the project website. On January 27th, 2021, an overview presentation of the report's findings was presented during a regular Norwich Selectboard Meeting.

The general consensus during this review was to move forward with implementation of the preferred alternative, and a motion was made to continue discussions regarding applications to a 2021 VTrans Bike/Ped program grant to fund survey and design work for the preferred alternative.

Additional interest was expressed by members of the selectboard and general public in favor of Alternative 4: Mobility Lanes. Alternative 4's high ranking in the evaluation matrix, and its support of bicycle riding on Beaver Meadow Road were identified as important considerations. Future design of the preferred alternative should explore how Alternative 1 could be best constructed while improving on-road bicycle travel to improve Beaver Meadow Road for all road users.

## Beyond the Scoping Study

This scoping study is intended to act as a springboard to support the Town of Norwich VT in designing, permitting, and funding a sidewalk construction project along Beaver Meadow Road.

This chapter outlines the general steps needed to take the plans, cost estimates, and diagrams contained in this report towards an investment in the safety and comfort of road users along Beaver Meadow Road.

- Step 0 - Find a champion
- Step 1 - Selectboard Approval
- Step 2 - Landowner Engagement
- Step 3 - Fundraising and Grant writing
- Step 4 - Survey, Design & Permitting
- Step 5 - Construction & Maintenance

### Step 0 - Find a Champion

City staff or engaged resident, every plan needs a champion. Human resources are needed to use this plan as a tool to communicate public sentiment, cost, and design intent of a future sidewalk project along Beaver Meadow Road. Ongoing conversations and projects are a part of bringing any infrastructure project to life, and a local champion plays an out-sized role in making sure projects such as a sidewalk can be approved, funded, and developed in a timely fashion. The regional planning commission may be able to offer some assistance to the Local Champion.

### Step 1 - Selectboard Approval

In order for a sidewalk improvement to be installed on Beaver Meadow Road, such a change to a public roadway must be approved by the Norwich Selectboard. This includes the physical change as well as the cost to the Town of providing matching funds for such a project. For projects of this scale, it is anticipated that the matching cost to the Town of Norwich would be approximately 20% of total project costs.

### Step 2 - Landowner Engagement

Norwich, and the Beaver Meadow Road neighborhoods are highly engaged communities. Nearly 13% of the 3318 recorded Norwich residents participated in the local concerns survey online (n=422). Should this

project move forward with the support of the Norwich Selectboard, it will be important for a project champion to circulate this study among affected landowners, including both those who own property abutting the project right of way, and answer questions about the design intent, potential impacts, estimated timelines to construction, and right of way boundaries. These crucial conversations can also reveal any needed changes to the design as proposed in this document, such as easements or driveway access modifications that are best understood well before the construction process.

### Step 3 - Fundraising & Grant writing

Fundraising efforts for a community sidewalk are likely to center around supporting public investment as matching funds to grants that will fund the final design and construction of such a facility.

Should cost become a defining issue, this project can be phased, either in two distinct phases of design and construction, or funding survey and full construction documents as first phase, and leveraging those efforts and refined cost estimates to fund construction as a second phase. A table is included in this chapter which outlines some of the common funding resources for Vermont Towns seeking to develop bicycle and pedestrian facilities such as a community sidewalk.

### Step 4 - Survey, Design & Permitting

Once selectboard approval is in place, local landowners have been consulted, and grant awards in hand, the Town can then move towards contracting an engineering firm to conduct a survey, develop construction documents.

This project should require a categorical exclusion permit that illustrates the lack of significant impact to the project area. Most notable impact will be construction within the designated river corridor surrounding bloody brook. Staff of the Department of Environmental Conservation's Rivers Program should be involved early in the implementation process.

### Step 5 - Construction & Maintenance

Construction is the final step towards a new facility. As the community plans towards this goal, long term (25 year) maintenance and repair, as well as winter maintenance should also be considered.

## Grant Resource Table

Grant Title	What does it fund?	Maximum Grant Amount	Match Req	Federal Funding	Grant Contact
<a href="#">CDBG - Planning Grants</a>	Feasibility studies, marketing plans, engineering and architectural plans, etc	\$60,000	10 percent	x	Julia Connell julia.connell@vermont.gov
<a href="#">VTrans - Transportation Alternatives Program (TAP)</a>	Construction, planning and design of on and off roadway facilities for active transportation facilities	\$300,000	20 percent	x	Scott Robertson - mailto:scott.robertson@vermont.gov 802-793-2395
<a href="#">VTrans - Bicycle and Pedestrian Program Grants</a>	Construction, planning and design of on and off roadway facilities for active transportation facilities	\$1,000,000	20 percent	x	Jon Kaplan - jon.kaplan@vermont.gov 802.498.4742
<a href="#">VTrans - Bicycle and Pedestrian Program Grants - Small Scale</a>	Distinguished from Bike/Ped program by smaller maximum funding amount and lack of federal requirements	\$100,000	50 percent		Jon Kaplan - jon.kaplan@vermont.gov 802.498.4742

# Appendices

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## **Planning Level Cost Estimate**

This planning level cost estimate is conceptual in nature, and additional design, survey, and detailed estimation is needed to refine these numbers prior to construction.

This cost estimate provides a starting point for Town budgeting and planning should the Selectboard approve the pursuit of a Beaver Meadow Sidewalk as outlined in this plan.

This cost estimate utilizes VTrans 5 Year Average Price lists which can be accessed on the VTrans website:

<https://vtrans.vermont.gov/cost-estimating>

Assuming the project is built in 2025, with grant funding requiring a 20% match from the Town of Norwich, the total project would cost \$1,080,000 with a \$216,000 estimated match required from Norwich.

This is a conservative estimate based on our understanding of the existing conditions. Some costs may be reduced, especially if this sidewalk project is included as part of a larger road reconstruction project.

<b>SIDEWALK ON BEAVER MEADOW ROAD - WEST SIDE</b>					
<b>ITEM NO.</b>	<b>DESCRIPTION</b>	<b>UNIT</b>	<b>QUANT.</b>	<b>UNIT PRICE</b>	<b>AMOUNT</b>
201.11	CLEARING AND GRUBBING	A	0.40	\$33,793.48	\$13,517.39
203.15	COMMON EXCAVATION	CY	850	\$12.35	\$10,497.50
203.16	SOLID ROCK EXCAVATION	CY	20	\$40.68	\$813.60
203.30	EARTH BORROW	CY	0	\$13.03	\$0.00
203.32	GRANULAR BORROW	CY	65	\$20.32	\$1,320.80
204.25	STRUCTURE EXCAVATION	CY	60	\$23.82	\$1,429.20
301.25	CRUSHED GRAVEL	CY	600	\$40.21	\$24,126.00
406.25	MARSHALL BITUMINOUS CONCRETE PAVEMENT	T	150	\$180.67	\$27,100.50
404.65	EMULSIFIED ASPHALT	CWT	2	\$22.00	\$44.00
613.15	RIPRAP HEAVY TYPE	CY	50	\$63.13	\$3,156.50
601.26	18" CPEP(SL)	LF	90	\$66.10	\$5,949.00
618.15	BITUMINOUS CONCRETE SIDEWALK 5"	T	590	\$367.12	\$216,600.80
616.21	VERTICAL GRANITE CURB	LF	2150	\$50.03	\$107,564.50
608.54	DETECTABLE WARNING SURFACE	SF	90	\$45.10	\$4,059.00
675.34	SQUARE TUBE SIGN POST AND ANCHOR	LB	30	\$20.00	\$600.00
675.20	TRAFFIC SIGNS TYPE A	SF	6	\$12.85	\$77.10
630.15	FLAGGERS	HR	480	\$30.88	\$14,822.40
641.10	TRAFFIC CONTROL	U	1	\$40,000.00	\$40,000.00
641.15	PORTABLE CHANGEABLE MESSAGE SIGN	U	2	\$4,300.16	\$8,600.32
651.35	TOPSOIL	CY	100	\$35.46	\$3,546.00
651.15	SEED	LB	30	\$8.73	\$261.90
651.25	HAY MULCH	T	0	\$857.35	\$342.94
651.18	FERTILIZER	LB	95	\$3.83	\$363.85
651.20	AGRICULTURAL LIMESTONE	T	0	\$575.41	\$230.16
635.11	MOBILIZATION/DEMOBILIZATION	U	1	\$40,000.00	\$40,000.00
653.01	EROSION PREVENTION AND SEDIMENT CONTROL PLAN	U	1	\$45,000.00	\$45,000.00
					Running Total
					\$570,023.47
					Contingency (20%)
					\$115,000.00
					<b>CONSTRUCTION COST</b>
					<b>\$686,000.00</b>
					<b>OTHER EXPENSES</b>
					Relocation 2 utility poles
					\$10,000.00
629.29					Relocation of 1 Fire Hydrant
					\$3,483.38
					<b>SOFT COSTS</b>
					Local project management(10%)
					\$69,000.00
					Design fee (15%)
					\$103,000.00
					Construction inspection fee (10%)
					\$69,000.00
					<b>Grand Total (2020 dollars)</b>
					<b>\$940,483.38</b>
					Assumed Inflation Rate
					2.8%
					<b>Future Total (2025 dollars)</b>
					<b>\$1,080,000.00</b>

## Public Engagement Record

This appendix element contains records of the two public meetings and content of related online surveys that were held as part of this scoping study. Due to the overwhelming number of responses to the surveys, those results are not included in this document, but were provided as a database to Norwich Town administrators.

### Local Concerns Meeting - 9.16.2020

6:30pm via Zoom Video-conference

#### Attendees

Project Team  
Herb Durfee, Town Manager  
Larry Wiggins, Public Works  
Dayton Crites, D&K  
Julia Ursaki, D&K  
Chris Sargent, D&K

38 members of the public

#### Project Introduction & Overview

Dayton Crites from DuBois & King provides an overview of the project so far and what a scoping study is. A scoping study is the first portion of planning. This study will give the Town the tools they need to decide to build a sidewalk or not. We are at the first stage of this project.

#### Initial Questions

Why did we just find out about the study if it's already started? Should residents have been notified that the Town was pursuing the grant?

*The decision to apply for the grant was made by the Selectboard during two different meetings. Those meetings are public and minutes where this was discussed are available to the public.*

#### Review of Public Input

- Approximately 350 survey responses
- Survey will be open September 1 - September 30
- General summary of the online survey results so far
- Most people use Beaver Meadow Road for a variety of reasons: both for transportation and recreation, to walk, bike, drive, run, and more.

- Most people believe that safety is a concern on Beaver Meadow Road, either throughout the road or in certain areas.
- So far, most people (68.2%) of people support constructing a sidewalk.
- Question - how can people without the Internet share their input if they can't fill out the survey?

*Get in touch with Town Manager's Office (Herb) and he will make sure your comments get to with D&K*

#### Breakout Sessions

The meeting was then divided into 3 rooms to facilitate roundtable conversations, and allow members to contribute their thoughts while still respecting the rough time limit of a 1 hour evening meeting, and create a more intimate feel among these groups. The breakout rooms were facilitated by staff from the Town and Dubois & King.

#### All Comments

Each bullet point represents one person's comments.

#### Summary of Breakout Room Themes

The main themes that emerged from public comment during the breakout sessions include:

- The loop is a popular walking route, especially for the many families in this neighborhood.
- Safety for all should be a main priority on this road.
- Building a sidewalk will be too expensive.
- People feel unsafe because of vehicles, and especially trucks, that drive fast on this road.
- Phasing construction of the sidewalk would ease the financial burden by spreading the cost out over time.
- Sight distance limitations throughout the road make it feel unsafe while using this road, be it driving, walking, bicycling, etc.
- Many sidewalks in Norwich are in disrepair, the Town should fix those before building more that they can't maintain.

## Beaver Room Session

- Excited for this project. Uses the roadway regularly, with small children. Supports a sidewalk.
- Lives by the northern corner of roadway near trailhead access. Corner speeds and sight lines are dangerous. Should be resolved.
- Uses the roadway all the time, to access bus rides, with kids. Supports sidewalk development. Suggests development of a comprehensive sidewalk plan that would allow town to bite off sidewalk construction in manageable chunks.
- Lives further out of town, but often utilizes the roadway. Ambivalent on installation of a sidewalk. Supports a phased process to respect budgetary limits, and has concerns about introducing new risks with designated crossings, if the alternatives point to that.
- Lives along main street, uses the roadway frequently. Here to listen into the public responses and gauge community concerns.
- Has children and supports sidewalk this project strongly. Does not understand those putting fiscal limits ahead of safety concerns.
- Not supportive or unsupportive, supports phased installation.
- Lives on the road near Ballard trailhead. Moved to location in hopes of walkability, but still will not let children walk on the roadway. Has gravel 'walkway' adjacent to property, is in constant need of repair. Against that material for a solution, as it does not do well for strollers or kids bikes. Raises concerns about a sidewalk and curb along roadway 'pinching' cyclists into a narrow auto-oriented lane.
- Supports development of sidewalk project in this area, as safety should be paramount
- Has cost concerns about the roadway, but recognizes safety issues throughout the area. Of particular note is roadway sight lines adjacent to Huntley Road. As a road user, they often detours through Huntley subdivision to avoid this portion of Beaver Meadow Road.

## Meadow Room Session

- Not in support of the project. Worried about maintenance. Does not want a poorly maintained sidewalk on their property.
- Supports the sidewalk. The number of people on the road who use it warrants a sidewalk. The design of the road does not meet the needs of people walking, biking. Understands fiscal concerns. Has young kids, a sidewalk allows family to walk to Town, kids can get to school, access pizza oven and other community amenities.
- Family bikes and walks and drives Beaver Meadow every day. Would love to see a sidewalk. Biggest concern is the neighbors, and people whose yards are going to be affected-wants to make sure their needs are met. Worried about widening the roadway (or having a widened shoulder instead of sidewalk) which could make trucks and other vehicles drive faster.
- Wants to connect the loop! Think about this in a larger context- safety, environmental, etc benefits of a sidewalk. Currently, some sidewalks are owned by fire dept, some are owned by Town. May eventually merge.
- Runner, walker, parent, biker. Supports constructing a sidewalk. Cars haven't slowed down on other roads after sidewalks were built - don't expect that on this road, but, a sidewalk would still give people a safe place to walk.
- Has two young children, and the entire family loves to walk the loop. Likes to see foot traffic, provides a sense of community in this neighborhood. Children independence is a big pro of building a sidewalk- they wouldn't feel comfortable sending their kid to walk to school on the road as it is.
- Big supporter of this project. Traffic and trucks especially move fast - not acceptable. No price tag on a fatality or injury. Health, environmental, safety benefits of a sidewalk outweigh the costs.
- Was hit by a car in Norwich in 1982 but doesn't think safety for pedestrians is a big issue given the economic climate. There are a variety of cost concerns: what about the septic systems? Water lines? High Cost. Look at everybody's leechfields.
- There are other priorities in the Town that we should be spending this money on - for example, the septic system at elementary school is a bigger health & safety issue for children than a sidewalk.

## Appendices

### Sidewalk Room

- In favor of the sidewalk; but, has a home office (psychotherapy and massage) that existing parking area likely would “compete” with a new sidewalk.
- Great idea; likes the public input process that is involved with the scoping (survey, forums, etc.)
- Though she does not live on BMR, she uses BMR to walk/exercise. She supports the sidewalk. As a mother, she would like to see the “loop” concept completed for non-vehicular safety issues.
- Supports the sidewalk; the stretch of roadway is dangerous for pedestrian, et al and, thus, the Town needs the sidewalk.
- speed and safety are clearly a concern. Wondering if law enforcement were stepped up would we really need the sidewalk. Believes some other measures might be useful (e.g., strategically placed crosswalks with proper signage, speed control, and other traffic “calming” techniques). There needs to be a better overall vision for the corridor. He knows of two vehicular accidents in the area - one was due to texting and driving and the other was due to speeding. He is sensitive to the cost of such a project and, thus, why he suggests there may be other methods of traffic calming that could be implemented, possibly, instead of a sidewalk.
- indicated that the sidewalk would be helpful to have for the reasons already expressed, and is supportive.
- has witnessed many walkers, bikers, strollers, and there is a lot of traffic, especially trucks (including logging and dump trucks). Is in favor of the sidewalk.
- knows that bicycles are not supposed to travel on sidewalks, so wants to make sure that whatever may be accomplished is sensitive to the multi-modal aspect of the “sidewalk”.
- perhaps in thinking about multi-modal uses and traffic calming techniques, the actual vehicular travel lanes could be reduced in width to help slow traffic and provide room for multi-modal path/lane.
- supports the sidewalk. There are a lot of kids that use the corridor. He has personal concern with letting them walk/bike around the area, due to traffic/safety

### Additional Comments

- Is there a process for informing D&K of neighborhood issues? Yes - email beavermeadowsidewalk@dubois-king.com

- How far will the sidewalk go into people's yards? ROW identification and issues will be ironed out during the alternatives phase.
- Next Steps
- D&K will post the meeting notes and Zoom recording meeting on the project website.
- The online survey will be closed on September 30, 2020.
- D&K will continue their existing conditions analysis, summarize survey results, develop a Purpose & Need Statement, and then begin developing alternatives.
- The next opportunity to provide public feedback will be at the Alternatives Presentation (date is TBD).

### Email Feedback

A project email of beavermeadowsidewalk@dubois-king.com was also utilized to collect public comment throughout the study period. 19 emails were provided individually to the Town Administrator and are summarized in the public engagement summary within the existing conditions chapter of this document.

### Local Concerns Survey Content

A total of 422 responses were recorded on this survey, and responses were provided in excel format to the Norwich Town Administrator as part of this overall scoping study process. Content of this survey is provided within the Existing Conditions chapter of this document.

### Alternatives Presentation

11/12/2020 at 6:30pm via Zoom Video Conference

### Attendance

Project Team  
Dayton Crites, D&K  
Julia Ursaki, D&K  
Herb Durfee, Town of Norwich

25 members of the public

## Alternatives Overview

DCrites provided detailed walk-through of four alternatives.

Breakout into 3 rooms (Beaver facilitated by Julia Ursaki, Meadow - Facilitated by Dayton Crites and Road Facilitated by Herb Durfee)

Individuals comments are bullet pointed.

### Beaver Room

- Friend was killing by a bicycle, people sharing lanes (walking and bicycling) is dangerous & we need more accurate ROW data
- Why don't you use Huntley Road instead for that stretch?
- Walks to school every day with her daughter. No opportunity to walk on Huntley for some people. Need full sidewalk along BMR. She lives on West side of the road, is in favor of a sidewalk on the west side.
- Wants everybody to be happy - sidewalk on the west would effect fewer neighbors who have houses near the road etc
- Truck route, popular route for everybody - trucks, walkers, bicyclists,
- Likes sidewalk on west side to not effect eastern side neighbors
- Trucks speed up as they hit the flat part - can you cross at the legion? Moore Ln crosswalk is dangerous
- Sidewalk should be on the west side of the road.
- Would like a sidewalk
- What are Norwich's priorities?
- Thinks septic system should be addressed first, any further expenses should be halted
- Preference for speed feedback radar sign, better signage
- Consider Crosswalk at American Legion, sidewalk to the west going south, to the east going north

### Meadow Room

- Huntley Street Detour supported. Concern about crossing where recommended.
- Need for speed control.
- Lives on east side of BMR.
- Very concerned about encroachments into private property. Particularly concerned w/ potential encroachment in her yard.

- Prefers west side alignment.
- Need for precise information regarding placement as soon as possible.
- Extremely concerned with impact to property.
- Agrees with safety concern because she has a 9 year old.
- Maybe there's no accident record because everyone avoids that road.
- Huntley would be a good bypass. Would be great to see cost savings for that alternative. Would be interesting to see a partial access to Pizza Park. along west side of roadway
- How much does paving cost in comparison to a sidewalk?
- Reminds group this is a project to address safety. Not only cost.
- Do Costs include survey? Does cost include relocation of utilities? Does that include maintenance?
- Hillside Cemetery resident, doesn't feel safe with child and dog. Doesn't feel compromise proposal with sidewalk only to Bragg Hill gets us much. Potential support for Huntley bypass.
- Concerns regarding maintenance and ongoing costs, including personnel costs.

### Road Room

This rooms record of notes can be accessed through the Norwich Town Administration.

### Additional Comments

Additional input was gathered upon resuming the full meeting with all members in attendance. These comments surrounded discussion of value of road-wide traffic calming techniques, concerns about ROW identification and sidewalk impact on private parcels. Some discussion was had regarding process of the Town to move forward with a selected alternative and how the related funding sources are authorized by the Town selectboard. Next steps in overall scoping study process were discussed.

## Cultural Report Placeholder



**HARTGEN**

archeological associates inc

## **ARCHEOLOGICAL RESOURCE ASSESSMENT**

### **Bicycle and Pedestrian Connectivity Scoping Study**

Town of Norwich, Windsor County, Vermont

HAA # 5552.11

**Submitted to:**

Dayton Crites, AICP  
Senior Planner  
DuBois & King Inc.  
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Randolph, VT 05060  
Phone: (802) 431-1465 Cell: (415) 734-7243  
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**Prepared by:**

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January 2021

## **MANAGEMENT SUMMARY**

SHPO Project Review Number:

Involved State and Federal Agencies: Vermont Agency of Transportation (VTrans)

Phase of Survey: Archeological Resource Assessment

## **LOCATION INFORMATION**

Municipality: Town of Norwich

County: Windsor County, Vermont

## **SURVEY AREA**

Length: Approximately 1/3 mile (0.4 km)

Width: Approximately 20 feet (6 m)

## **RESULTS OF RESEARCH**

Precontact Archeological sites within one mile: *None*

Historic Archeological sites within one mile: *None*

NR/NRE districts in or adjacent: *None*

Precontact Sensitivity: *Low to High*. Two areas of precontact archeological sensitivity were identified adjacent to Bloody Brook.

Historic Sensitivity: *Low*

## **RECOMMENDATIONS**

Two areas of precontact archeological sensitivity were identified adjacent to Bloody Brook, located at the eastern and western ends of the project alignment. Phase IB testing is proposed in these two areas.

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Report Authors: Elise Manning-Sterling

Date of Report: *January 2021*

## ARCHEOLOGICAL INVESTIGATION

### INTRODUCTION AND CURRENT CONDITIONS

Hartgen Archeological Associates, Inc. was retained to conduct an archeological and historical assessment for the proposed scoping study to identify issues with construction of a pedestrian/bicycle facility on Beaver Meadow Road in the Town of Norwich, Windsor County Vermont (Map 1). The project is being funded in part by the Federal Highway Administration and the Town of Norwich, Vermont, through the Vermont Agency of Transportation (VTTrans) Municipal Assistance Bureau (MAB). The pedestrian/bicycle facility shall, at a minimum, be a sidewalk which extends from the existing sidewalk on Beaver Meadow Road (near the intersection of Huntley Ave.) to the intersection of Moore Road and Beaver Meadow Road (Map 2). The project requires approval by Trans, and the archaeology and historic preservation reports will be reviewed by VTTrans archeology and historic preservation officers.

### Environmental Overview

Environmental characteristics of an area are significant for determining the sensitivity for archeological resources. Precontact and historic groups often favored level, well-drained locations near wetlands and waterways. Therefore, topography, proximity to wetlands, and soils are examined to determine if there are landforms in the project area that are more likely to contain archeological resources. In addition, bedrock formations or other lithic sources may contain resources that may have been quarried by precontact groups. Other locations can also be special purpose sacred and traditional use sites. Soil conditions can provide a clue to past climatic conditions, as well as changes in local hydrology.

The Area of Potential Effects (APE) is located on a level to gently sloping terrace landform located at an approximate elevation of 540 feet above mean sea level within the Bloody Brook river valley. Within this valley, there are a number of small to medium-sized streams that flow into Bloody Brook. To the south of Beaver Meadow Road, the land slopes upward, rising to heights over 1,000 feet amsl approximately one mile distant.

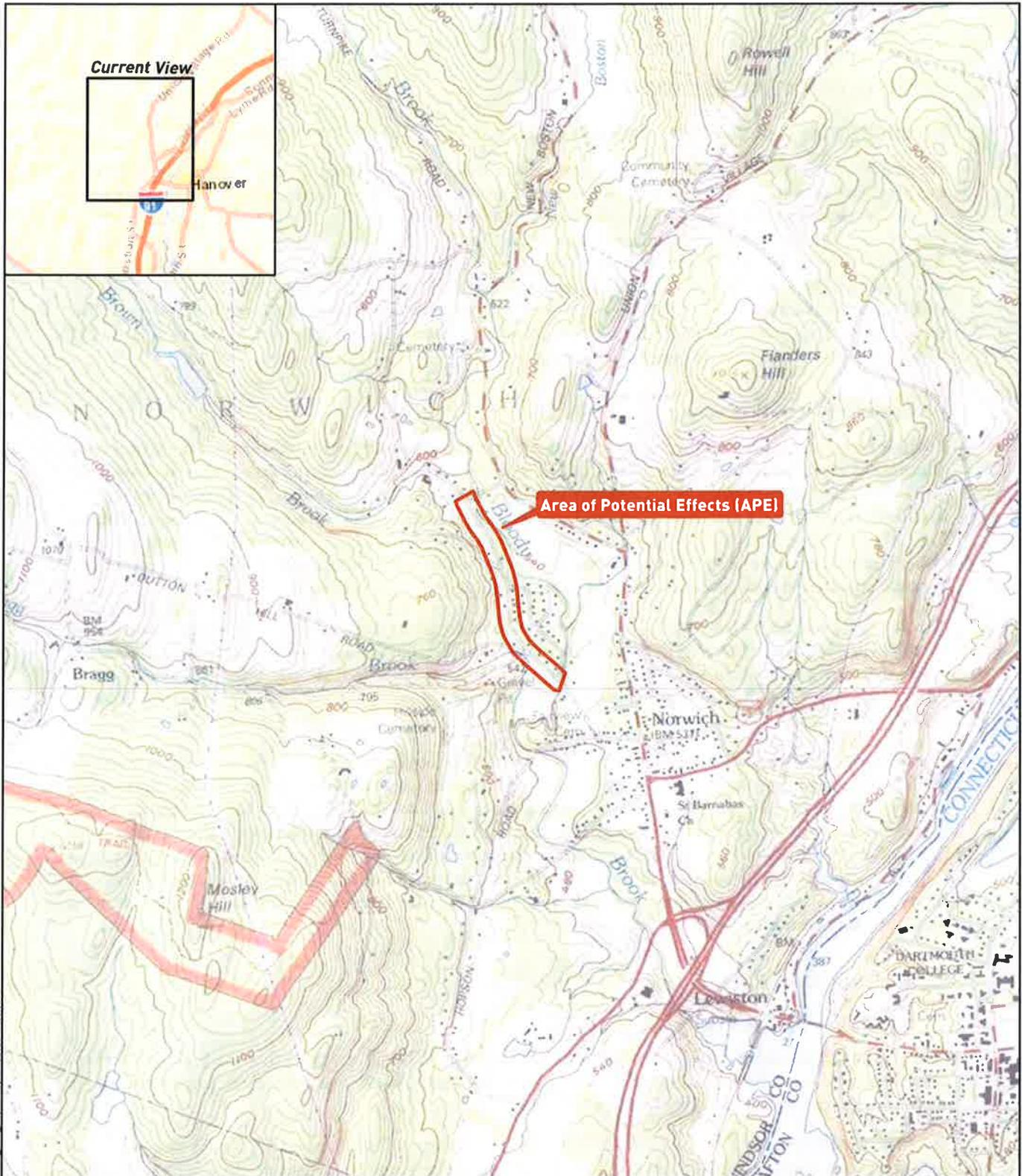
The Bloody Brook meanders through the wide valley to the north, situated at varying distances from the project area, ranging from directly adjacent, to several hundred feet distant. A few hundred feet to the northwest of the project area near Moore Road, Brown Brook flows into Bloody Brook. The confluence of Bloody Brook with Bragg Brook is located approximately 1,000 feet to the southeast. The confluence of Bloody Brook with a small unnamed stream is located approximately 900 feet north of the central portion of the APE. Approximately one mile to the southeast is the confluence of Bloody Brook with the Connecticut River. There is one soil type present within the project area on both sides of Beaver Meadow Road – Windsor loamy sand 8-15% (USDA 2021).

### BACKGROUND RESEARCH

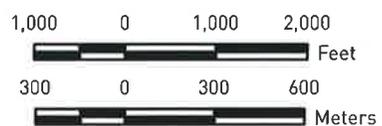
#### Historic Site File and Map Research

Research at the Vermont Division for Historic Preservation (VDHP) revealed that there are no historic archeological sites located within or adjacent to the project area. Within one mile of the project area, there are four recorded historic archeological sites. These include:

*The Montshire Museum Mill Site – VT-WN-0493.* This 19<sup>th</sup>-century mill was located on Bloody Brook near its confluence with the Connecticut River, at the present site of the Montshire Museum. An informational sign at the museum indicates that a grist mill was established at this locale in 1766. The mill continued operation through the 19<sup>th</sup>-century, as shown on 1856 and 1869 maps, and into the 20<sup>th</sup> century as a door and window sash manufactory (Online Resource Center VDHP).



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Note: Contour interval is 20 feet.

Project Location

GIS Services Accessed 9/17/2020:  
 Environmental Systems Research  
 Institute, Inc., World Street Map;  
 USGS The National Map

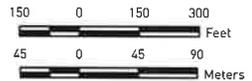


**HARTGEN**  
 archeological associates inc

**Map 1**



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**Legend**

- Precontact Sensitivity Area
- Area of Potential Effects (APE)

Project Map **HARTGEN**  
archaeological associates, inc.  
 Vermont Center for Geographic Information, Orthoimagery, 2016-2020  
**Map 2**

*Gleason Site – VT-WN-0237.* This historic site contained concrete and dry-laid stone on the edge of the Connecticut River.

*Lewis Site – VT-WN-238.* This historic site contained subsurface structural remains and features located adjacent to the Connecticut River.

*Dartmouth Pottery – VT-WN-0239.* This mill site, located on the Connecticut River, contained subsurface historic deposits and a standing brick structure.

There were no NR listed (NRL) properties, no NR eligible (NRE), and no properties previously determined to be ineligible for the NR. Within the project area, there is one State Register listed property – the circa 1780 Burton-Ballard House located at 149 Beaver Meadow Road.

A review of 19<sup>th</sup>-century historic maps of the project area was conducted to attain an overview of the changing historical and environmental landscape within the project area. This review includes the study of historic structures that may be or may no longer be extant, alterations to road and rail systems, and changes in stream and river courses.

The Historic Resources Identification report for this project noted that there are two resources (structures) that were constructed prior to 1856 (Hartgen 2020). The Burton-Ballard House located at 149 Beaver Meadow Road, was built c. 1780 (Photo 1). In the 1850s, this was the home of Alba and Phebe Stimpson, listed as farmers in the 1850s census (United States Census Bureau 1850, in Hartgen 2020).



**Photo 1.** Photo shows the circa 1780 Burton-Ballard House located at 149 Beaver Meadow Road. View is to the west.

The Samuel and Sophia Wright House, located at 288 Beaver Meadow Road, was constructed c. 1815 and was the home of Samuel Wright during the mid-19<sup>th</sup> century (Photo 2). The 1856 Chace and 1869 Beers maps show no other structures located along this section of Beaver Meadow Road (Map 3).

Historic features representative of domestic activities were typically located in the back yards of residences, rather than situated in front of the houses along the main road. Therefore, the proposed project alignment is considered to have a low sensitivity for the presence of significant historic archeological resources.



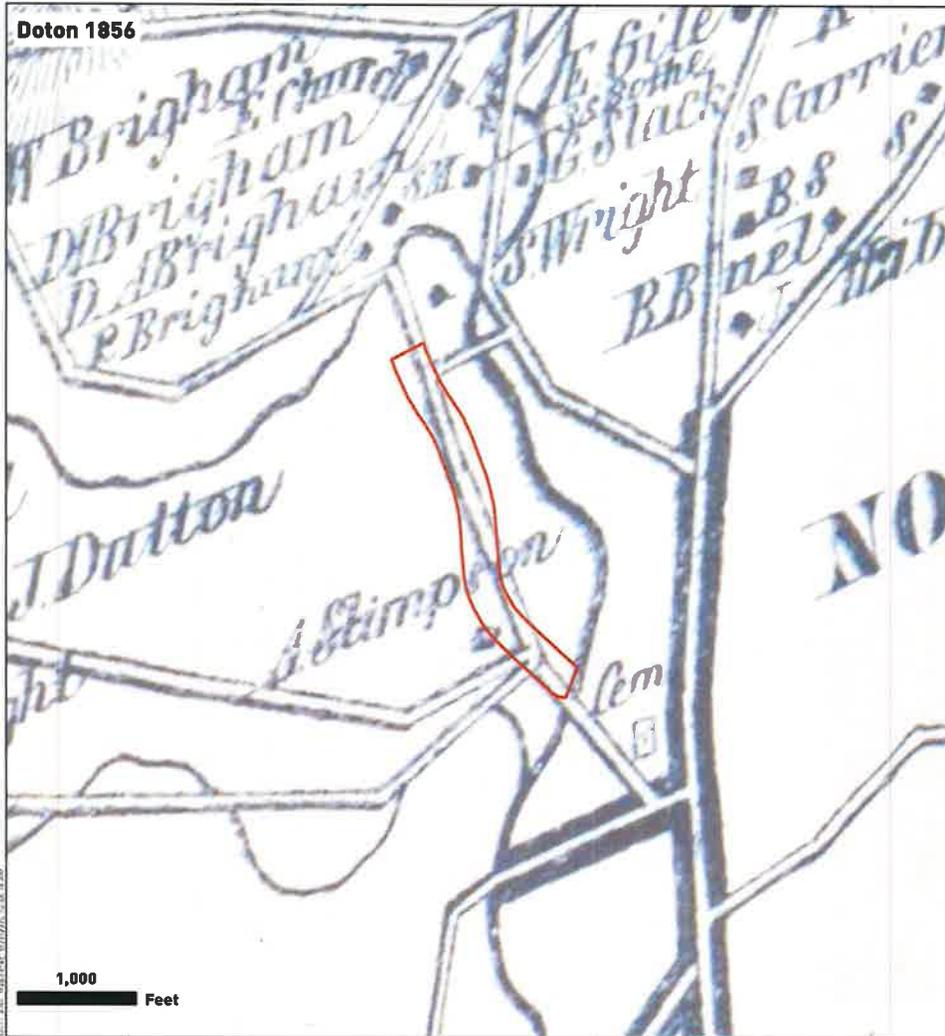
**Photo 2.** Photo shows the circa 1815 Samuel and Sophia Wright House located at 288 Beaver Meadow Road, located at the western end of the project APE. View is to the west.

### **Precontact Site File Research**

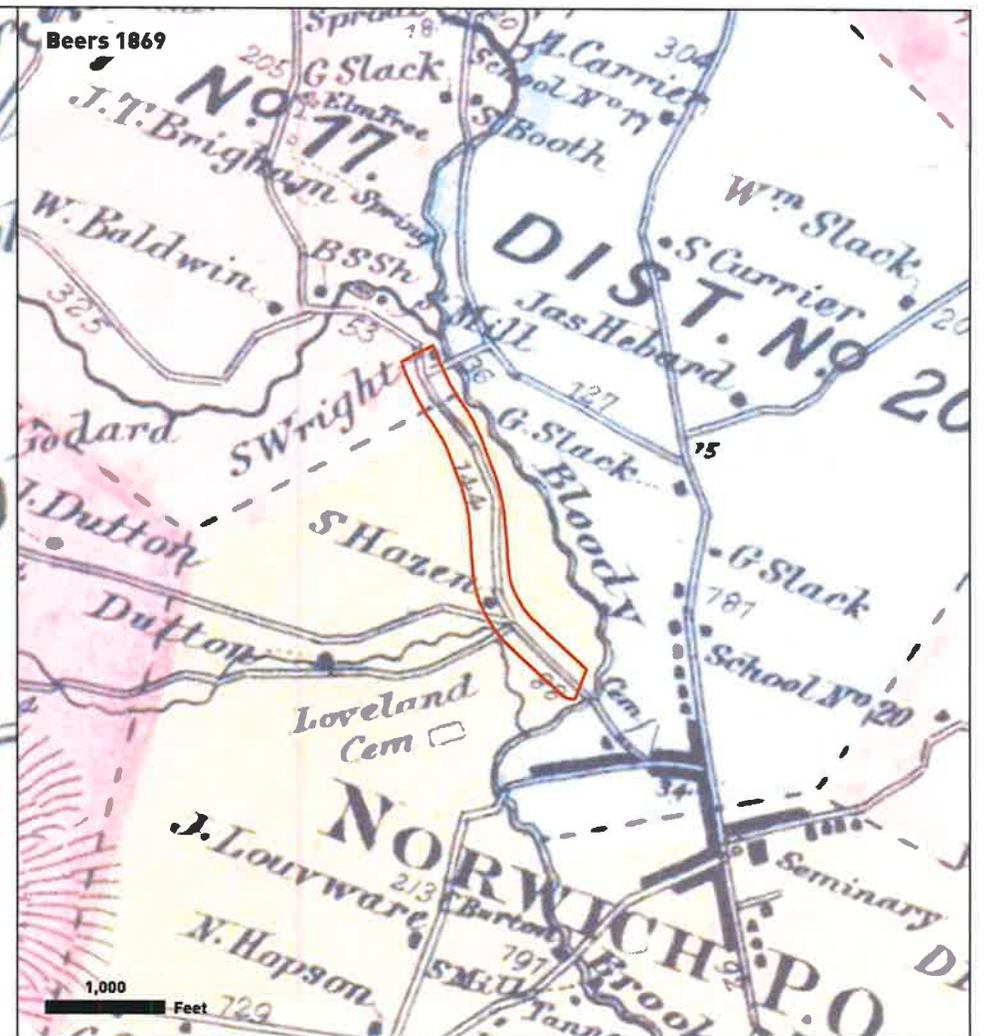
The VDHP site file search revealed that no precontact sites are located within the project area. The majority of recorded precontact sites are located adjacent to the Connecticut River. The closest precontact site to the project area is the Wilder Falls Site, located approximately five miles to the south on the Connecticut River. The paucity of known precontact sites in the project vicinity is quite likely a result of limited amount of archeological testing rather than the absence of sites, as the Bloody Brook and its tributaries in this valley, as well as the Connecticut River, possessed landscape features that would have been attractive for precontact occupation.

The VDHP Environmental Predictive Model (EPM) was completed for the project area which produced an overall rating of 24 (Appendix 1), with a rating of 32 or above indicating precontact sensitivity. The project area received points based on an alluvial terrace, proximity to a permanent stream – Bloody Brook – and situated within a natural travel corridor. If the negative 32 points for previous disturbance were included within the EPM calculation, the project area would have a rating of negative twelve.

**Doton 1856**



**Beers 1869**



1,000 Feet

1,000 Feet



**Legend**

 Area of Potential Effects (APE)

Historical Maps

1856-1869

**HARTGEN**

archaeological consultants, inc.



Map 3

Within the project APE, areas of precontact archeological sensitivity would include undisturbed level terrain adjacent to Bloody Brook. Two such areas were identified, located at the eastern and western ends of the project alignment (Map 2).

### Site Reconnaissance and Recommendations

The project area is characterized primarily as a residential street that contains mostly 20<sup>th</sup> century homes, along with several 19<sup>th</sup>-century homes, which are set back between approximately 25 to 100 feet from Beaver Meadow Road. This study includes both sides of Beaver Meadow Road, so for ease of discussion, the north side and south side of the road will be presented separately below. The descriptions of the project alignment are outlined from the east to the west.

#### North Side of Beaver Meadow Road

On the north side of Beaver Meadow Road, the eastern end of the project area begins at a bridge over the Bloody Brook. Directly west of the brook and bridge is a small level terrace, bordered to the west by Huntley Street, which was identified as Precontact Sensitivity Area 2 (Map 2, Photo 3). To the west of Huntley Street, there is a line of well-maintained 20<sup>th</sup>-century residences which extends to approximately to the half-way point of the project alignment (Photo 4).



**Photo 3.** Photo shows Precontact Sensitivity Area 2 at the eastern end of the project area. View is to the north.

The American Legion Hall is located in the central portion of the project area, surrounded on the east by open grass lawn, and to the west by woods (Photo 5). At this location, Bloody Brook is visible approximately 200 feet to the north, beyond which is the Norwich Town Park (Photo 6). The land contours east of the American Legion Hall, as well as presence of fill soils through the sparse grass cover, suggest that this area



**Photo 4.** Photo shows the streetscape at the eastern end of the project area. View is to the west. Residences are located on the right. The large agricultural field is on the left.



**Photo 5.** Photo shows the grass lawn on the eastern side of the American Legion. View is to the west.



**Photo 6.** Photo shows the landscaped hill on the grass lawn east of the American Legion. Bloody Brook is visible in the background. View is to the north.

has likely been the focus of earthmoving and landscaping associated with flood control of the nearby Bloody Brook (Photos 5 and 6).

To the west of the American Legion, the road shoulder is raised several feet above the adjacent wooded area to the west, suggesting importation of fill to raise the level of the road (Photo 7). Further to the west, the bank of the Bloody Brook comes within approximately 10 feet of the Beaver Meadow Road surface (Photo 8). The presence of landscaping netting and gravel fill are evident on the ground surface – indicating flood control measures were required at this location so close to the road.

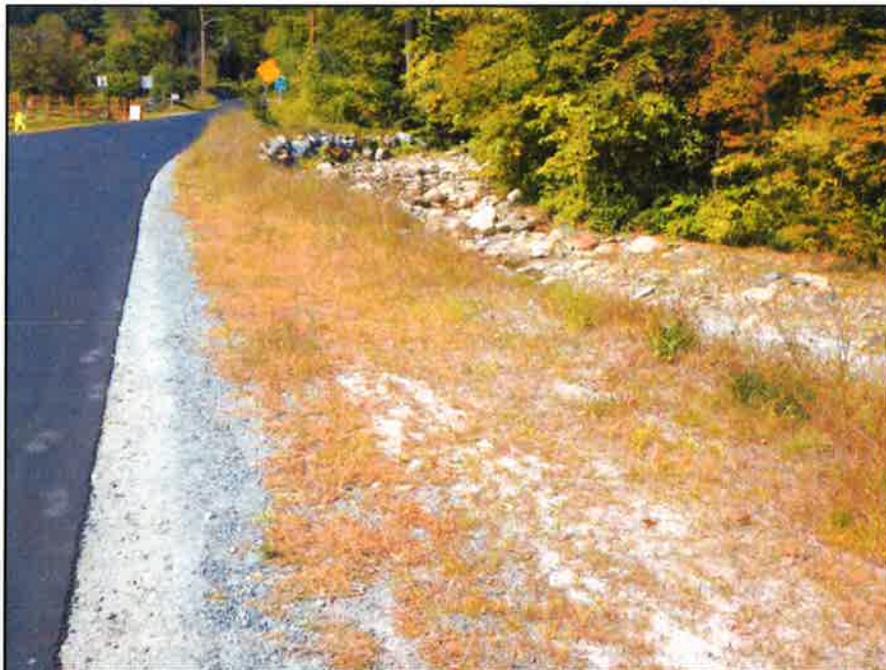
The western end of the project alignment is characterized as a wedge-shaped piece of level land bordered by Beaver Meadow Road to the south, Moore Lane to the west and Bloody Brook to the north and east (Photo 9). This was identified as a precontact Sensitivity Area 1 because its proximity to Bloody Brook and Brown Brook to the northwest. Located on the west side of Moore Lane is the circa 1815 Samuel and Sophia Wright House.

### **South Side of Beaver Meadow Road**

On the south side of Beaver Meadow, between the bridge at the eastern end of the project alignment and Heritage Lane, the area is characterized as slope (Photo 10). To the west of Heritage Lane is an agricultural field which extends approximately 500 feet to the west (Photo 4). At its closest, the eastern end of the field is located approximately 275 feet distant from Bloody Brook. Beyond the field to the west, there is a turn in the road where Bragg Hill Road and Dutton Hill Road fork off to the south. Directly west of Dutton Hill Road is the historic Burton-Ballard homestead (See Photo 1).



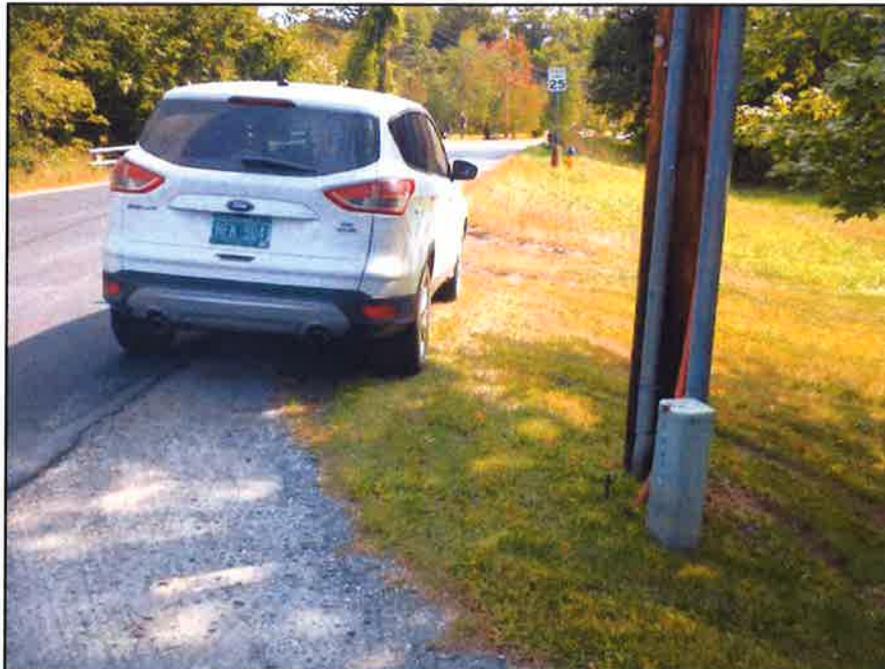
**Photo 7.** Photo shows the terrain on the road shoulder located between the American Legion and the area where Bloody Brook is located adjacent to the roadway. View is to the east.



**Photo 8.** Photo shows Bloody Brook adjacent to Beaver Meadow Road. View is to the west.



**Photo 9.** Photo shows Precontact Sensitivity Area 1 at the eastern end of the project area. View is to the east.



**Photo 10.** Photo shows the sloping terrain directly adjacent to the bridge over Bloody Brook on the south side of Beaver Meadow Road. View is to the east.

Along the section of road north of Dutton Hill Road, the landscape changes dramatically, with moderate to steep slopes and bedrock outcroppings located to the south (Photo 11). Only a few structures are located along this stretch, situated adjacent to outcroppings or on top of hills. At the western end of the project area there are three large modern homes, the last of which is located directly across from Moore Lane.



**Photo 11.** Photo shows a bedrock outcrop located on the south side of Beaver Meadow Road. View is to the southeast.

The project area is considered to have a low historic archeological sensitivity and limited precontact sensitivity. Two areas of precontact archeological sensitivity were identified adjacent to Bloody Brook, located at the eastern and western ends of the project alignment. Phase IB testing is proposed in these two areas.

This ARA report should be submitted to VTtrans archaeology officers for review and concurrence.

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## **APPENDIX I: VDHP Environmental Predictive Model**



17) Marine/Lake Delta Complex**		12	
18) Champlain Sea or Glacial Lake Shore Line**		32	
<b>E. OTHER ENVIRONMENTAL FACTORS:</b>			
19) Caves /Rockshelters		32	
20) <input checked="" type="checkbox"/> Natural Travel Corridor <input type="checkbox"/> Sole or important access to another drainage <input type="checkbox"/> Drainage divide		12	12
21) Existing or Relict Spring	0 – 90 m 90 – 180 m	8 4	
22) Potential or Apparent Prehistoric Quarry for stone procurement	0 – 180 m	32	
23) ) Special Environmental or Natural Area, such as Milton acquifer, mountain top, etc. (these may be historic or prehistoric sacred or traditional site locations and prehistoric site types as well)		32	
<b>F. OTHER HIGH SENSITIVITY FACTORS:</b>			
24) High Likelihood of Burials		32	
25) High Recorded Site Density		32	
26) High likelihood of containing significant site based on recorded or archival data or oral tradition		32	
<b>G. NEGATIVE FACTORS:</b>			
27) Excessive Slope (>15%) or Steep Erosional Slope (>20)		- 32	
28) Previously disturbed land as evaluated by a qualified archeological professional or engineer based on coring, earlier as-built plans, or obvious surface evidence (such as a gravel pit)		- 32	-32
<b>** refer to 1970 Surficial Geological Map of Vermont</b>			
			<b>Total Score: 24</b>
<b>Other Comments :</b>			
<b>0- 31 = Archeologically Non- Sensitive</b> <b>32+ = Archeologically Sensitive</b>			



**HARTGEN**

archeological associates inc

**HISTORIC RESOURCES IDENTIFICATION**  
**Bicycle and Pedestrian Connectivity Scoping Study**

Town of Norwich, Windsor County, Vermont

HAA # 5552-11

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January 2021

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## **1 Introduction**

Hartgen Archeological Associates, Inc. (Hartgen) conducted an Historic Resources Identification Assessment for the proposed Bicycle and Pedestrian Connectivity Scoping Study (Project) located in the Town of Norwich, Windsor County, Vermont (Map 1). The Project requires approvals by the Vermont Agency of Transportation (VTrans). This investigation was conducted to comply with Section 106 of the National Historic Preservation Act of 1966, as amended, and will be reviewed by VTrans.

Background research was conducted at the Vermont Division for Historic Preservation (VDHP) ORC (Online Resource Center) site where archeological site files, National Register (NR), State Register (SR) and town information were reviewed. A site visit was conducted by Elise Manning-Sterling on September 16, 2020, to observe and photograph existing conditions within the Project Area.

## **2 Project Location and Description**

The project is an Architectural Resource Identification for the proposed scoping study to identify issues with construction of a pedestrian/bicycle facility on Beaver Meadow Road in the Town of Norwich, Vermont.

The project is being funded in part by the Federal Highway Administration and the Town of Norwich, Vermont, through the Vermont Agency of Transportation (VTrans) Municipal Assistance Bureau (MAB). The pedestrian/bicycle facility shall, at a minimum, be a sidewalk which extends from the existing sidewalk on Beaver Meadow Road (near the intersection of Huntley Ave.) to the intersection of Moore Road and Beaver Meadow Road.

### **2.1 Description of the Area of Potential Effects (APE)**

Approximately 24 buildings will be surveyed for the Huntley Ave to the intersection of Moore Road and Beaver Meadow Road segment.

## **3 Historical Background**

The town of Norwich is located in the northeastern portion of Windsor County. The Connecticut River borders the town to the east and the Bloody Brook flows through the current Project area.

The boundaries of Norwich were established on July 4, 1761, when Governor Benning Wentworth of New Hampshire defined the boundaries of townships in Vermont. The town was named after Norwich, Connecticut, the state from which the first settlers originated (Slafter 1869).

The Norwich Historical Society described the early history of the town

Two important conditions favored settlement: the termination of the French & Indian War in 1762, opening up a new country finally free from the danger of Indian raids, and the clearing of a bridle path in 1763 from Charlestown, New Hampshire, up the Connecticut River to Hanover. The existence of settlers in Norwich as early as 1763 is proven by the death record of one of them - Jacob Fenton - in that year. Other pioneers soon came. In 1766, John Hatch surveyed the township and divided it into lots, and he also laid out most of the highways in town during the next twenty-five years.

Town meetings were held in Norwich from the year 1768 on, and a period of vigorous immigration set in. The first clearing was at the "Ferry Place" at the foot of what is now Loveland Road, but after 1768 the settlement began to gravitate toward the high land away from the river, because the dense growth on the low land made clearing difficult and the ground was often too wet for cultivation. As early as 1770 a gristmill was built by Elisha Burton, on Blood Brook a little west of the present village. By 1771 Norwich was the most populous town in Cumberland County, with 206 inhabitants.

The decade of the eighties showed rapid growth. The first ferry had been established in 1778, and in 1790 this was replaced by the ingenious Rope Ferry, leading on the Dresden side by "Rope Ferry Road" to the middle of Hanover. (At that time the northern part of the township of Hanover was called Dresden.) This was followed in 1796 by the first bridge from Norwich to Hanover, a very interesting structure. It was built by the "White River Falls Company," chartered June 20, 1792, consisting of Rufus Graves of Hanover, a Lebanon lawyer named Aaron Hutchinson, and Ebenezer Brewster, an innkeeper of Hanover. The bridge was designed, and construction supervised, by Rufus Graves.

In 1810 the population had reached 1,000. Throughout this early period the center of the town was about a mile north of the present village, around the first meeting house and what is now known as "Meeting House Hill." It was called "The Center," whereas the site of the present village was called, at first, "Burton's Plain," and later simply "The Plain." The nucleus of the village on the Plain had been established by the first settlers, Joseph Hatch, Jacob Burton, and Elisha Burton having settled there by 1775. In 1805 the first post-office was built, and in 1806 the "Chelsea Turnpike" was laid through the Village, from Hanover Bridge to Chelsea Courthouse. The building of the South Congregational Church in 1817 contributed to the growth of the Plain and gradually the Center dwindled away. By 1824 the American Literary, Scientific, and Military Academy later Norwich University, had been flourishing for four years and had one hundred and sixty, students. There were several stores, a grammar school a tavern, mechanic shops of various kinds, mills, and some thirty residences. The census of 1830 showed 2,316 inhabitants, the maximum population ever reached and for several years at this time Norwich was larger than her rival town across the river.

In 1848 the Connecticut & Passumpsic Railroad completed its line through to the Norwich-and-Hanover station, but even this symbol of progress did not result in prosperity. The population of Norwich had already begun to dwindle. The emigration to the West began in the forties, and the census of 1850 showed only 1,978 inhabitants. A further decline occurred after the removal of Norwich University to Northfield Vermont in 1866. The times were changing, and agricultural towns throughout New England were losing to the new industrial cities. It is for this reason that little of architectural interest went on in Norwich after the heyday of the 1820's and 1830's.

Nevertheless, in its prime Norwich was wealthy and populous; it boasted many men prominent in positions of state, a good educational institution, five churches, and many fine old dwellings along its elm-bordered streets (Norwich Historical Society 1973).

The Project runs through a part of the town of Norwich which was historically known as District No. 1, District No. 17, and District No. 20 as seen on the 1869 Beers Atlas (Figure 3) (Beers 1869).



Figure 1. The APE outlined on aerial imagery.

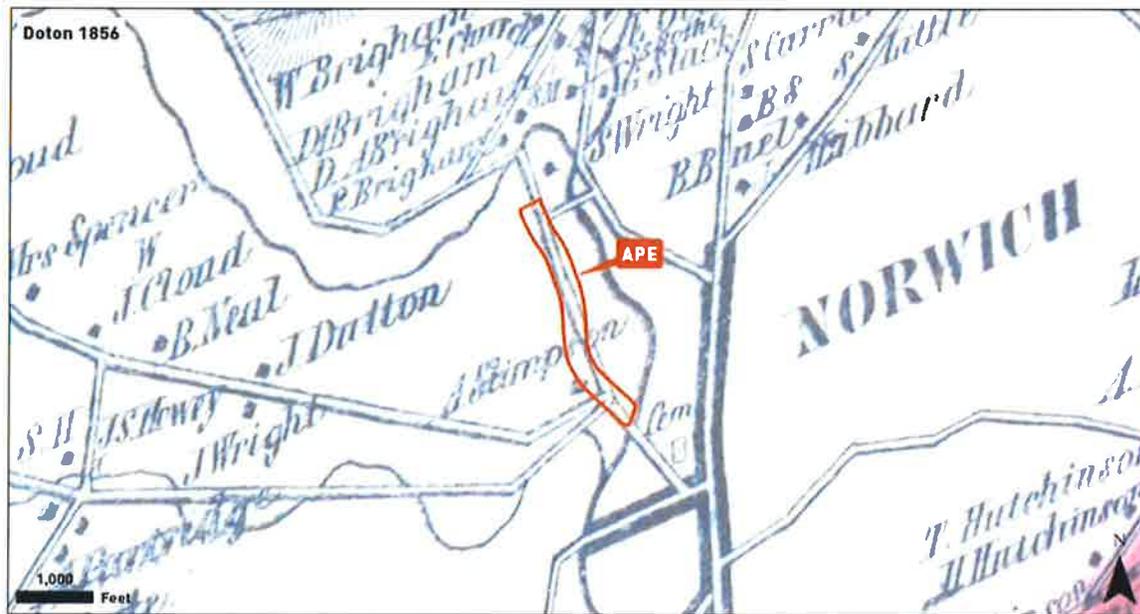


Figure 2. The APE outlined on the 1856 Chase *Map of Windsor County* (Chace 1856).

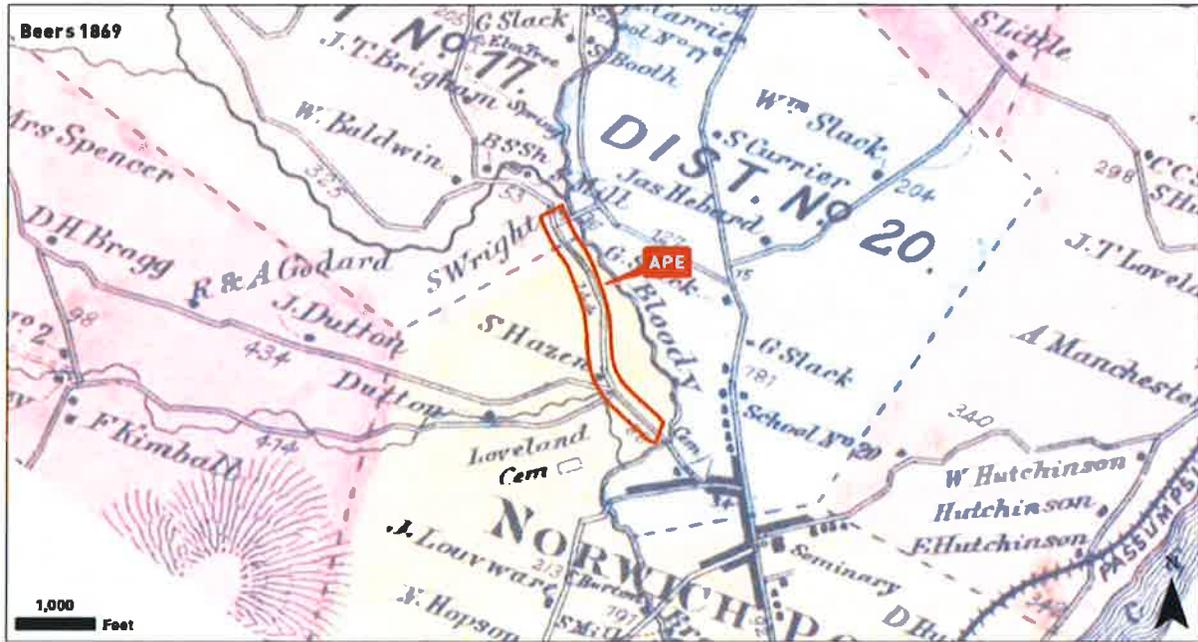


Figure 3. The APE outlined on the 1869 Beers *Atlas of Windsor County* (Beers 1869).

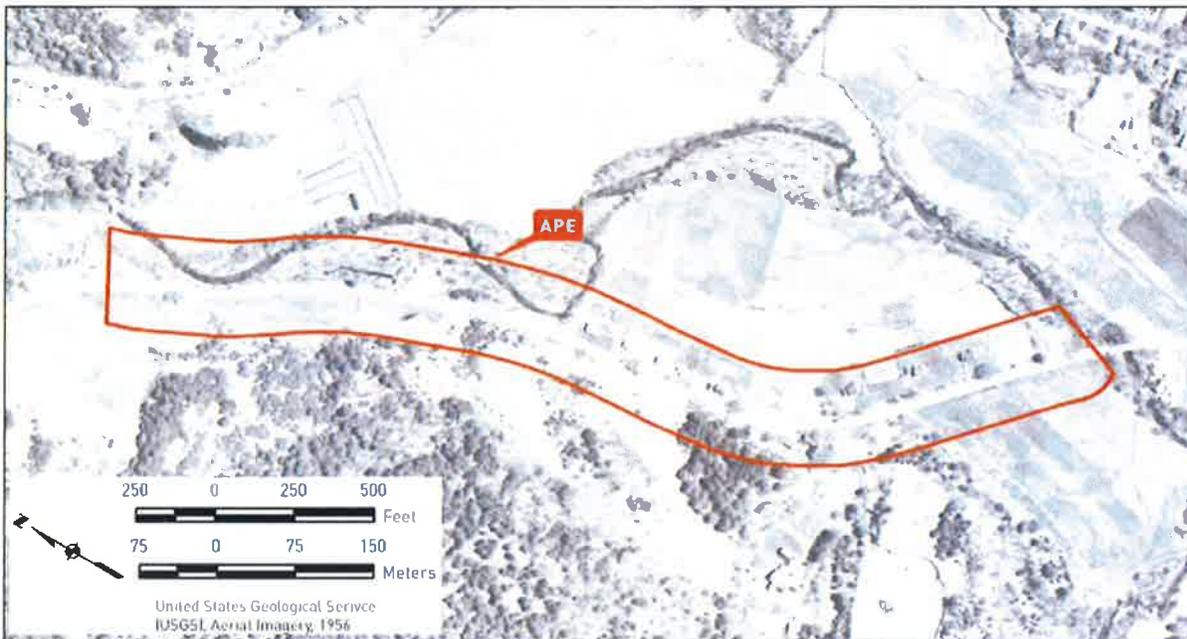


Figure 4. The APE outlined on a 1956 aerial photograph (U.S. Geological Survey 1956).

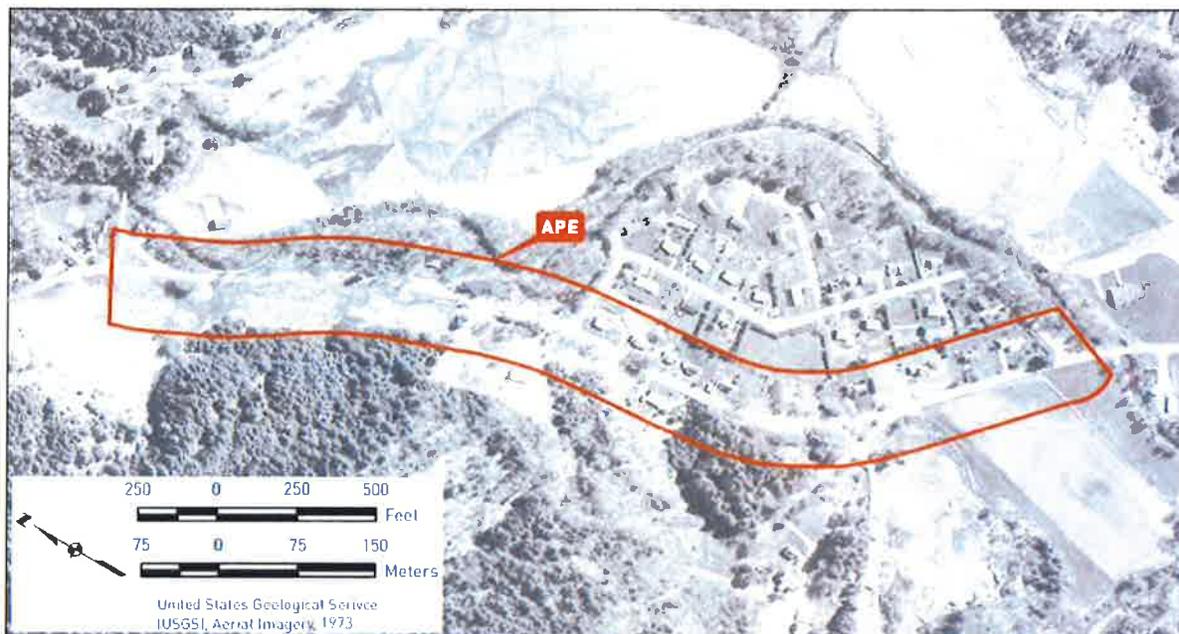


Figure 5. The APE outlined on a 1973 aerial photograph (U.S. Geological Survey 1973).

### 3.1 Historical Map Review

Two of the resources involved in this study were constructed prior to 1856 (Figure 2). Structure 2 at 149 Beaver Meadow Road was built c. 1780 and was the home of Alba and Phebe Stimpson in the 1850s. Alba Stimpson was listed as a farmer in the population census for that year (United States Census Bureau 1850). Structure 11 at 288 Beaver Meadow Road was constructed c. 1815. Structure 11 was the home of “S. Wright” according to both the Chase and Beers landowner maps (Beers 1869; Chace 1856). Samuel Wright, a Vermont native, was a carpenter who lived in this house with his wife, Sophia, also a native of Vermont (United States Census Bureau 1860).

Nine of the resources were constructed between 1856 and 1956 (Figure 4). Structure 12 at 6 Moore Lane was built c. 1955. Structures 16, 17, 18, 19, and 20 were all built sometime between c. 1945 and c.1950. Structures 22, 23, and 24 were also built between c. 1945 and c.1950.

Only four of the resources included in the survey were built between 1956 and 1973 (Figure 5). Structure 3 at 179 Beaver Meadow Road was built c. 1960, Structure 5 at 213 Beaver Meadow Road was built c. 1971, Structure 13 at 18 Moore Lane was built c. 1970 and Structure 25 at 106 Beaver Meadow Road was built c. 1957.

The remaining resources were constructed after 1973. Structure 1 at 15-31 Heritage Lane was built c. 1987. Structure 4 at 191 Beaver Meadow Road was constructed c. 1974. Structure 6 at 18 Glen Ridge Road was built c. 1978. Both Structures 7 and 8 were built c. 2006 and Structure 9 was constructed c. 2014. Structure 10 was constructed c. 1977. The outbuilding associated with Structure 11 at 288 Beaver Meadow Road was built sometime after 1973 according to aerial photography. The Huntley Meadow Park (Structure 14) was created over the span of several years during 1973 and 2013. Structure 21 at 152 Beaver Meadow Road was built c. 2000.

### 3.2 Previously Surveyed Properties

An examination of the files at VDHP identified no NR listed (NRL) properties, no NR eligible (NRE) properties, one property that has been listed on the State Register (SRL) no properties previously determined to be ineligible, and no properties of undetermined status within the APE. These properties are indicated in Table 1 on Page 43.

#### 4 Streetscape Views



Photo 1. View up Beaver Meadow Road, looking northwest. Structure 23 in view.



Photo 2. View along Beaver Meadow Road, looking south. Structures 17 and 18 in view.



Photo 3. Beaver Meadow Road, looking northwest. Structure 15 in view.



Photo 4. View of intersection of Beaver Meadow Road and Moore Lane, looking southeast. Structure 12 in view.

## 5 Architectural Descriptions

### 5.1 Structure 1. 15-31 Heritage Lane

Structure 1 (Photo 5, Photo 6 and Photo 7) consists of three separate buildings: 15, 23, and 31 Heritage Lane. All three buildings have been characterized as a condo or townhouse style building. According to the Norwich property cards for 15, 23, and 31 Heritage Lane, all three structures were constructed c. 1987 (Town of Norwich 2020).

These buildings have not been previously surveyed, either for the Vermont State Register or for their potential inclusion on the National Register of Historic Places. Each is a semi-detached two-story wood-frame dwelling with attached garage, having wood clapboard exteriors, standing seam metal roofs and brick chimneys.

Structure 1 is not eligible for listing on the National Register due to insufficient age.



Photo 5. Structure 1, 15 Heritage Lane, facing south (Town of Norwich 2020).



Photo 6. Structure 1, 23 Heritage Lane, facing southeast (Town of Norwich 2020).



Photo 7. Structure 1, 23 Heritage Lane, facing south (Town of Norwich 2020).

## 5.2 Structure 2. 149 Beaver Meadow Road—Burton-Ballard House

Structure 2 (Photo 9, Photo 8, Photo 10, Photo 11 and Figure 6) was first described in a publication from the Norwich Historical Society in 1973 as an

...example of the old central chimney type of house, and almost a twin of the Hatch-Peisch House, is the Burton-Ballard House, in the 1930's better known as the "Happy Hill Tavern". It is fairly certain that the house was built by Pierce Burton, although when he came to Norwich or whether he was a member of the Jacob and Elisha and John Burton family is not recorded. Mrs. Ellen Knight notes the similarity of the house to the Hatch-Peisch House, and believes it was built in the same year. The "History" says: "The dwelling of W. S. Hazen (who owned the house in question in 1905), the Deacon John Salfter House built in 1786, and that of Charles Hazen (in Hartford built in 1775) are each specimens yet remaining of a similar style of the better farm houses of that day. This suggests an early date for the house, and it seems reasonable on stylistic evidence to date it around 1775 to 1780. Some old account books found in the house reveal that it was used for a general store in the early 1800's and possibly earlier (Norwich Historical Society 1973).

Structure 2 was later recorded on a Vermont Historic Sites & Structure Survey form completed in 1974. It was listed on the Vermont State Register in 1977 (Fisher 1974). Structure 2 was described at that time as being built c. 1780 and as

An excellent example of a 2 ½-story, wood frame, center chimney, Georgian style house with a center chimney first floor plan; i.e. basically two rooms in the front flanking a front entrance hall and a center chimney stack, and a large center room in the rear with one or two rooms flanking each side (Fisher 1974).

This structure has not been previously surveyed for its potential inclusion on the National Register of Historic Places. Since being listed on the Vermont State Register in 1977, a large covered porch has been added to the west end of the house. A one-and-one-half story wood frame wing extends north from the east end of the house; it appears to date to the nineteenth century and has a gable roof and a shed-roofed porch at its north end. Structure 2 occupies a mortared stone foundation. A one-story gable-entry garage is attached to the east side of the wing; it and the large porch have concrete foundations. A carriage barn, two-stories in height, wood-framed with clapboard sheathing, and of rectangular plan and with a gable roof, is of unknown age, but predates 1953, when it appears on aerial photography, and likely during the nineteenth century.

Despite the construction of the covered porch on the west end of the house, Structure 2 retains a high level of integrity, and is a rare example of a large eighteenth century wood-frame house in the area. It would be eligible for listing on the National Register under Criteria A and C.



Photo 8. Structure 2, 149 Beaver Meadow Road, facing northwest.



Photo 9. Structure 2, 149 Beaver Meadow Road, facing south.



Photo 10. Outbuilding associated with Structure 2, 149 Beaver Meadow Road, facing southwest.



Photo 11. Stone wall associated with Structure 2, 149 Beaver Meadow Road, facing south-southeast.



Figure 6. 1974 photograph of Structure 2 (Fisher 1974).

### 5.3 Structure 3. 179 Beaver Meadow Road

Structure 3 (Photo 12) is a Cape-style house. Although the Norwich property card for 179 Beaver Meadow Road reports that it was built c. 1953, aerial photography suggests that this house was built between 1956 and 1973 (Town of Norwich 2020; U.S. Geological Survey 1956, 1973).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

Structure 3 is a one-story wood-frame cape dwelling, with side gable roof that has two gable-roofed dormers. The exterior is sheathed with clapboards and it has an asphalt shingle roof. A brick chimney is located on the north slope of the roof, close to the center of the building. It rests on a concrete foundation. A two-bay gable-front garage is attached to the house via a breezeway. The fenestration of the house has been altered and replaced with vinyl windows. The present location of the door is unusual for this house type, and may indicate additional alterations.

Structure 3 does not retain sufficient integrity to qualify for listing on the National Register.



Photo 12. Structure 3, 179 Beaver Meadow Road, facing southwest.

#### 5.4 Structure 4. 191 Beaver Meadow Road

Structure 4 (Photo 13 and Figure 7) was described as a Contemporary-style building built c. 1974 on its Norwich property card (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

Structure 4 is a two-and-one-half story wood frame gable-front dwelling with two story frame wing, both parts of which are sheathed with vertical siding and have standing seam metal roofs. The foundation is concrete and two small brick chimneys are located near the juncture of the main gable roof and the intersecting gabled roof of the wing. Windows throughout are casements, either side or top hung. A basement garage door opening has, since 2015, been altered into a second entrance (Photo 13; Figure 7).

Structure 4 is ineligible for listing on the National Register due to insufficient age.

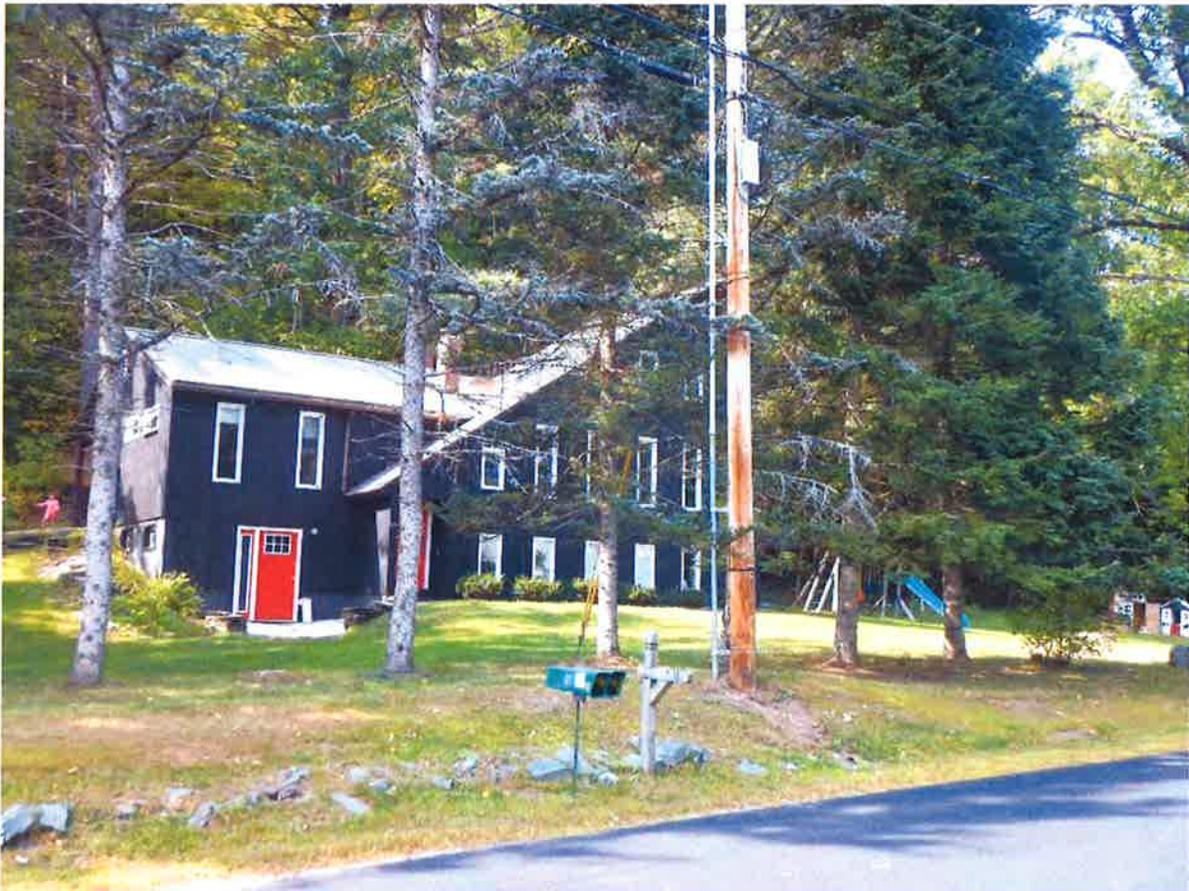


Photo 13. Structure 4, 191 Beaver Meadow Road, facing northwest.



Figure 7. 2015 photograph of Structure 4, facing northwest (Town of Norwich 2020).

### 5.5 Structure 5. 213 Beaver Meadow Road

Structure 5 ( Photo 14) was built c. 1971 according to its Norwich property card (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

Structure 5 is a two-story wood-frame raised ranch with two garage bays at the north end of its banked, stone-faced foundation. Windows are casement type throughout, and are recent replacements. The house is sheathed with wood or aluminum siding and has an asphalt shingle roof. A one-story wing was attached to the east side of the house at a date subsequent to its initial construction.

213 Beaver Meadow Road is ineligible for listing on the National Register due to loss of integrity.



Photo 14. Structure 5, 213 Beaver Meadow Road, facing south.

### 5.6 Structure 6. 18 Glen Ridge Road

Structure 6 (Photo 15 and Photo 16) is a Cape-style house. According to the Norwich property card for 18 Glen Ridge Road, Structure 6 was built c. 1978 (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

Structure 6 is a cape dwelling, one-and-one-half storied in height with an asymmetrical gable roof. It is of wood frame construction and is sheathed with wood clapboards. It has an asphalt shingle roof. A garage addition, one story in height, makes use of the same materials and is connected to the house via a breezeway (Photo 16).

Structure 6 is ineligible for listing on the National Register due to insufficient age.



Photo 15. Structure 6, 18 Glen Ridge Road, facing southwest.



Photo 16. 2012 photograph of Structure 6, facing north-northwest (Town of Norwich 2020).

### 5.7 Structure 7. 259 Beaver Meadow Road

Structure 7 (Photo 17) was constructed c. 2006 (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

Structure 7 is a two-story, five-bay wide side-gable wood-frame dwelling, with attached two-bay gable-entry garage, connected to the house via a one-story hyphen with recessed porch. The exteriors are sheathed with vinyl siding, and the roofs are covered with asphalt shingles. It sits on a concrete foundation.

259 Beaver Meadow Road is ineligible for listing on the National Register due to insufficient age.



Photo 17. Structure 7, 259 Beaver Meadow Road, facing southwest.

### 5.8 Structure 8. 261 Beaver Meadow Road

According to the property card, Structure 8 (Photo 18) was constructed c. 2006 (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

261 Beaver Meadow Road is a two-story wood-frame single family dwelling consisting of a three-bay wide, side passage main block with a two-bay wide wing with catslide roof. A three-bay garage with side-gable roof and central glazed cupola is attached to the south end of the house. All roofs are covered with asphalt shingles. Exterior sheathing material is vinyl siding. The house sits on a concrete foundation.

Structure 8 is ineligible for listing on the National Register due to insufficient age.



Photo 18. Structure 8, 261 Beaver Meadow Road, facing northwest.

### 5.9 Structure 9. 275 Beaver Meadow Road

According to the property card, Structure 9 (Photo 19 and Photo 20) was constructed c. 2014 (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

Structure 9 is a wood-frame building with gambrel roof, sheathed in wood clapboards and having a standing seam metal roof. It sits on a concrete foundation. A glazed cupola is centered on the roof. Windows are top-hung casements or double-hung sash. The house is designed to have the appearance of a barn; principal entry is through a gable-end door with pentice overhang.

275 Beaver Meadow Road is ineligible for listing on the National Register due to insufficient age.



Photo 19. Structure 9, 275 Beaver Meadow Road, facing northwest.



Photo 20. Structure 9, 275 Beaver Meadow Road, facing southwest.

#### 5.10 Structure 10. 297 Beaver Meadow Road

According to the Norwich property card for 297 Beaver Meadow Road, Structure 10 (Photo 21) constructed c. 1977 (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

Structure 10 is a one-story wood-frame cape dwelling with side-gable roof. A shed-roofed dormer extends across much of the north face of the gable roof; two gable-roofed dormers light the south slope of the roof. The house is connected to a two-bay gable-entry garage by an open gable-roofed breezeway. The breezeway and garage are sheathed with vertical board and batten siding; the house is covered with wood clapboards. All parts of the building are covered with asphalt shingle roofs. Windows are a combination of casements and divided lite double-hung sash. Windows appear to be vinyl-clad replacements for the original sash.

297 Beaver meadow Road is ineligible for listing on the National Register due to insufficient age.



Photo 21. Structure 10, 297 Beaver Meadow Road, facing southwest (Town of Norwich 2020).

### 5.11 Structure 11. 288 Beaver Meadow Road

According to the Norwich property card for 288 Beaver Meadow Road, Structure 11 (Photo 22 and Photo 23) was constructed c. 1815 (Town of Norwich 2020). Structure 11 was the home of “S. Wright” according to both the Chase and Beers landowner maps (Beers 1869; Chace 1856). Samuel Wright, a Vermont native, was a carpenter who lived in this house with his wife, Sophia, also a native of Vermont (United States Census Bureau 1860).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

288 Beaver Meadow Road is a one-and-one-half story five-bay wide cottage with central wall dormer. A one-story wing is four bays in width and contains a secondary entrance with sidelights. Both have side-gable roofs covered with standing seam metal pans. Skylights have been introduced in lieu of dormers, in an effort to preserve the original form of the dwelling. The house is of wood-frame construction and is sheathed with clapboards. It has double-hung windows with undivided lights.

A two-and-one-half story gable-roofed barn is attached to the house via a breezeway. It is sheathed with board and batten siding, and has a standing seam metal roof. It was converted into living space at an unknown date and has two-over-two double-hung sash.

This house retains sufficient integrity of design and materials to be eligible for listing on the National Register under Criteria C.



Photo 22. Structure 11, 288 Beaver Meadow Road, facing north.



Photo 23. Outbuilding associated with Structure 11, 288 Beaver Meadow Road, facing north.

**5.12 Structure 12. 6 Moore Lane**

According to the Norwich property card for 6 Moore Lane, Structure 12 (Photo 24) is a Cape-style house which was constructed c. 1955 (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

A one-story wood frame cottage with side-gable roof, this house has a projecting entrance, sheltered by a gable roof. Both are sheathed with wood shingles. Two gable-roofed dormers light the second-floor spaces. Windows are six-over-one double-hung sash, either single or in pairs. The windows appear to have been replaced. A large one-story wing with secondary door may have originally housed an attached garage. All are covered with standing seam metal roofs and sit on concrete foundations.

6 Moore Street is ineligible for listing on the National Register due to a loss of integrity to its initial period of construction.



Photo 24. Structure 12, 6 Moore Lane, facing east.

**5.13 Structure 13. 18 Moore Lane**

Structure 13 (Photo 25) was described as a Conventional-style building built c. 1970 on its Norwich property card (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

Structure 13 is a two-story wood-frame raised ranch with low side-gable roof and jettied second floor. It has a wood clapboard exterior and is covered with a standing seam metal roof. A one-story enclosed porch was added to the house at an unknown date. A brick chimney is located on the south elevation. Windows are horizontally-divided double-hung sash, arranged singly or in pairs. A tripartite window is located at the south end of the first floor of the principal elevation; a paneled garage door is located at the other end of the first floor of that façade.

Structure 13 is not considered to be eligible for listing on the National Register due to lack of integrity to its initial period of construction.



Photo 25. Structure 13, 18 Moore Lane, facing south (Town of Norwich 2020).

**5.14 Structure 14. 111 Turnpike Road—Huntley Meadow Park/ Kearney Niles Pavilion**

Structure 14 (Photo 26) was described as a 27 acre park owned by the Town of Norwich (Vermont Center for Geographic Information 2020). An examination of aerial photography indicates that the park was built over the course of several years between 1973 and 2013 (U.S. Geological Survey 1973, 2013). This park has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

The Kearney Niles Pavilion was constructed between 1992 and 2003 (U.S. Geological Survey 1992, 2003). It is a one-story gable-roofed unenclosed recreational pavilion. Because of its recent construction date, it is not eligible for listing on the National Register.



Photo 26. Structure 14, 111 Turnpike Road, facing southwest.

**5.15 Structure 15. 228 Beaver Meadow Road—American Legion Lyman F. Pell Post 08**

Structure 15 (Photo 27) was described on its Norwich property card as being built c. 1970; aerial photography indicates that an earlier structure had been located on this site, and that it was constructed prior to 1956 (Town of Norwich 2020; U.S. Geological Survey 1956, 1973). This American Legion post was registered as a Vermont Corporation on 9 March 1956.

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places. It is a one-story concrete block building on a banked concrete foundation. It is rectangular in plan and has a shallow side-gable roof. A centrally-disposed entrance porch has a gable roof supported on two square corner posts. The roofs of both the building and its porch are covered with standing seam metal pans. The eight-over-eight double-hung windows and the hollow core metal entrance door are replacements.

Structure 15 is ineligible for listing on the National Register due to loss of integrity to its original period of construction.



Photo 27. Structure 15, 228 Beaver Meadow Road, facing northeast.

#### 5.16 Structure 16. 196 Beaver Meadow Road

Structure 16 (Photo 28) was described as a Cape-style building built c. 1945 on its Norwich property card (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

196 Beaver Meadow Road is a one-story wood-frame cottage, four bays in width, with two gabled dormers on the west slope of its side-gable roof. A one-story wing at the south end of the house and a second wing also of one-story height are later additions. Both are of wood frame construction. The roofs are covered with asphalt shingles, and the exterior is sheathed with aluminum siding. The house sits on a concrete foundation. Windows are six-over-six double-hung sash, arranged singly and in pairs. Aluminum or vinyl shutters flank the windows.

Structure 16 is ineligible for listing on the National Register due to loss of integrity to its initial period of construction.



Photo 28. Structure 16, 196 Beaver Meadow Road, facing northeast.

**5.17 Structure 17. 186 Beaver Meadow Road**

Structure 17 (Photo 29) was described as a Cape-style building built c. 1948 on its Norwich property card (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

186 Beaver Meadow Road is a one-story wood-frame side-gable cottage, three bays in width, with a one-story wood-frame hipped roof addition on the south and an attached one-bay garage, at the north. The house and its additions are sheathed with wood shingles and rest on concrete block foundations. Asphalt roofing shingles cover all roofs except that of the south addition, which is covered with a standing seam metal roof. A shed-roofed dormer extends the full width of the east slope of the roof of the main block of the house; it is unknown whether this was an original feature of the house, or if it constitutes an alteration. The entrance is sheltered by a gable roof supported on diagonal braces. Windows are six-over-six double-hung sash, except for that which lights the living room at the north end of the street elevation, which is a fixed-sash window divided into 28 lights. An aluminum and vinyl garage door of recent vintage replaces the original.

Structure 17 is ineligible for listing on the National Register due to numerous alterations and additions which have diminished its integrity to its period of construction.



Photo 29. Structure 17, 186 Beaver Meadow Road, facing east.

#### 5.18 Structure 18. 182 Beaver Meadow Road

Structure 18 (Photo 30) was described as a Cape-style building built c. 1949 on its Norwich property card (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

182 Beaver Meadow Road is a one-story wood-frame cottage, rectangular in plan with a side-gable roof. It is sheathed with aluminum siding and covered with a standing seam metal roof. The house sits on a concrete foundation. The central portion of the street elevation projects slightly from the body of the house and is covered with a shed roof that extends from the ridge of the main roof of the house. This projection contains an inset entrance that has a small gabled roof, and a large fixed sash window which presumably lights the living room. A brick chimney is attached to the center of the south elevation. Windows are typically six-over-six double-hung sash, spaced either singly or in pairs; some of the windows appear to be vinyl replacement sash. Vinyl decorative shutters flank the window openings.

Structure 18 is ineligible for listing on the National Register due to a loss of integrity to its period of initial construction.



Photo 30. Structure 18, 182 Beaver Meadow Road, facing northeast.

**5.19 Structure 19. 176 Beaver Meadow Road**

Structure 19 (Photo 31) was described as a Cape-style building built c. 1950 on its Norwich property card (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

176 Beaver Meadow Road is a one-story wood-frame cottage, three bays in width with a side gable roof. It has a two-bay wide wing attached to its north elevation, which has an inset porch. The wing appears to be contemporary with the house; both are sheathed with wood clapboards, occupy a concrete block foundation and have asphalt shingle roofs. Windows are double-hung divided light sash; larger windows are 12-over-12, smaller windows are divided into six-over-six lights. Two small gable-roofed dormers on the west slope of the roof light the second-floor living spaces. A brick chimney is attached to the center of the south elevation.

This house retains all of its character-defining features, and is a good intact and representative example of a mid-20<sup>th</sup> century Colonial Revival dwelling of modest size. It would be eligible for the National Register under Criterion C.



Photo 31. Structure 19, 176 Beaver Meadow Road, facing northeast.

## 5.20 Structure 20. 166 Beaver Meadow Road

Structure 20 (Photo 32 and Photo 33) was described as a Cape-style building built c. 1947 on its Norwich property card (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

166 Beaver Meadow Road is a one-story wood-frame cottage with side gable roof. It is three bays in width; the principal entrance occupies the center bay. It is rectangular in plan and has a brick chimney located in the center of its roof ridge. The house sits on a concrete foundation and is sheathed with aluminum siding. It has a standing seam metal roof. A wood frame garage of the same materials is connected to the house by a glazed passage. Both have gabled roofs; the garage has a small cupola straddling its ridge. Windows are typically double-hung sash; those on the principal elevation are divided into six-over-six lights. It is not clear whether the garage was part of the original construction phase; however, it has been greatly altered by the removal of its entrance bay door and a near doubling in depth, apparently associated with its conversion into additional living space (Photo 33).

Structure 20 is not eligible for listing on the National Register due to loss of integrity to its period of initial construction.



Photo 32. Structure 20, 166 Beaver Meadow Road, facing east.



Photo 33. Structure 20, 166 Beaver Meadow Road, facing southeast.

**5.21 Structure 21. 152 Beaver Meadow Road**

According to the Norwich property card for 152 Beaver Meadow Road, Structure 21 (Photo 34) was constructed c. 2000 (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

152 Beaver Meadow Road is a one-story wood-frame cottage with side-gable roof, five bays in width, with a two-bay gable-entry garage, connected by a two-bay wide hyphen. The house, hyphen and garage are sheathed with vinyl siding and have asphalt shingle roofs. Windows are vinyl and are double-hung divided light sash. The house sits on a concrete foundation. A small brick chimney is located in the west slope of the roof, near the center of the house.

Structure 21 is ineligible for listing on the National Register due to insufficient age.



Photo 34. Structure 21, 152 Beaver Meadow Road, facing north.

## 5.22 Structure 22. 138 Beaver Meadow Road

According to the Norwich property card for 138 Beaver Meadow Road, Structure 22 (Photo 35) was constructed c. 1946 (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

138 Beaver Meadow Road is a one-story cape, three bays in width with a side-gable roof. It has a one-story wing attached to its south elevation, and a former attached garage and breezeway has been converted into additional living space by the removal of the garage bay door and insertion of a three-part window with fixed central plate glass panel. The house and its additions have been resheathed with vinyl siding, and the original windows have been replaced with vinyl sash, some of which has false divisions. The house sits on a concrete foundation and has asphalt shingle roofs. A brick chimney straddles the center of the roof. A detached one-story garage, with two vehicular bays and gable entry, is located to the north of the house. It has an asphalt shingle roof with solar panels mounted on it and is sheathed with vinyl siding.

Structure 22 is ineligible for listing on the National Register due to loss of integrity to its period of construction.



Photo 35. Structure 22, 138 Beaver Meadow Road, facing northeast.

**5.23 Structure 23. 124 Beaver Meadow Road**

According to the Norwich property card for 124 Beaver Meadow Road, Structure 23 (Photo 36 and Photo 37) was constructed c. 1947 (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

Structure 23 is a one-story cape, five bays wide with center door, and rectangular in plan. The west slope of its side-gable roof has two gabled dormers. A large brick chimney straddles the ridge of the roof, near the center of the dwelling. The house is sheathed with clapboards and sits on a concrete block foundation. Windows are 12-over-8 double hung sash. Multiple one-story additions on the east side of the house are largely obscured from streetside view, but are visible in orthoimagery.

A large two-story gable-entry wood-frame addition houses two garage bays in its first floor, with living space above. It is sheathed with board and batten siding, and appears to be of recent date of construction. It is connected to the house by a one-story hyphen. Both the house and its several additions have asphalt shingle roofs.

Structure 23 is ineligible for listing on the National Register due to loss of integrity to its period of construction.



Photo 36. Structure 23, 124 Beaver Meadow Road, facing north.



Photo 37. Structure 23, 124 Beaver Meadow Road, facing east.

#### 5.24 Structure 24. 114 Beaver Meadow Road

According to the Norwich property card for 114 Beaver Meadow Road, Structure 24 (Photo 38 and Photo 39) was constructed c. 1949 (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

Structure 24 is a one-story cottage, three bays in width, with a central brick chimney located in the middle of the ridge of its side-gabled roof. The façade of the house is covered with brick; secondary elevations are clapboarded. The roof is covered with standing seam metal pans. The house sits on a concrete foundation. Two gable-roofed dormers light the second-floor living spaces.

A secondary entrance is housed in a one-story gable-roofed addition, which is itself connected to a two-bay garage by a gable-roofed hyphen. Although executed with sensitivity to the original house, these additions have altered the original appearance of the building and it is not considered to be eligible for listing on the National Register.



Photo 38. Structure 24, 114 Beaver Meadow Road, facing northeast.



Photo 39. Structure 24, 114 Beaver Meadow Road, facing north.

**5.25 Structure 25. 106 Beaver Meadow Road**

According to the Norwich property card for 106 Beaver Meadow Road, Structure 25 (Photo 40) was constructed c. 1957 (Town of Norwich 2020).

This structure has not been previously surveyed, either for the Vermont State Register or for its potential inclusion on the National Register of Historic Places.

Structure 25 is a one-story wood-frame single-family dwelling with side-gable roof. It is three bays in width, with a central entrance. A semi-attached garage addition appears to house an apartment. The house and its wing sits on a concrete foundation, have wood-shingle wall covering and asphalt shingle roofs. It appears to have been initially constructed as a twin to 138 Beaver Meadow Road (Structure 22). The two dormers lighting the second floor over the garage wing are later additions. Windows are horizontally-divided two-over-two double hung sash, and may be original.

Structure 25 is ineligible for listing on the National Register due to loss of integrity to its period of initial construction.



Photo 40. Structure 25, 106 Beaver Meadow Road, facing northeast (Town of Norwich 2020).

## 6 National Register Eligibility Summary

A total of 25 structures were included in this survey. One of these (Structure 2) had previously been surveyed and is listed on the Vermont State Register. Three structures (2, 11 and 19) are recommended as eligible for listing on the National Register. The remaining structures are recommended as not National Register eligible due to alterations or insufficient age.

Based upon a review of preliminary project plans, there are no historic preservation concerns with respect to this project. However it should be noted that impacts to the stone wall and historic plantings associated with Structure 2 should be avoided.

Table 1. Summary of Resources Surveyed for the Bicycle and Pedestrian Connectivity Scoping Study Area

<b>Building No. (see Map 2)</b>	<b>Resource Address</b>	<b>Construction Date</b>	<b>Historic Use or Name</b>	<b>Previous Survey and/or NR status</b>	<b>Recommended National Register Status</b>
1	15-31 Heritage Lane	c. 1987		None	Not NRE
2	149 Beaver Meadow Road	c. 1780	Burton-Ballard House	1977 individually SRL structure (#1411-11)	NRE
3	179 Beaver Meadow Road	c. 1960		None	Not NRE
4	191 Beaver Meadow Road	c. 1974		None	Not NRE
5	213 Beaver Meadow Road	c. 1971		None	Not NRE
6	18 Glen Ridge Road	c. 1978		None	Not NRE
7	259 Beaver Meadow Road	c. 2006		None	Not NRE
8	261 Beaver Meadow Road	c. 2006		None	Not NRE
9	275 Beaver Meadow Road	c. 2014		None	Not NRE
10	297 Beaver Meadow Road	c. 1977		None	Not NRE
11	288 Beaver Meadow Road	c. 1815	Samuel and Sophia Wright house	None	NRE
12	6 Moore Lane	c. 1955		None	Not NRE
13	18 Moore Lane	c. 1970		None	Not NRE
14	111 Turnpike Road	1973-2013	Huntley Meadow Park/ Kearney Niles Pavilion	None	Not NRE
15	228 Beaver Meadow Road	c. 1970	American Legion Lyman F. Pell Post 08	None	Not NRE
16	196 Beaver Meadow Road	c. 1945		None	Not NRE
17	186 Beaver Meadow Road	c. 1948		None	Not NRE
18	182 Beaver Meadow Road	c. 1949		None	Not NRE
19	176 Beaver Meadow Road	c. 1950		None	NRE
20	166 Beaver Meadow Road	c. 1947		None	Not NRE

<b>Building No. (see Map 2)</b>	<b>Resource Address</b>	<b>Construction Date</b>	<b>Historic Use or Name</b>	<b>Previous Survey and/or NR status</b>	<b>Recommended National Register Status</b>
21	152 Beaver Meadow Road	c. 2000		None	Not NRE
22	138 Beaver Meadow Road	c. 1946		None	Not NRE
23	124 Beaver Meadow Road	c. 1947		None	Not NRE
24	114 Beaver Meadow Road	c. 1949		None	Not NRE
25	106 Beaver Meadow Road	c. 1957		None	Not NRE

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Beers, Frederick W.

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1856 *Map of Windsor County, Vermont*. J. Chace, Troy.

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Norwich Historical Society

1973 *Early Houses of Norwich, Vermont*. The Norwich Historical Society, Norwich, Vermont.

Slafter, Edmund Farwell

1869 *The Charter of Norwich, Vermont: And Names of the Original Proprietors*. David Clapp & Son, Boston.

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U.S. Geological Survey

1956 *Aerial Single Frame Photo ID for Norwich, Vermont*. U.S. Geological Survey, Sioux Falls, South Dakota.

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2003 *Aerial photography. Norwich, Vermont*. U.S. Geological Survey, Sioux Falls, South Dakota.

2013 *Aerial photography. Norwich, Vermont*. U.S. Geological Survey, Sioux Falls, South Dakota.

United States Census Bureau

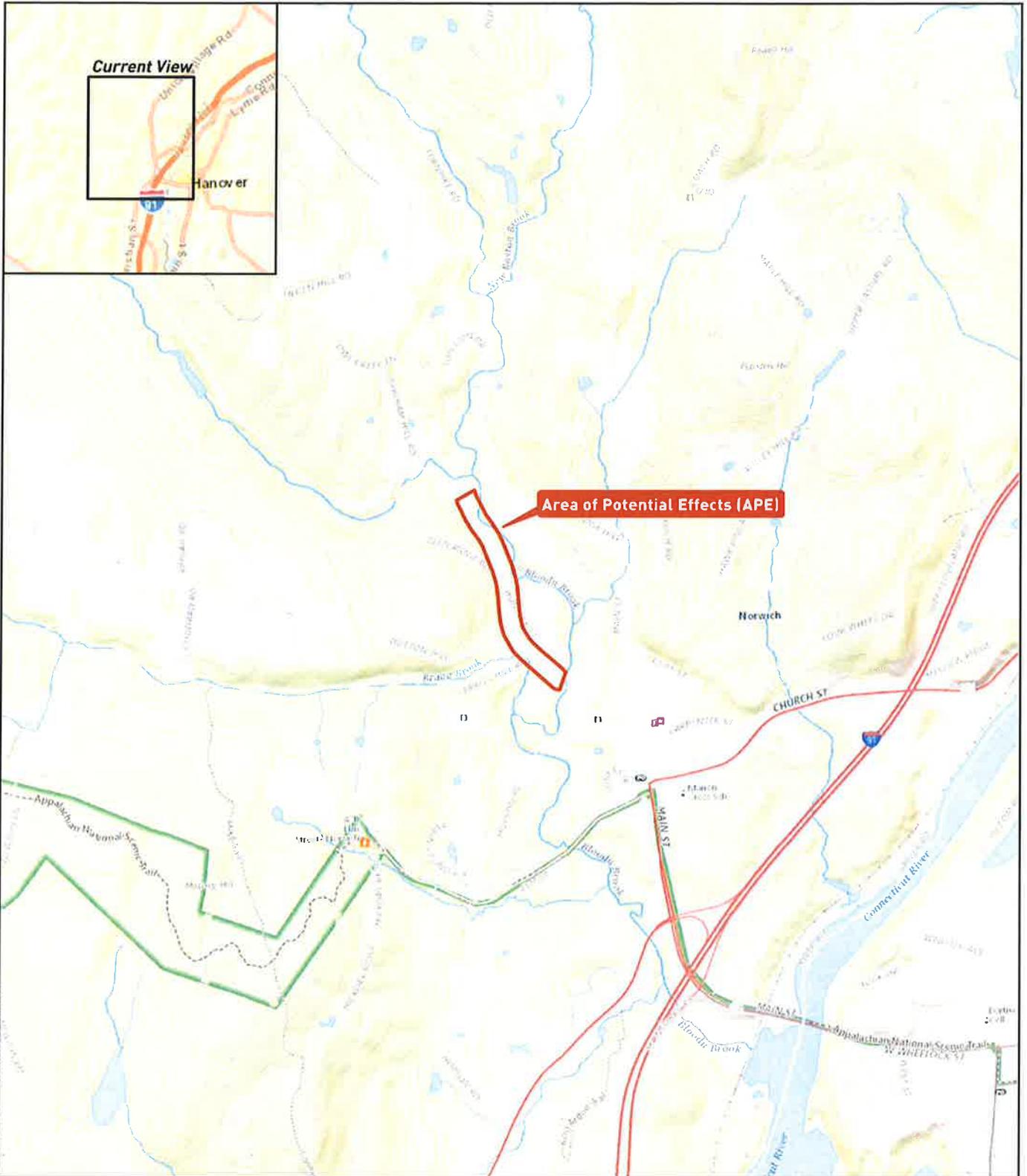
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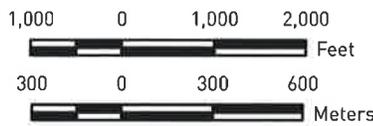
Vermont Center for Geographic Information

2020 Town of Norwich Standardized Parcel Data. Electronic document, accessed December 8, 2020, <http://maps.vermont.gov/vcgi/html5viewer/?viewer=vtmapviewer>. Electronic document, \\haa-nas2017\Library\Maps\Vermont\Washington\_County\1873\_Beers\_VT\_WashingtonCo.pdf.

## Maps



E:\5552\GIS\Documents\HAA\_5552-11\_HRI\_Map1.mxd, 12/15/2020 8:33:10 AM



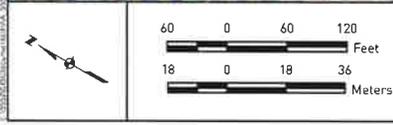
Note: Contour interval is 20 feet.

Project Location

GIS Services Accessed 12/15/2020:  
 Environmental Systems Research  
 Institute, Inc., World Street Map;  
 USGS The National Map

**HARTGEN**  
 archeological associates inc

Map 1



- Legend**
-  Photo Angle
  -  Structure
  -  Area of Potential Effects (APE)

Project Map **HARTGEN**  
geographic information systems

Vermont Center for Geographic Information, Orthoimagery, 2016-2020

**Map 2a**



**Qualifications**



archeological associates inc

## EDUCATION:

Rensselaer Polytechnic Institute  
Bachelor of Architecture May 1987  
Bachelor of Science, Building Science, May 1986

## QUALIFICATIONS:

36 CFR Part 61 Qualified Architectural Historian

## SPECIAL TRAINING:

Architectural History Consultant Training  
VDHP, Montpelier, VT, April 2019.  
Vermont Community Development Program Qualified Professionals Training  
VDHP, Montpelier, VT, September 2016.  
Evaluating Significance of Historic and Archeological Resources Workshop  
Vermont College, Montpelier, VT, May 2001  
Historic Preservation Consultant training and Section 106 training

## PROFESSIONAL EXPERIENCE:

- June 1999 – Present Senior Architectural Historian  
Hartgen Archeological Associates, Inc.  
Oversee and prepare architectural resource surveys, including pre-assessments, literature reviews and historical documentation; field reconnaissance; report and proposal preparation. Responsible for preparing documents to be reviewed by VAOT, VDHP, and USACOE, for SEQR, Section 106 and NEPA. Preparation of reports generated under ACT 250 and the FCCs Nationwide Programmatic Agreement, including preparation of forms 620 and 621.
- November 1992 – June 1999 Architectural History Consultant  
Identified, analyzed, and assessed historic structures; researched and wrote for exhibitions and publications including Historic Structures Reports; executed drawings in connection with restoration projects. Clients included Rensselaer County Historical Society; Robert Pierpont, both in Troy, NY; towns of Durham and Oak Hill, NY; Albany Institute of History and Art; Metropolitan Museum of Art; the New York Public Library, and John G. Waite Associates, Albany, NY.
- May 1984—November 1992 Junior Architect  
Worked for the Office of the New York State Architect, Wagoner & Reynolds, and in the office of Robert N. Pierpont as a Junior Architect. Responsible for restoration projects including the Governor's Mansion, the New York State Capitol, and Wilborn Temple (all in Albany, NY), and the Knickerbocker Mansion, in Schaghticoke, NY.

## PRINCIPAL PUBLICATIONS:

- 2020 "Post-Colonial New World Dutch Framing Innovations and the Development of the Balloon Frame," in James W. P. Campbell et al eds., *Proceedings of the Seventh Annual Conference of the Construction History Society*. Cambridge, England: The Construction History Society.
- 2016 "Magical Dwelling: Apotropaic Building Practices in the New World Dutch Cultural Hearth," in Christiane Bis-Worch and Claudia Theune, eds., *Ruralia XI: Religion, Cults & Rituals in the Medieval Rural Environment*. Leiden, Netherlands: Sidestone Press, 373-396.
- 2010 "Once adorned with quaint Dutch tiles...: A Preliminary Analysis of Delft Tiles Found in Archaeological Contexts and Historical Collections in the Upper Hudson Valley," in Penelope Ballard Drooker and John P. Hart, eds., *Soldiers, Cities and Landscapes: Papers in Honor of Charles L. Fisher*. New York State Museum Bulletin 513, 107-150. Albany, NY: New York State Museum.
- 2009 *Architects in Albany*. Diana S. Waite, editor. Albany, NY: Mt Ida Press/ Historic Albany Foundation. Contributed two biographical essays.
- 2005 *The Encyclopedia of New York State*, Peter Eisenstadt, editor. Syracuse, NY: Syracuse University Press, 2005. Author of entries "Philip Hooker," "Archimedes Russell," "Upright and Wing Houses," "Cobblestone Architecture," "Empire State Plaza," and "Architects and Architecture of Syracuse and Central New York."
- 2000 *The Marble House in Second Street: Biography of a Town House and its Occupants, 1825-2000*. Troy, NY: Rensselaer County Historical Society.
- 1993 *A Neat Plain Modern Style: Philip Hooker and His Contemporaries, 1796-1836*. University of Mass. Press, Amherst, Mass.

Beaver Meadow Sidewalk Extension Petition

As a resident of Norwich, I support appropriating up to \$20,600 from the Town's Sidewalk Designated Fund, for the purpose of paying the Town's share of a phase II study for extending the Town sidewalk network, specifically the section of Beaver Meadow, Alternative 1: Sidewalk West, as outlined in Dubois and King's Beaver Meadow Sidewalk Scoping Study (January 2021), contingent upon approval of grant funding from the State of Vermont

#	Timestamp	Name	Email	Address	Comments
1	1/25/2021 8:38	Vince Crow	pvcrow@gmail.com	191 Beaver Meadow	
2	1/25/2021 11:18	Alberto Rodríguez	apaniagua81@gmail.com	261 Beaver Meadow Rd	the sitewalk will provide a safer environment for everyone
3	1/25/2021 11:36	peter orner	peterorner@gmail.com	6 Moore Lane	Absolutely in favor of a a sidewalk that completes the loop, for safety, walkability, and the environment.
4	1/25/2021 13:22	Jeffrey Slayton	oldbridgeroad@hotmail.com	436 Main St Norwich, VT 05055	
5	1/25/2021 13:23	Liz Blum	elizblum@gmail.com	48 Cliff St	In favor. Please apply for bike/ped grant.
6	1/25/2021 13:23	Linda C. Gray	linda.c.gray@gmail.com	175 Kerwin Hill Rd.	
7	1/25/2021 13:25	Richard Adams	richarda82@gmail.com	394 Main ST Norwich, VT	
8	1/25/2021 13:26	Matthew Buck	matt.buck@marioncross.org	64 Carpenter Street	
9	1/25/2021 13:27	Wendy Teller-Elsberg	wendyteller@gmail.com	60 Church St., Norwich	
10	1/25/2021 13:29	Brooke Blicher	brookedmd@gmail.com	62 Union Village Rd	This is a safety concern. Please build a sidewalk to complete a safe loop around town!
11	1/25/2021 13:38	Stephanie Hamilton	stephanielovehamilton@gmail.com	387 Hawk pine road norwich VT	I support the beaver meadow side walk loop.
12	1/25/2021 13:51	Anne Hoen	anniehoen@gmail.com	1508 New Boston Road	I strongly support making Norwich a MORE ACCESSIBLE community for residents of ALL ABILITIES by expanding the sidewalk network.
13	1/25/2021 14:19	Hilary Lynch	hilarylynchlicsw@gmail.com	15 Heritage Lane unit 1	I support a sidewalk on Beaver Meadow Road.
14	1/25/2021 14:20	Jonathan Smolin	jonathan.smolin@dartmouth.edu	47 Elm Street	Yes, please! This sidewalk is desperately needed.
15	1/25/2021 14:42	Jennifer Galloway	mamajgalloway@gmail.com	24 Sargent St	
16	1/25/2021 14:50	Jessica Eakin	jceakin12@gmail.com	55 Turnpike Road	

Beaver Meadow Sidewalk Extension Petition

#	Timestamp	Name	Email	Address	Comments
17	1/25/2021 14:58	Ainsley Morse	ainsley.e.morse@gmail.com	54 Carpenter St.	It would be lovely to have a sidewalk all the way down to Moore! I am a frequent walker with small children and would like to experience less fear and stress as I/we go along.
18	1/25/2021 15:15	Leah McLaughry	l.mclaughry@gmail.com	1047 Bragg Hill Road, Norwich VT	
19	1/25/2021 15:26	Mary Riordan	marymriordan@gmail.com	59 Union Village Rd	
20	1/25/2021 17:14	Rebecca Crow	rebeccacrow@gmail.com	191 beaver meadow	Hooray! Much needed to make this a more walkable city. Safe for all.
21	1/25/2021 17:21	Becca Jones	rjones369@gmail.com	340 hawk pine road	
22	1/25/2021 17:26	Eric picconi	ericpicconi@hotmail.com	55 turnpike road, Norwich, VT 05055	
23	1/25/2021 17:26	Beckie Eakin	eakinbc12@gmail.com	356 Hawk Pine Road	
24	1/25/2021 18:09	Katie Ruth Hluchyj	katie.hluchyj@gmail.com	218 Upper Turnpike Rd. Norwich VT 05055	Yes!! Build the sidewalk!
25	1/25/2021 18:23	Ricardo Siri	liniers15@gmail.com	444 Turnpike Rd	I hope the sidewalk project works out! Safety first.
26	1/25/2021 18:33	Gered Dunne	gereddunne@gmail.com	62 Union village rd	Build that sidewalk! Please.
27	1/25/2021 18:35	Elliot Harik	elliot.harik@gmail.com	27 Hillside Rd	
28	1/25/2021 19:00	Robert Scott	scottmdphd@gmail.com	11 Trumbull Lane	
29	1/25/2021 19:04	Brian Shiner	brian.r.shiner@dartmouth.edu	53 beaver meadow road	I support infrastructure at any cost.
30	1/25/2021 19:19	Marisa Lorenzo	marisaannlorenzo@gmail.com	17 Bragg Hill Road	
31	1/25/2021 19:30	Jim Eakin	jbeakin12@yahoo.com	356 Hawk Pine Road	This project will make Norwich a safer and enjoyable place for pedestrians and runners.
32	1/25/2021 19:49	Shoshana Hort	sjh44@earthlink.net	149 Beaver Meadow Road	
33	1/25/2021 19:49	Alex Fuld	afuld@earthlink.net	149 Beaver Meadow Road	
34	1/25/2021 19:51	Jonathan Teller-Elsberg	jelsberg@gmail.com	60 Church St	
35	1/25/2021 20:26	John Langhus	johnlanghus@gmail.com	517 Union Village Rd., Norwich	The preliminary work is detailed and thoughtful. I fully support this.
36	1/25/2021 20:28	Demo Sofronas	demosofronas@gmail.com	82 Glen Ridge Rd	
37	1/25/2021 20:28	Georgia Sofronas		82 Glen Ridge Rd	

Beaver Meadow Sidewalk Extension Petition

#	Timestamp	Name	Email	Address	Comments
38	1/25/2021 20:28	Andy Scherer	andmyscherer@gmail.com	23 Hazen St	
39	1/25/2021 21:46	Ryan Gardner	ryan.gardner@gmail.com	80 Four Wheel Drive	
40	1/25/2021 21:46	Helena Gardner		80 Four Wheel Drive	
41	1/25/2021 21:47	Katie Crouch	katicrouch@mac.com	6 Moore lane	
42	1/25/2021 21:52	Omer Trajman	otrajman@gmail.com	236 Main St	
43	1/25/2021 21:52	Lily Trajman	otrajman@gmail.com	236 Main St	
44	1/25/2021 21:58	Lisa Sjostrom	lsjostrom13@gmail.com	422 Main Street	
45	1/25/2021 21:59	Elizabeth Gemignani	egemignani@gmail.com	450 New Boston Road	
46	1/25/2021 21:59	Anthony Gemignani		450 New Boston Road	
47	1/25/2021 22:29	Thomas Cochran	tmcteach@gmail.com	91 Stagecoach Road	We strongly support the proposal to construct a sidewalk adjacent to Beaver Meadow Road.
48	1/25/2021 22:29	Tanya Cluff		91 Stagecoach Road	
49	1/25/2021 22:32	Laura Brown	laurabrowngray@gmail.com	723 Turnpike RD	
50	1/25/2021 22:32	Chris Gray		723 Turnpike RD	
51	1/26/2021 3:59	Mark Laser	mark.laser@dartmouth.edu	43 Hazen Street, Apt. 2, Norwich, VT 05055	Can't think of a better way to invest in our community!
52	1/26/2021 7:19	Liz Blum	kasfir@dartmouth.edu	48 Cliff St.	
53	1/26/2021 7:19	Nelson Kasfir		48 Cliff St.	
54	1/26/2021 7:33	Kate Barlow	katebarlow8@gmail.com	328 Turnpike Road	We need to make Norwich a safe, pedestrian town. Closing the loop is the way to go. Let's look out for our most senior and most junior town members and keep them safe as they stroll/bike/run around town.
55	1/26/2021 7:33	David Barlow		328 Turnpike Road	
56	1/26/2021 7:40	Angelica del campo	angiedelcampo@gmail.com	444 turnpike rd	Sidewalks are beneficial for health, community, safety, children able to walk more
57	1/26/2021 7:48	Grace McLaughry	grace.mclaughry@gmail.com	522 Turnpike Rd	
58	1/26/2021 8:20	Patricia Piotrowski	patty.piotrowski@gmail.com	yes, I support the sidewalk study	This is important work, and the timing will best be prioritized after the septic system and school budget needs.

Beaver Meadow Sidewalk Extension Petition

#	Timestamp	Name	Email	Address	Comments
59	1/26/2021 8:26	Chad Morig	chadmorig@hotmail.com	73 Huntley Street	<p>I am writing in support of the study and the project.</p> <p>Completing the dedicated pedestrian pathway for "the loop" around BM, Main, Turnpike and Moore will enable not only a safer route for citizens on the path but for those who would need to travel that path to get to the village.</p> <p>Norwich holds a commitment to the health and wellness of its citizens and this effort is in direct correlation with that goal. Not only will it promote more use, and safe exercise potential but will also enable our children to extend their territory and travel at an earlier age and to gain quicker access to establishing independence in their lives.</p> <p>With the overwhelming majority of the costs being supported by the state's grant funding, should it be made available, this is the time to complete this path that has long since been perceived to be missing from a route travelled by so many.</p> <p>Thank you for the consideration.</p>
60	1/26/2021 8:26	Stacy Morig		73 Huntley Street	
61	1/26/2021 8:34	Jacqueline Allen	allenjaci@gmail.com	15 Union Village Road	Worthwhile investment
62	1/26/2021 8:40	Kathryn Kelly	kathrynkelly55@hotmail.com	182 Beaver Meadow Road Norwich VT	

Beaver Meadow Sidewalk Extension Petition

#	Timestamp	Name	Email	Address	Comments
63	1/26/2021 8:40	Kevin Thompson		182 Beaver Meadow Road Norwich VT	
64	1/26/2021 8:48	Peter W. Griggs	petergriggs55@gmail.com	256 Dutton Hill Rd, Norwich, VT	I feel as though this project is long overdue. Let's get it done.
65	1/26/2021 8:48	Sydney E. Smith		256 Dutton Hill Rd, Norwich, VT	
66	1/26/2021 8:49	Marlene McDonald	marlene.mcdonald@gmail.com	44 Glen Ridge Rd	a sidewalk extension would enhance pedestrian safety and improve access to village residents
67	1/26/2021 8:49	Carl Tracy		44 Glen Ridge Rd	
68	1/26/2021 9:12	Jennifer Hopkins	jshaevelhopkins@gmail.com	47 Carpenter Street Norwich	
69	1/26/2021 9:29	Kata Sasvari	katasasvari@gmail.com	55 Starlake Lane	
70	1/26/2021 9:29	Jeremiah Stocking		55 Starlake Lane	
71	1/26/2021 9:40	Jenny Barba	jennyhbarba@gmail.com	1037 Turnpike Rd. Norwich, VT 05055	
72	1/26/2021 9:40	Christopher Coughlin		1037 Turnpike Rd. Norwich, VT 05055	
73	1/26/2021 9:50	Todd Thompson	todd.g.thompson@dartmouth.edu	31 Dutton Hill Rd. Norwich, VT	
74	1/26/2021 9:50	Leslie Dustin		31 Dutton Hill Rd. Norwich, VT	
75	1/26/2021 9:50	Helen Thompson		31 Dutton Hill Rd. Norwich, VT	
76	1/26/2021 9:54	Katie Osgood	katielisa@msn.com	217 Town Farm Rd. Norwich VT 05055	
77	1/26/2021 9:54	Seth Osgood		217 Town Farm Rd. Norwich VT 05055	
78	1/26/2021 11:00	Michele Tine	michele.tine@dartmouth.edu	19 Bragg Hill Rd	Thanks to all of the thoughtful planning. I am an enormous advocate of a sidewalk.
79	1/26/2021 11:01	alicia green	aliciagreen802@gmail.com		
80	1/26/2021 11:03	john laser	jake.e.laser@gmail.com		
81	1/26/2021 11:12	Gwendolyn Thompson	gwendolynthompson68@gmail.com	377 Main Street	
82	1/26/2021 11:12	Natalie Junio- Thompson		377 Main Street	

Beaver Meadow Sidewalk Extension Petition

#	Timestamp	Name	Email	Address	Comments
83	1/26/2021 11:15	Peter Milliken	peter@tuckermancap tal.com	160 Happy Hill Road	We absolutely support this project.
84	1/26/2021 11:15	Ashley Milliken		160 Happy Hill Road	
85	1/26/2021 11:17	Troy McBride	mcbridetroy@gmail.com	175 Hawk Pine	It would be great to have a safer loop. This is a widely used loop for walking and running.
86	1/26/2021 11:19	Stephen Pidgeon	sjpidgeon@gmail.com	66 Willey Hill Road	The Norwich loop is a lovely walk, run or bike ride, but every time I do it I worry about safety. I also worry when I drive that I will hit someone, especially in low light conditions.
87	1/26/2021 11:20	Katharine Navins	kmacstudio@mac.com	211 Brigham Hill Road	
88	1/26/2021 11:20	David Navins		211 Brigham Hill Road	
89	1/26/2021 11:21	Emma Cottage	ejcottage@hotmail.com	484 Main St, Apt 1, Norwich.	
90	1/26/2021 11:22	Noah Skogerboe	skog0107@Uumn.edu	435 Bragg Hill Rd. Norwich	This would be a tremendous benefit to our community, especially to this ethat are challenged in mobility or safety either by age or liability!
91	1/26/2021 11:32	Pippa Lilienthal	pippa.gage@gmail.com	39 Brookside Drive, Norwich VT 05055	we support the sidewalk
92	1/26/2021 11:32	Mark Lilienthal		39 Brookside Drive, Norwich VT 05055	
93	1/26/2021 11:56	Elizabeth Phelps	lizwphelps@gmail.com	8 Hopson Road	I never run along the section of Beaver Meadow with no sidewalk with my jogging stroller. Who wants to push a stroller in the road? This would be a really nice resource.
94	1/26/2021 11:56	Jess Phelps		8 Hopson Road	
95	1/26/2021 12:00	Catherine Bucci	bucci.katie@gmail.com	829 Turnpike Road, Norwich, VT 05055	
96	1/26/2021 12:03	Rob Adams	robadams96@gmail.com	96 Elm Street	
97	1/26/2021 12:05	Steelie Korpi	steelie.korpi@gmail.com		
98	1/26/2021 12:05	Ethan Korpi	steelie.korpi@gmail.com		

Beaver Meadow Sidewalk Extension Petition

#	Timestamp	Name	Email	Address	Comments
99	1/26/2021 12:06	b. Justin Krawitt	jkrawitt@yahoo.com	752 Bragg Hill Road	I exit Bragg on to Beaver Meadow daily and the number of pedestrians appears sufficient to support a sidewalk. I think using statistics alone not sufficient in the decision as it fails to account for evasive actions by pedestrians, cyclists, and drivers not to mention anxiety inducing events. Near misses in other settings are enough to promote inquiries, but there is not tracking measure here other than accidents that I am aware of.
100	1/26/2021 12:06	Jennifer Krawitt		752 Bragg Hill Road	
101	1/26/2021 12:23	Gina des Cognets	ginacdescognets@gmail.com	427 hawk pine rd	This would be a tremendous addition to Norwich
102	1/26/2021 12:45	Stephanie McCaull	smccaull@gmail.com	28 Goddard Road	
103	1/26/2021 12:45	Philip McCaull		28 Goddard Road	
104	1/26/2021 12:55	Matthew Babineau	mbabinea@gmail.com	46 Douglas Lane	
105	1/26/2021 12:55	Mark Melamut	rabbimarkm@gmail.com	524 Turnpike Rd	
106	1/26/2021 13:04	Danielle Cohen	daniellercohen@gmail.com	66 Willey Hill, Norwich VT 05055	
107	1/26/2021 13:04	Lauren Pidgeon		66 Willey Hill, Norwich VT 05055	
108	1/26/2021 13:21	Philip McCaull	Philip.mccaull@gmail.com	28 Goddard Rd	We are 100% in support of the sidewalk project. It would be a wonderful addition to the town and make Norwich much safer.
109	1/26/2021 13:21	Stephanie McCaull		28 Goddard Rd	
110	1/26/2021 13:31	Sasha Cahoon	sashacahoon@gmail.com	360 New Boston Rd	
111	1/26/2021 13:45	David Barlow	Barlowpt@gmail.com	In Favor - Vote yes	
112	1/26/2021 14:03	Erin Butler	erin.elizabeth.butler@gmail.com	295 Hawk Pine Rd, Norwich	
113	1/26/2021 14:04	Bernard Haskell	Bernard.w.haskell@dartmouth.edu	17 sargent st	well needed
114	1/26/2021 14:04	Sandy Haskell		17 sargent st	

Beaver Meadow Sidewalk Extension Petition

#	Timestamp	Name	Email	Address	Comments
115	1/26/2021 14:15	Susan Hardy	susan.e.hardy@me.com	122 Kendall Station Road	Anything that promotes safe walking and/or bicycling over car travel has my full support.. We need to move away from a car-centric view of roadways, especially in more populated areas.
116	1/26/2021 14:19	Kathryn Doherty	kathryn.l.doherty@gmail.com	31 Turnpike Rd/	
117	1/26/2021 15:04	Paul Manganiello	paul.d.manganiello@dartmouth.edu	226 turnpike rd.	I support moving forward with the sidewalk project
118	1/26/2021 15:20	Leslie Dustin	lesliedust@gmail.com	31 Dutton Hill Road	We absolutely support building a sidewalk along the field-side of Beaver Meadow Road. So, so many people and cars brush past each other on this busy passage. Time for some safety!
119	1/26/2021 15:20	Todd Thompson		31 Dutton Hill Road	
120	1/26/2021 15:20	Rosey Thompson		31 Dutton Hill Road	
121	1/26/2021 15:28	Teja Chatty	tejaswini.chatty.th@dartmouth.edu	377 Main Street, Norwich, VT 05055	
122	1/26/2021 15:37	Gena Heng			
123	1/26/2021 15:59	Jon Felde	felde@post.harvard.edu	70 Koch Road	
124	1/26/2021 16:01	Terry mcdonnell	t.p.mcd@comcast.net	15 Brigham hill rd	Let's do this
125	1/26/2021 16:01	Katie mcdonnell		15 Brigham hill rd	
126	1/26/2021 16:13	Colleen Fox	cjfoxh@yahoo.com	308 Beaver Meadow Rd	
127	1/26/2021 16:23	Hayley DeLugach	hdelugach@gmail.com	524 Turnpike Rd	I support the sidewalk project! Thank you.
128	1/26/2021 16:23	Mark Melamut		524 Turnpike Rd	
129	1/26/2021 16:29	Diana Rojas	dimaroso@gmail.com	261 Beaver Meadow Rd	Pedestrians and motorized vehicles should not share a way. It is a primordial safety issue.
130	1/26/2021 16:59	Jonathan Rosenbloom	jrosenbloom@vermontlaw.edu	128 Turnpike Road	<i>See Extra Comments below</i>
131	1/26/2021 17:09	Adam pearce	xadampearcex@gmail.com	221 Hopson rd	
132	1/26/2021 17:09	Kyla pearce		221 Hopson rd	
133	1/26/2021 17:09	Bear Pearce		221 Hopson rd	
134	1/26/2021 17:14	Margaret Lukovits	mlukovits@comcast.net	186 Hopson Rd , Norwich	

Beaver Meadow Sidewalk Extension Petition

#	Timestamp	Name	Email	Address	Comments
135	1/26/2021 17:14	Tim Lukovits		186 Hopson Rd , Norwich	
136	1/26/2021 17:14	Karina Lukovits		186 Hopson Rd , Norwich	
137	1/26/2021 17:19	John Cushman	jack.cushman@mac.com	69 Meetinghouse Road	I think more sidewalks are generally beneficial and more study is worthwhile.
138	1/26/2021 17:35	Erin DeSilva	erindesilva@gmail.com	11 Eagle Lane	
139	1/26/2021 17:35	Jerry DeSilva		11 Eagle Lane	
140	1/26/2021 19:38	Raphael Adamek	raphadamek@gmail.com	247 New Boston Rd	
141	1/26/2021 19:38	Julia Lau		247 New Boston Rd	
142	1/26/2021 19:41	Jane Finlay	jfinlay26@gmail.com	455 Hawk Pine Rd. Norwich VT 05055	I think a sidewalk there would be safer and helpful.
143	1/26/2021 19:41	Charlie Lindner		455 Hawk Pine Rd. Norwich VT 05055	
144	1/26/2021 20:20	Alexa Manning	findalexa@yahoo.com	442 Main St	
145	1/26/2021 20:20	James Manning		442 Main St	
146	1/26/2021 22:27	Emily Scherer	blood.emily@gmail.com	23 Hazen St.	
147	1/27/2021 6:22	Emily Newick	enewick@gmail.com	Cliff St, Norwich	
148	1/27/2021 6:31	Ingrid Svensborn	isvensborn@gmail.com	15 heritage lane 2	Yes to sidewalk extension
149	1/27/2021 7:37	Oona Gardner	oonagardner@gmail.com	107 Spring Pond Rd	
150	1/27/2021 8:42	Amy Nartowicz	amy.nartowicz@gmail.com	28 Turnpike Road	
151	1/27/2021 8:43	Jane LeMasurier	jane.lemasurier@gmail.com	37 Goodrich 4 Corners Rd Norwich, VT 05055	
152	1/27/2021 8:43	Graham Webster		37 Goodrich 4 Corners Rd Norwich, VT 05055	
153	1/27/2021 9:27	Nathaniel Robbins	nathanielrobbins@gmail.com	32 elm st	
154	1/27/2021 9:27	Kathleen Mellor		32 elm st	

Extra  
Comments:

Beaver Meadow Sidewalk Extension Petition

#	Timestamp	Name	Email	Address	Comments
		Jonathan Rosenbloom			<p>Thank you for pushing this forward. It is a great move for a great community. Last year, I completed a sustainable zoning code chapter on pedestrian mobility. One recommendation concluded: "A significant benefit of increasing walkability of a community is the health benefits physical activity can provide. Increased physical activity correlates with areas that have high amounts of connectivity.[14] Adults who live in more pedestrian friendly areas walk around 30 minutes more per week than those who live in less pedestrian friendly areas.[15] By encouraging walking as opposed to driving, these ordinances can help reduce the risk of a number of health problems, such as heart attacks, obesity, stroke, heart disease and high blood pressure.[16]</p> <p>Promoting walking can also have large environmental benefits. A significant amount of air pollution in more urbanized areas comes from motor vehicles.[17] For example, if a person were to walk 1.5 miles, it "would generate less than a quarter of the GHG [greenhouse gas] that would be emitted if the person drove the same distance." [18] Implementing connectivity ordinances that allow better walkability allows local governments to reduce emissions benefitting citizen health and the environment.[19]. . . "</p> <p>Another action concluded: "Walkable communities have higher property values, resulting in higher property tax revenue for the local government.[21]"</p> <p>One last one found: "Sidewalks promote opportunities for citizens to actively connect with their communities, create safer pedestrian environments, and produce healthier neighborhoods.[12]". Something that will be much needed post-COVID.</p> <p>All citations and quotes can be found at: <a href="https://sustainablecitycode.org/chapter/chapter-4/4-3/">https://sustainablecitycode.org/chapter/chapter-4/4-3/</a></p>

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**Town of Norwich**  
P.O. Box 376  
Norwich VT 05055-0376  
(802) 649-1419 Ext. 101 or 102

**APPLICATION FOR BOARDS/COMMISSIONS**  
*(and for those reapplying for continuing appointments)*

Name: Chet Clem

Address: 1488 Beaver Meadow Road, Norwich, VT 05055

Day phone: 603-676-7106

Evening phone: 207-576-1410

E-mail: chet@lymeproperties.com

Position Applied For: CT River Joint Commissions – Upper Valley River

1. If you are re-applying for the same board/commission, how many terms/years have you already served?

Terms: N/A

Years:

2. Would you be available for evening and/or morning meetings?

Evening: (Yes  No ) Morning: (Yes  No )

Are there other restrictions on your availability? If so, please describe:

3. Please list any experiences, skills and/or qualifications which you feel would especially suit you for this appointment.

I received my BA in Environmental Policy from Bates College, having completed my senior thesis on the FERC hydroelectric dam relicensing process as an opportunity for environmental, recreational, and economic revitalization. As part of that I interned on the Penobscot River Restoration Project, a coalition of 6 NPOs and the Penobscot Indian Nation working with FERC and the dam owner to develop a new collaborative model for dam removal, increased efficiency of energy production, and ecological and recreational improvements. That multi-stakeholder approach continues to inform the work I do now, including the work at River Park West Lebanon (the riverfront parcel having been deeded for publicly accessible recreational use and now owned by a NH not-for-profit) and with the West Lebanon Revitalization Committee.

4. Please include service on other municipal or school district Boards, Commissions, or Committees both in Norwich and elsewhere and indicate whether or not any of those appointments are current ones:

Vice-Chair, West Lebanon Revitalization Advisory Committee (Current)

5. Education and Current Employment

Name of Company: Lyme Properties  
Title: President

Location: West Lebanon, NH

Describe your work:

Lyme Properties is nationally known for excellence in urban design, green buildings, and adaptive re-use with a portfolio of mixed-use projects that have established pioneering standards for architecture, land use, and conservation. We focus on the master planning, permitting, and execution of transformative real estate projects, grounded in a philosophy of community-conscious development.

6. Pertinent Education and/or Experience:

BA, Environmental Policy, Bates College, 2005  
MBA, Entrepreneurship, Babson College, 2013

7. Do you feel there could be *any conflict of interest* with your personal beliefs, occupation or employer in serving on this board, commission or committee? (Yes  No)  If yes, please explain:

Comments:

I believe the opportunity to serve on this committee dovetails with my personal interests, educational background, and professional focus. I look forward to the chance to serve the town I grew up in, and the river that I grew up alongside and continue to be an active recreational user of.

Signature



Date

1/22/2021

**Town of Norwich**  
P.O. Box 376  
Norwich VT 05055-0376  
(802) 649-1419 Ext. 101 or 102

**APPLICATION FOR BOARDS/COMMISSIONS**  
*(and for those reapplying for continuing appointments)*

Name: John Cushman

Address: Meetinghouse Road

Day phone: 6033067017

Evening phone: 8026497073

E-mail: jack.cushman@mac.com

Position Applied For: Appointment to Solid Waste Committee

- 1. If you are re-applying for the same board/commission, how many terms/years have you already served?

Terms:

Years:

- 2. Would you be available for evening and/or morning meetings?

Evening: (Yes  No  Morning: (Yes  No

Are there other restrictions on your availability? If so, please describe:

I am retired and have ample time but I have other sporadic meetings.

- 3. Please list any experiences, skills and/or qualifications which you feel would especially suit you for this appointment.

I have an extensive background in environmental matters as a journalist. Stror

- 4. Please include service on other municipal or school district Boards, Commissions, or Committees both in Norwich and elsewhere and indicate whether or not any of those appointments are current ones:

5. Education and Current Employment

Name of Company: retired

Location:

Title:

Describe your work:

6. Pertinent Education and/or Experience:

BA Dartmouth College 1976

7. Do you feel there could be *any conflict of interest* with your personal beliefs, occupation or employer in serving on this board, commission or committee? (Yes  No)  If yes, please explain:

Comments:

I have heard that the Select Board needs a third member of the municipal solid waste comm

Signature

John Cushman

Date

January 29, 2021

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Town of Norwich Stop / Yield Sign Ordinance											
<b>Date:</b>	12/11/2020										
<b>Reference:</b>	Town of Norwich Ordinance <u>update</u> of: "An ordinance to regulate local law enforcement of stop and yield signs and other traffic control devices in the town of Norwich"										
<b>Author:</b>	Chief Jennifer Frank   Norwich Police Department										
<b>Purpose:</b>	Ordinances should be regularly reviewed to confirm that they are accurate, free of conflicts and/or internal inconsistencies, and conform to state law.										
<b>Impact:</b>	The successful passage of this updated ordinance does <u>not</u> change or modify any of the existing stop or yield sign intersections. It <u>updates</u> the ordinance to more accurately reflect the current signage in place and serves as a piece of legislation enacted by a municipal authority, that takes the current codification enacted by the town on November 15, 2017, and makes the content legally enforceable.										
<b>History:</b>	<p>In 1972, the Selectboard passed the "Highway Intersection Ordinance" which designated and established the "stop" and "yield" intersections: 13 stop signs and 4 yield signs were listed.</p> <p>In 2017, the Town of Norwich attempted to codify its ordinances, copying the original language with additions: 17 stop signs, 2 yield signs were listed. The codification itself is not an ordinance, but an amalgamation of ordinances.</p> <p>NPD conducted a drive through survey of the town completing a full sign inventory, which has additionally been shared with the Department of Public Works and the Zoning and Planning Office.</p> <p>The current ordinance is not reflective of the actual signage in place: 72 stop signs, 8 yield signs</p> <p>An updated ordinance allows for a greater percentage of the fine collected to be returned back to the Town of Norwich, as opposed to State funds.</p>										
<b>Regulations:</b>	<table border="1"> <thead> <tr> <th>Requirement</th> <th>Reference</th> </tr> </thead> <tbody> <tr> <td>A traffic ordinance can be passed by the local governing body unless petitioned by at least 5% of the people, in which case a vote of the townspeople is also required. A petition for a vote on the question of disapproving an ordinance or rule shall be signed by not less than five per cent of the qualified voters of the municipality, and presented to the legislative body or the clerk of the municipality within 44 days following the date of adoption of the ordinance or rule by the legislative body.</td> <td> <ul style="list-style-type: none"> <li>■ Setting Speed Limits- A Guide for Vermont Towns. Vermont Local Roads Program, Berlin, VT, August 2016.</li> <li>■ 24 V.S.A. § 1973</li> </ul> </td> </tr> <tr> <td>Entered in the minutes of the municipality</td> <td>■ 24 V.S.A. § 1972</td> </tr> <tr> <td>Posted in at least five conspicuous places within the municipality.</td> <td>■ 24 V.S.A. § 1972</td> </tr> <tr> <td>The legislative body shall arrange for one formal publication of the ordinance or rule or a concise summary thereof in a newspaper circulating in the municipality on a day not more than 14 days following</td> <td>■ 24 V.S.A. § 1972</td> </tr> </tbody> </table>	Requirement	Reference	A traffic ordinance can be passed by the local governing body unless petitioned by at least 5% of the people, in which case a vote of the townspeople is also required. A petition for a vote on the question of disapproving an ordinance or rule shall be signed by not less than five per cent of the qualified voters of the municipality, and presented to the legislative body or the clerk of the municipality within 44 days following the date of adoption of the ordinance or rule by the legislative body.	<ul style="list-style-type: none"> <li>■ Setting Speed Limits- A Guide for Vermont Towns. Vermont Local Roads Program, Berlin, VT, August 2016.</li> <li>■ 24 V.S.A. § 1973</li> </ul>	Entered in the minutes of the municipality	■ 24 V.S.A. § 1972	Posted in at least five conspicuous places within the municipality.	■ 24 V.S.A. § 1972	The legislative body shall arrange for one formal publication of the ordinance or rule or a concise summary thereof in a newspaper circulating in the municipality on a day not more than 14 days following	■ 24 V.S.A. § 1972
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	<p>the date when the proposed provision is so adopted, the name of the municipality's website, if the municipality actively updates its website on a regular basis; the title or subject of the ordinance or rule; the name, telephone number, and mailing address of a municipal official designated to answer questions and receive comments on the proposal; and where the full text may be examined. The same notice shall explain citizens' rights to petition for a vote on the ordinance or rule at an annual or special meeting as provided in section 1973 of this title</p>	
	<p>Unless a petition is filed in accordance with section 1973 of this title, the ordinance or rule shall become effective 60 days after the date of its adoption, or at such time following the expiration of 60 days from the date of its adoption as is determined by the legislative body</p>	<p>■ 24 V.S.A. § 1972</p>

Town of Norwich  
PO Box 376  
Norwich, VT 05055-0376

**AN ORDINANCE TO REGULATE LOCAL ENFORCEMENT OF STOP AND YIELD SIGNS AND OTHER TRAFFIC CONTROL DEVICES IN THE TOWN OF NORWICH**

**1. AUTHORITY**

1.1 This ordinance is adopted under the authority granted and provisions of Title 23, Section 1008; Title 24, Section 229(4); and Title 24, Chapter 59, of the Vermont Statutes Annotated.

1.2 Pursuant to the provisions of Title 23, Vermont Statutes Annotated, Section 1007 and 1008, and Title 24, Vermont Statutes Annotated Sections 1971-1973 and 2291(1)(4) and (5), and such other general enactments as may be material hereto, it is hereby ordained by the Board of Selectmen of the Town of Norwich that the following ordinance is adopted for the Town of Norwich, Vermont.

**2. PURPOSE AND SCOPE**

2.1 The purpose of this ordinance, adopted in accordance with 23 V.S.A. § 13 and 19 V.S.A, is to regulate traffic in order to protect public health, safety and welfare.

2.2 This ordinance establishes special traffic regulations on public highways within the Town of Norwich, Vermont.

**3. DEFINITIONS**

3.1 For purposes of this Ordinance, the following words and/or phrases shall be defined as identified below to include the definitions of Title 23, Vermont Statutes Annotated, Section 4, which have been incorporated by reference.

- A. **Highway**: shall be as defined in Title 19, Sections 1 (12) and (21), of the Vermont Statutes Annotated.
- B. **Intersection**: shall mean any point at which a highway joins another at an angle, whether or not it crosses the other.
- C. **MUTCD**: shall mean the Manual for Uniform Traffic Control Devices, issued by the Federal Highway Administration.
- D. **Stop Sign**: shall mean a sign as described in the M.U.T.C.D., Section 2B.04 and Title 23, Sections 1025 and 1048(b), of the Vermont Statutes Annotated.
- E. **Town Highway**: shall be as defined in Title 19, Sections 1 (12) and (21), of the Vermont Statutes Annotated.
- F. **Yield Sign**: shall mean a sign as described in the M.U.T.C.D., Section 2B.04, and Title 23, Sections 1025 and 1048(c), of the Vermont Statutes Annotated.

#### **4. ESTABLISHMENT OF "STOP" INTERSECTIONS**

4.1 The following intersections of Town highways are hereby designated as "stop intersections" pursuant to 23 V.S.A. § 1008:

<b>Street</b>	<b>Intersection</b>
Academy Road	VT Route 132
Beaver Meadow Road	Main Street
Blood Hill East	Goodrich Four Corners Road
Bowen Road	VT Route 132
Bragg Hill Road	Beaver Meadow Road
Brigham Hill Road	Beaver Meadow Road
Butternut Road	US Route 5 North
Campbell Flats Road	VT Route 132
Carpenter Street	Church Street
Carpenter Street	Main Street
Chapel Hill Road (North)	Beaver Meadow Road
Chapel Hill Road (South)	Beaver Meadow Road
Church Street	Main Street
Douglas Road	New Boston Road
Dutton Hill Road (East)	Bragg Hill Road
Elm Street	Hopson Road
Elm Street	Main Street
Farrell Farm Road	US Route 5 North
Four Wheel Drive	Church Street
Goddard Road	Bragg Hill Road
Goodrich Four Corners Road	Union Village Road
Hawk Pine Road	Willey Hill Road
Hawk Pine Road	NE intersection of Hawk Pine Circle
Hazen Street	Main Street (East)
Hazen Street	Main Street (West)
Hemlock Road	US Route 5 North
Hogback Road	VT Route 132
Hopson Road	Beaver Meadow Road
Hopson Road	Elm Street
Hopson Road	US Route 5 South
Huntley Road	Beaver Meadow Road
Jones Circle	Elm Street
Kendall Station Road	US Route 5 North
Kerwin Hill Road	VT Route 132
Koch Road	Main Street
Ladeau Road	Norford Lake Road
Lary Lane	Carpenter Street
Loveland Road	US Route 5 North
Main Street	Church Street
Maple Hill Road	Union Village Road

Maple Hill Road	Upper Pasture Road
Maple Hill Road	Willey Hill Road
Mckenna Road	VT Route 10A
Mitchell Brook Road	Beaver Meadow Road
Mill Road	US Route 5 South
Montshire Road	VT Route 10A
Moore Lane	Beaver Meadow Road
Moore Lane	Turnpike Road
Needham Road	Turnpike Road
New Boston Road	Turnpike Road
Norford Lake Road	New Boston Road
Old Bridge Road	US Route 5 North
Old Coach Road	Union Village Road
Partridge Hill	Union Village Road
Pattrell Road	Union Village Road
River Road	Church Street
Rock Ledge Lane	New Boston Road
Starlake Lane	Farrell Farm Road
Stevens Road	US Route 5 North
Stowell Road	Norford Lake Road
Sugarhouse Road	Beaver Meadow Road
Tilden Hill Road	Turnpike Road
Trumbull Lane	Main Street
Turnpike Road	Main Street
Union Village Road	VT Route 132
Upper Loveland Road	Church Street
Upper Turnpike Road	Turnpike Road
VT Route 132	US Route 5 North
Waterman Hill Road	Union Village Road
Willey Hill Road	Union Village Road
Willey Hill Road	Maple Hill Road

**5. ESTABLISHMENT OF “YIELD” INTERSECTIONS**

5.1 The following intersections are hereby established as "Yield Right-of-Way" intersections:

<b>Street</b>	<b>Intersection</b>
Bradley Hill Road	Union Village Road
Glen Ridge Road	Beaver Meadow Road
Huntley Street	Beaver Meadow Road
Needham Road	Upper Turnpike Road
River Road	US Route 5 North
Tucker Hill Road	Beaver Meadow Road
VT Route 132	Union Village Road

5.2 Establishment of additional "Stop" or "Yield" intersections. The Selectboard may establish other "Stop" or "Yield" Intersections on designated throughways as it deems necessary by the recording of a description of the intersection and its designation in the Town Clerk's Office. The designation shall become effective upon compliance with 19 V.S.A. Section 27 and 28, as amended, or such other applicable law in effect at that time.

5.3 Traffic regulation. Except when directed to proceed by an enforcement officer every driver of a vehicle approaching a stop intersection indicated by a stop sign shall stop at a clearly marked stop line, but if none, before entering the crosswalk on the near side of the intersection, or, if none, then at the point nearest the intersecting roadway where the driver has a view of approaching traffic on the intersecting roadway before entering the intersection. After having stopped, the driver shall yield the right-of-way to any vehicle which has entered the intersection from another highway or which is approaching so closely on said highway as to constitute an immediate hazard during the time when such driver is moving across or within the intersection.

## **6. PENALTIES AND FINES:**

6.1 This is a civil ordinance which shall be enforced in accordance with the provisions in 24 V.S.A. § 1974(a) and § 1977 et seq.

6.2 A violation of any provision of this Ordinance shall constitute a traffic offense and a penalty shall be imposed, as provided by 23 V.S.A. § 2302(a) (4).

6.3 The provisions of this ordinance shall be cumulative to the fullest extent permitted by law with respect to all other statutes or ordinances now or hereafter adopted regardless of their order of passage or enactment. The penalty for violation of any of the above shall be that set by the Judicial Bureau Waiver Penalty Schedule of Fines established by the State of Vermont as provided in VSA Title 23 Chapters 13, 23, and 24.

## **7. ENFORCEMENT**

7.1 This Ordinance may be enforced by any law enforcement officer who has been certified according to the provisions of 20 V.S.A. § 2358.

## **8. GENERAL PROVISIONS AND SEVERABILITY**

8.1 Separate Offenses: Each violation of a provision of this ordinance shall be deemed a separate offense.

8.2 Severability: The provisions of this ordinance are declared to be severable and if any provisions hereof be adjudged invalid such judgment shall not affect the validity of any other provisions.

8.3 Designation: This ordinance may be referred to as the Norwich Traffic Ordinance – Regulation of Stop and Yield Signs and other Traffic Control Devices and in a prosecution hereunder a copy of such ordinance, certified by the Town Clerk shall be prima facie evidence

thereof. An allegation that the act constituting the offense charged is contrary to a specified provision of this ordinance shall be a sufficient reference hereto.

8.4 Repeal of Prior Ordinances: Any other ordinance or traffic regulation relative to Regulation of Stop and Yield Signs and other Traffic Control Devices heretofore adopted by the Town of Norwich are hereby repealed.

8.5 No section of this Ordinance shall be construed to supersede or replace any Vermont statute.

**9. EFFECTIVE DATE, PUBLICATION AND POSTING**

9.1 This ordinance shall become effective 60 days after its adoption by the Town of Norwich Selectboard. If a petition is filed under 24 V.S.A. § 1973, that statute shall govern the taking effect of this ordinance.

9.2 Adopted by the Board of Selectmen, Town of Norwich at its meeting held on the \_\_\_\_th day of \_\_\_\_\_, 20\_\_\_\_.

\_\_\_\_\_  
Claudette Brochu, Selectboard Chair

Published: \_\_\_\_\_ on \_\_\_\_\_

Filed with Town Clerk: \_\_\_\_\_

Effective Date: \_\_\_\_\_

## **APPENDICES:**

### **Authority to adopt**

Universal Citation: 24 V.S.A. § 1971

“(a) A municipality may adopt, amend, repeal, and enforce ordinances or rules for any purposes authorized by law. (b) An ordinance or rule adopted or amended by a municipality under this chapter or under its municipal charter authority shall be designated as either criminal or civil, but not both.”

### **Procedure**

Universal Citation: 24 V.S.A. § 1972.

“(a)(1) The legislative body of a municipality desiring to adopt an ordinance or rule may adopt it subject to the petition set forth in section 1973 of this title and shall cause it to be entered in the minutes of the municipality and posted in at least five conspicuous places within the municipality. The legislative body shall arrange for one formal publication of the ordinance or rule or a concise summary thereof in a newspaper circulating in the municipality on a day not more than 14 days following the date when the proposed provision is so adopted. Information included in the publication shall be the name of the municipality; the name of the municipality's website, if the municipality actively updates its website on a regular basis; the title or subject of the ordinance or rule; the name, telephone number, and mailing address of a municipal official designated to answer questions and receive comments on the proposal; and where the full text may be examined. The same notice shall explain citizens' rights to petition for a vote on the ordinance or rule at an annual or special meeting as provided in section 1973 of this title. (2) Unless a petition is filed in accordance with section 1973 of this title, the ordinance or rule shall become effective 60 days after the date of its adoption, or at such time following the expiration of 60 days from the date of its adoption as is determined by the legislative body. If a petition is filed in accordance with section 1973 of this title, the taking effect of the ordinance or rule shall be governed by subsection 1973(e) of this title. (b) All ordinances and rules adopted by a municipality shall be recorded in the records of the municipality. (c) The procedure herein provided shall apply to the adoption of any ordinance or rule by a municipality unless another procedure is provided by charter, special law, or particular statute.”

### **Permissive referendum**

Universal Citation: 24 V.S.A. § 1973

“(a) An ordinance or rule adopted by a municipality may be disapproved by a vote of a majority of the qualified voters of the municipality voting on the question at an annual or special meeting duly warned for the purpose, pursuant to a petition signed and submitted in accordance with subsection (b) of this section. (b) A petition for a vote on the question of disapproving an ordinance or rule shall be signed by not less than five per cent of the qualified voters of the municipality, and presented to the legislative body or the clerk of the municipality within 44 days following the date of adoption of the ordinance or rule by the legislative body. (c) When a

petition is submitted in accordance with subsection (b) of this section, the legislative body shall call a special meeting within 60 days from the date of receipt of the petition, or include an article in the warning for the next annual meeting of the municipality if the annual meeting falls within the 60-day period, to determine whether the voters will disapprove the ordinance or rule. (d) Not less than two copies of the ordinance or rule shall be posted at each polling place during the hours of voting, and copies thereof made available to voters at the polls on request. It shall be sufficient to refer to the ordinance or rule in the warning by title. (e) If a petition for an annual or a special meeting is duly submitted in accordance with this section, to determine whether an ordinance or rule shall be disapproved by the voters of the municipality, the ordinance or rule shall take effect on the conclusion of the meeting, or at such later date as is specified in the ordinance or rule, unless a majority of the qualified voters voting on the question at the meeting vote to disapprove the ordinance or rule in which event it shall not take effect.”

### **Enumeration of powers**

Universal Citation: 24 V.S.A. § 2291

“For the purpose of promoting the public health, safety, welfare, and convenience, a town, city, or incorporated village shall have the following powers: ... (4) To regulate the operation and use of vehicles of every kind including the power: to erect traffic signs and signals; to regulate the speed of vehicles subject to 23 V.S.A. chapter 13, subchapter 12; to regulate or exclude the parking of all vehicles; and to provide for waiver of the right of appearance and arraignment in court by persons charged with parking violations by payment of specified fines within a stated period of time.”

### **Stop or Yield Intersections**

Universal Citation: 23 V.S.A. § 104

“(a) Preferential right of way at an intersection may be indicated by "stop" signs or "yield" signs. (b) Except when directed to proceed by an enforcement officer or traffic-control signal, every driver of a vehicle approaching a stop intersection indicated by a stop sign shall stop at a clearly marked stop line, but if none, before entering the crosswalk on the near side of the intersection, or, if none, then at the point nearest the intersecting roadway where the driver has a view of approaching traffic on the intersecting roadway before entering the intersection. After having stopped, the driver shall yield the right of way to any vehicle which has entered the intersection from another highway or which is approaching so closely on said highway as to constitute an immediate hazard during the time when such driver is moving across or within the intersection. (c) The driver of a vehicle approaching a yield sign shall in obedience to the yield sign slow down to a speed reasonable for the existing conditions and, if required for safety to stop, shall stop before entering the crosswalk on the near side of the intersection, or, if none, then at the point nearest the intersecting roadway where the driver has a view of approaching traffic on the intersecting roadway. After slowing or stopping, the driver shall yield the right of way to any vehicle in the intersection or approaching on another highway so closely as to constitute an immediate hazard during the time the driver is moving across or within the intersection.

However, if the driver is involved in a collision with a vehicle in the intersection, after driving past a yield sign without stopping, the collision shall be deemed prima facie evidence of the driver's failure to yield right of way.”

**Municipalities; payment to and liability of**

Universal Citation: 13 V.S.A. § 7251

“(a) Fines, forfeitures and penalties, imposed by the district or superior court or by the judicial bureau for violation of a village, town, or city ordinance shall be paid to the village, town or city, respectively, except for a \$12.50 administrative charge for each case which shall be retained by the state. (b) Fines, forfeitures and penalties imposed by the judicial bureau for all speeding traffic violations under subchapter 8 of chapter 13 of Title 23 entitled "speed restrictions" on state highways and for height and width violations under 23 V.S.A. § 1431 and length violations under 23 V.S.A. § 1432 on town highways resulting from the enforcement by towns within the jurisdiction of the town shall be paid to the town by the formula set forth in subsection (c) of this section except for the administrative charge according to the provisions of subsection (a) of this section which shall be retained by the state. The enforcement by towns shall be by a town law enforcement officer or a law enforcement officer by contract with the town. Such law enforcement officer shall be certified according to the provisions of 20 V.S.A. § 2358. Nothing in this section shall be construed to limit the jurisdiction of a certified law enforcement officer. The revenue that is collected by the state pursuant to enforcement of this section by a town shall be distributed annually during the first quarter of the fiscal year immediately following the fiscal year in which the fines, forfeitures and penalties are collected. (c) The allocation of revenue to the towns under the formula shall be updated annually by the state court administrator and shall provide that the revenue be distributed to those towns whose law enforcement efforts on state highways and town highways as specifically set forth in subsection (b) of this section have resulted in the imposition of the fines, forfeitures and penalties for all speeding traffic violations under subchapter 8 of chapter 13 of Title 23 entitled "speed restrictions" and for height and width violations under 23 V.S.A. § 1431 and length violations under 23 V.S.A. § 1432 provided that no town may receive more than five percent of the total revenue in any given year. The formula used for distribution shall reflect the percentage of a town's law enforcement expenditures as it relates to the town's total municipal taxing effort. The town's total municipal taxing effort shall be determined by subtracting the town's school taxes assessed from the total taxes assessed as provided each year in the annual report of the division of property valuation and review by the Vermont department of taxes. By July 31 of each year, the local legislative body of any town which had law enforcement efforts resulting in the imposition of fines, forfeitures and penalties and which wishes to participate shall submit to the court administrator the total amount of the funds spent for law enforcement in the most recently completed town fiscal year. (d) Fines, forfeitures, and penalties imposed by the judicial bureau for violations of subdivisions 352(3), (4), and (9) of this title, relating to animal cruelty that result from the enforcement by villages, towns, and cities within their jurisdiction shall be paid to the respective village, town, or city, except for a \$12.50 administrative charge for each violation which shall be retained by the state. The enforcement by villages, towns, and cities shall be by a local law enforcement officer or a

law enforcement officer by contract with the village, town, or city. Such law enforcement officer shall be certified according to the provisions of 20 V.S.A. § 2358.”

**Enforcement of civil ordinance violations**

Universal Citation: 24 V.S.A. § 1974a

“(a) A civil penalty of not more than \$800.00 may be imposed for a violation of a civil ordinance. Each day the violation continues shall constitute a separate violation. (b) All civil ordinance violations, except municipal parking violations, and all continuing civil ordinance violations, where the penalty is \$800.00 or less, shall be brought before the Judicial Bureau pursuant to Title 4 and this chapter. If the penalty for all continuing civil ordinance violations is greater than \$800.00, or injunctive relief, other than as provided in subsection (c) of this section, is sought, the action shall be brought in the Criminal Division of the Superior Court, unless the matter relates to enforcement under chapter 117 of this title, in which instance the action shall be brought in the Environmental Division of the Superior Court. (c) The Judicial Bureau, on application of a municipality, may order that a civil ordinance violation cease. (d) Civil enforcement of municipal zoning violations may be brought as a civil ordinance violation pursuant to this section or in an enforcement action pursuant to the requirements of chapter 117 of this title. (e)(1) When filed in court as an enforcement action by the municipality, municipal parking violations shall be brought as civil violations. The right to trial by jury shall not apply in such cases. (2) A person who received a criminal conviction in District Court for a municipal parking violation committed before January 1, 2005 may petition the court to seal all records in the matter. The person shall provide a copy of the petition to the State or municipal official who was the prosecuting authority on the matter in District Court. The court shall grant the petition if, after providing the prosecuting authority with an opportunity to respond, the court finds that sealing the records would serve the interests of justice.”

**Regulations in Municipalities:**

Universal Citation: 23 V.S.A. § 1008

“(a) The legislative body of a municipality may make special regulations as to the operation, use, and parking of motor vehicles, including angle parking, as to the location, design, and structure of traffic lights, as to "stop" signs and "yield right of way" signs at intersections, as to "no-passing" zones, and as to streets designated for one way traffic in the thickly settled portions of the municipality and may cause any street or highway of adequate width to be divided by appropriate markings into three or more lanes, and may, by ordinance or regulation, regulate the direction of travel and the turning of vehicles proceeding in those lanes and the passing of vehicles in one lane by overtaking vehicles in another lane, may cause markers, buttons or signs to be placed within or adjacent to intersections and thereby direct the course traveled by vehicles turning at an intersection, and when markers, buttons or signs are so placed no driver may turn a vehicle at an intersection other than as directed by the markers, buttons or signs. However, signs indicating the special regulations must be conspicuously posted in and near all areas affected. Special regulations may not be established on any state highway as defined by section 1 of Title

19. Regulations on all state highways may be made only by the traffic committee under section 1003 of this title, except that the traffic committee may authorize the legislative body of a municipality to regulate parking within a thickly settled area of a municipality, particularly described in the authorization, on state highways. The board of school directors of a union high school district may make special regulations as to the operation, use, and parking of motor vehicles within the boundaries of its school property. (b) The legislative body of a municipality may make special regulations as to the use of lights at night on motor vehicles at rest or in motion on well lighted streets. (c) Municipal motor vehicle regulations shall not duplicate or contradict any provision of this title.”

### **Complaint for municipal civil ordinance violations**

Universal Citation: 24 V.S.A. § 1977

“(a) The complaint in a municipal civil case shall be signed by the issuing municipal official. The original copy shall be filed with the Judicial Bureau, a copy shall be retained by the issuing municipal official, and two copies shall be given to the defendant. (b) The municipal official may void or amend the municipal complaint issued by that official by so marking the complaint and sending it to the Judicial Bureau. (c) The Court Administrator shall approve an appropriate summons and complaint form, pursuant to 4 V.S.A. § 1105(a), to implement the assessment of the full and waiver penalty provisions of this section.”

### **Minimum training standards; definitions**

Universal Citation: 20 V.S.A. § 2358

“(a) Unless waived by the Council under standards adopted by rule, and notwithstanding any statute or charter to the contrary, no person shall exercise law enforcement authority as a law enforcement officer without completing a basic training course and annual in-service training within a time and manner prescribed by the Council by rule. (b) The Council shall offer or approve basic training and annual in-service training for each of the following three levels of law enforcement officer certification in accordance with the scope of practice for each level, and shall determine by rule the scope of practice for each level in accordance with the provisions of this section: (1) Level I certification. (A) An applicant for certification as a Level I law enforcement officer shall first complete an off-site training program prior to entering and completing Level I basic training. Level I basic training shall include training to react to the circumstances described in subdivision (B) of this subdivision (1). (B)(i) The scope of practice of a Level I law enforcement officer shall be limited to security, transport, vehicle escorts, and traffic control, as those terms are defined by the Council, except that a Level I officer may react in the following circumstances if the officer determines that it is necessary to do any of the following: (I) protect an individual in the presence of the officer from the imminent infliction of serious bodily injury; (II) provide immediate assistance to an individual who has suffered or is threatened with serious bodily injury; (III) detain or arrest an individual who the officer reasonably believes has committed a crime in the presence of the officer; or (IV) detain or arrest an individual who the officer reasonably believes has committed a felony under Vermont law.

(ii) If a Level I officer reacts to any of the circumstances described in subdivision (i) of this subdivision (B), he or she shall call upon an officer certified to respond and assume law enforcement authority over the incident. (2) Level II certification. (A) An applicant for certification as a Level II law enforcement officer shall first complete Level II basic training and may then become certified in a specialized practice area as set forth in subdivision (B)(ii) of this subdivision (2). Level II basic training shall include training to respond to calls regarding alleged crimes in progress and to react to the circumstances described in subdivision (B)(iii) of this subdivision (2). (B)(i) Except as provided in subdivisions (ii) and (iii) of this subdivision (B), the scope of practice of a Level II law enforcement officer shall be limited to investigating the following matters: (I) [Repealed.] (II) 7 V.S.A. § 658 (sale or furnishing to minors; enabling consumption by minors); (III) 13 V.S.A. chapter 7 (advertisements); (IV) 13 V.S.A. chapter 8 (humane and proper treatment of animals); (V) 13 V.S.A. §§ 505 (fourth degree arson), 508 (setting fires), and 509 (attempts); (VI) 13 V.S.A. chapter 19, subchapter 1 (riots); (VII) 13 V.S.A. §§ 1022 (noise in the nighttime), 1023 (simple assault), 1025 (recklessly endangering another person), 1026 (disorderly conduct), 1026a (aggravated disorderly conduct), 1027 (disturbing peace by use of telephone or other electronic communications), 1030 (violation of an abuse prevention order, an order against stalking or sexual assault, or a protective order concerning contact with a child), 1031 (interference with access to emergency services), 1042 (domestic assault), and 1062 (stalking); (VIII) 13 V.S.A. chapter 35 (escape); (IX) 13 V.S.A. chapter 41 (false alarms and reports); (X) 13 V.S.A. chapter 45 (flags and ensigns); (XI) 13 V.S.A. chapter 47 (frauds); (XII) 13 V.S.A. chapter 49 (fraud in commercial transactions); (XIII) 13 V.S.A. chapter 51 (gambling and lotteries); (XIV) 13 V.S.A. chapter 57 (larceny and embezzlement), except for subchapter 2 (embezzlement); (XV) 13 V.S.A. chapter 67 (public justice and public officers); (XVI) 13 V.S.A. chapter 69 (railroads); (XVII) 13 V.S.A. chapter 77 (trees and plants); (XVIII) 13 V.S.A. chapter 81 (trespass and malicious injuries to property); (XIX) 13 V.S.A. chapter 83 (vagrants); (XX) 13 V.S.A. chapter 85 (weapons); (XXI) 13 V.S.A. § 7559(d), (e), and (f) (violating condition of release); (XXII) 18 V.S.A. §§ 4230(a) and 4230d (marijuana possession); (XXIII) 18 V.S.A. § 4231(a) (cocaine possession); (XXIV) 18 V.S.A. § 4232(a) (LSD possession); (XXV) 18 V.S.A. § 4233(a) (heroin possession); (XXVI) 18 V.S.A. § 4234(a) (depressant, stimulant, or narcotic drug possession); (XXVII) 18 V.S.A. § 4234a(a) (methamphetamine possession); (XXVIII) 18 V.S.A. § 4235(b) (hallucinogenic drug possession); (XXIX) 18 V.S.A. § 4235a(a) (ecstasy possession); (XXX) 18 V.S.A. § 4476 (drug paraphernalia offenses); (XXXI) 20 V.S.A. § 3132 (firework prohibitions); (XXXII) 21 V.S.A. § 692(c)(2) (criminal violation of stop-work order); (XXXIII) any misdemeanor set forth in Title 23 of the Vermont Statutes Annotated, except for 23 V.S.A. chapter 13, subchapter 13 (drunken driving), 23 V.S.A. § 3207a (snowmobiling under the influence), 23 V.S.A. § 3323 (boating under the influence), or 23 V.S.A. § 3506(b)(8) (operating an all-terrain vehicle under the influence); (XXXIV) any motor vehicle accident that includes property damage and injuries, as permitted by the Council by rule; (XXXV) any matter within the jurisdiction of the Judicial Bureau as set forth in 4 V.S.A. § 1102; (XXXVI) municipal ordinance violations; (XXXVII) any matter within the jurisdiction of a game warden or deputy game warden as set forth in 10 V.S.A. chapter 103, subchapter 4 (game wardens); and (XXXVIII) any matter within the scope of practice of a Level I law enforcement officer. (ii) In addition to the scope of practice permitted

under subdivision (i) of this subdivision (B), a Level II law enforcement officer may also practice in additional areas approved in writing by the Council based on a special certification or training approved by the Council. (iii) Notwithstanding the limitations set forth in subdivisions (i) and (ii) of this subdivision (B), a Level II officer may respond to calls regarding alleged crimes in progress and may react in the following circumstances if the officer determines that it is necessary to do any of the following: (I) protect an individual in the presence of the officer from the imminent infliction of serious bodily injury; (II) provide immediate assistance to an individual who has suffered or is threatened with serious bodily injury; (III) detain or arrest an individual who the officer reasonably believes has committed a crime in the presence of the officer; or (IV) detain or arrest an individual who the officer reasonably believes has committed a felony under Vermont law. (iv) If a Level II officer responds to calls regarding alleged crimes in progress or reacts to any of the circumstances described in subdivision (iii) of this subdivision (B) and that response or reaction is outside the scope of his or her scope of practice, he or she shall call upon an officer certified to respond and assume law enforcement authority over the incident. (3) Level III certification. (A) An applicant for certification as a Level III law enforcement officer shall complete Level III basic training. (B) The scope of practice of a Level III law enforcement officer shall include all law enforcement authority. (c) All programs required by this section shall be approved by the Council. Completion of a program shall be established by a certificate to that effect signed by the Executive Director of the Council. (d) [Repealed.] (e)(1) The criteria for all minimum training standards under this section shall include anti-bias training approved by the Vermont Criminal Justice Training Council and training on the State, county, or municipal law enforcement agency's fair and impartial policing policy, adopted pursuant to subsection 2366(a) of this title. (2) On or before December 31, 2018, law enforcement officers shall receive a minimum of four hours of training as required by this subsection. [Subdivision (e)(3) effective January 1, 2019.] (3) In order to remain certified, law enforcement officers shall receive a refresher course on the training required by this subsection during every odd-numbered year in a program approved by the Vermont Criminal Justice Training Council. (4) The Criminal Justice Training Council shall, on an annual basis, report to the Racial Disparities in the Criminal and Juvenile Justice System Advisory Panel regarding: (A) the adoption and implementation of the Panel's recommended data collection methods and trainings and policies pursuant to 3 V.S.A. § 168(f)(2) and (3); (B) the incorporation of implicit bias training into the requirements of basic training pursuant to this subsection; and (C) the implementation of all trainings as required by this subsection.”

## 1972 Ordinance:

### Establishment of "Stop Intersections"

The following intersections of Town highways are hereby designated as "Stop Intersections" pursuant to 19 VSA Ch. 1.

1. The intersection of Jones Circle Road with Elm Street.
2. The intersection of Hopson Road with Elm Street on both the North and South intersections.
3. Mechanic Street and Main Street.
4. Cross Road and Meadowbrook Road.
5. Partridge Hill Road and Union Village Road.
6. Willey Hill Road and Union Village Road.
7. Turnpike Road and Main Street.

8. Koch Road and Willey Hill Road
9. Koch Road and Main Street
10. The North end of Hazen Street and Main Street.
11. The South end of Hazen Street and Main Street.
12. Carpenter Street and Main Street.

### Establishment of "Yield Right-of-Way Intersections"

The following intersections are hereby established as "Yield Right-of-Way" intersections:

1. The intersection of Mechanic Street and Meadowbrook Road.
2. The intersection of Huntley Road, both intersections thereof, with Meadowbrook Road.
3. Brigham Hill Road and Beavermeadow Road.
4. Turnpike Road and New Boston Road.

Establishment of Additional "Stop" or "Yield" Intersections: The Board of Selectmen may establish other "Stop" or "Yield" Intersections as it deems necessary by the recording of a description of the intersection and its designation in the Town Clerk's Office. The designation shall become effective upon compliance with 19 VSA Section 38 & 39, as amended, or such other applicable law in effect at that time.

**Codification:**

**§ 103-7. Establishment of "stop intersections".**

The following intersections of Town highways are hereby designated as "stop intersections" pursuant to 23 V.S.A. § 1008.

<b>Streets</b>	<b>Intersections</b>
Beaver Meadow Road	Main Street
Brigham Hill Road	Beaver Meadow Road
Bragg Hill Road	Beaver Meadow Road
Carpenter Street	Main Street
Dutton Hill Road	Beaver Meadow Road
Elm Street	Hopson Road (both sides) and Main Street
Hazen Street	North and South ends of Main Street
Hopson Road	Elm Street on both the North and South intersections
Huntley Street	East end of Beaver Meadow Road
Jones Circle Road	Elm Street
Koch Road at	Main Street and Willey Hill Road
New Boston Road	Turnpike Road
Partridge Hill Road	Union Village Road

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<b>Streets</b>	<b>Intersections</b>
Turnpike Road	Main Street
Willey Hill Road	Union Village Road

**§ 103-8. Establishment of "yield right-of-way intersections".**

The following intersections are hereby established as "Yield Right-of-Way" intersections:

<b>Street</b>	<b>Intersection</b>
Bradley Hill Road	Union Village Road
Huntley Street	West end of Beaver Meadow Road

**§ 103-9. Establishment of additional "Stop" or "Yield" intersections.**

The Selectboard may establish other "Stop" or "Yield" Intersections on designated throughways as it deems necessary by the recording of a description of the intersection and its designation in the Town Clerk's Office. The designation shall become effective upon compliance with 19 V.S.A. Section 27 and 28, as amended, or such other applicable law in effect at that time.

# [Article 36] Task Force

In March 2019, the Town of Norwich advised, through the passing of Article 36, all Town Officials to “take immediate and sustained efforts to gradually and continually reduce the Town’s direct use of fossil fuels, beginning at a rate of no less than 5% per year starting in the 2019-20 and continuing until they are eliminated entirely.”

This task force will initiate work towards an aggressive and comprehensive work plan to achieve the elimination of direct fossil fuels in the Town of Norwich.

## About the Task Force

### Membership

The task force will be composed of seven (7) members, as follows:

- Two members of the energy committee, recommended by the committee and appointed by the selectboard
- One member of the finance committee, recommended by the committee and appointed by the selectboard
- One member of the planning commission, recommended by the commission and appointed by the selectboard
- One selectboard member, appointed by the selectboard
- Two community members appointed by the selectboard through their application/appointment process. The background of the applicant may include:
  - An individual with a background in environmental justice work or organizing
  - An individual with experience in greenhouse gas reduction and climate resilience strategy

The task force will report to the Selectboard and make recommendations jointly to the Board and Town Manager. The Selectboard is the appointing authority and may remove any member for good cause, including unexcused absence from two or more consecutive meetings without cause. Task force members are expected to abide by all Town of Norwich policies.

Members shall serve until the elimination of the Town's direct use of fossil fuels or otherwise by an action of the Selectboard.

## Authorities of the Task Force

The Task Force shall have the authority to:

- 1) Elect one or more members to serve as Chairperson and elect one or more members to serve as Vice Chairperson, who shall serve as Chair in the absence of the Chairperson.
- 2) Organize itself, establish committees or subcommittees, enlist the expertise of others as needed, and delegate duties for the performance of its work. All committee and subcommittee membership representing a quorum must adhere to guidelines of the Vermont Open Meeting Law.
- 3) Adopt rules of procedure for its meetings to accomplish its functions. Provision for public input shall be listed on the agenda of each meeting, and agendas and minutes taken and posted should be in a timely manner in accordance with the Vermont Open Meeting Law.
- 4) Make requests to the Town Manager's office for staff time, expertise, and documents.

Meetings that are conducted via Zoom shall include links to the video conference via phone or internet and shall include a telephone number should the Zoom link fail to open for any or all participants. The Town Manager's office will provide assistance in scheduling Zoom meetings and posting minutes.

## Task Force Scope

- 1) Coordinate efforts with the Town Manager's Green Fleet [Group], Energy Committee, Finance Committee, and other Town Committees and Commissions, as needed
- 2) Provide proposals or recommendations for the design of new policies, programs, projects, and staffing, and for modifications to existing policies, programs, projects, and staffing to the Town Manager and Selectboard for the reduction of direct fossil fuels. These proposal or recommendations may include, but are not limited to:
  - a) Organizational capacity
    - i) Help determine role of town staff in benchmarking and tracking energy performance of town-owned buildings and fleet

- ii) Recommend plans to improve town staff's commitment and awareness to fossil fuel reduction and elimination through training or communication strategies
- b) Measurement and tracking of energy performance
  - i) Create a system for benchmarking and publicly disclosing energy consumption for Town buildings and fleet, including methodologies for creating a Green House Gas inventory and fuel reporting standards
  - ii) Implement and improve the process for collecting data for buildings and vehicles. It may be proposed that benchmarking efforts, facility energy audits, cost of carbon calculations, and other current information regarding building and vehicle data may be managed in a central and accessible location
  - iii) Improve data management for building characteristics, including information regarding building insulation levels and mechanical systems
- c) Improved operations and maintenance for buildings and other capital assets
  - i) Establish guidelines that ensure that investments yield optimal results towards fossil fuel reduction. These guidelines may focus on best practices such as HVAC system controls, scheduling and set points, ventilation loads, and lighting system controls, sensors and scheduling
- d) Capital investments in building and transportation inclusive of analyses and prioritization, which may include:
  - i) Proposals for extensive energy use reductions factoring both up-front and life-cycle costs for fossil fuel reduction
  - ii) Develop a standard methodology for determining the effectiveness of capital projects, which may stem beyond life-cycle costing towards discount rate, inflation and utility rate escalations, and more
  - iii) An exemption process for investments in assets that do not reflect the goals of fossil fuel reduction but accomplish other town-wide goals or needs
- e) Pilot programs, purchases, or technologies to gather more information on certain assets before significant investment

- f) Recommendations for spending appropriate to the guidelines of the Climate Emergency Designated Fund
- 3) Submit an initial work plan to the Selectboard by August 1, 2021. An August 2021 work plan may contain items as outlined in items 1-3, but must:
- a) Define what constitutes a policy, program, or project that advances the directives of Article 36 as it relates to the Town-owned buildings and fleet.
  - b) Summarize work-to-date on Town's Green House Gas (GHS) emission and provide an analysis. Identify areas of improvement or missing data, if applicable, and identify approaches to fill in missing data, to the extent deemed possible.
  - c) Provide an engagement plan for collaborating with existing committees whose scope aligns with those of the Task Force.
  - d) Provide an audit of the Task Force's scope of work, including priorities, goals and outcomes, and key stakeholders to consult. This audit may include suggestions for climate action work beyond the Article 36's call for direct fossil fuel reduction.

*The Task Force charge can be amended or revised by action of the Selectboard.*

# [Article 36] Task Force

In March 2019, the Town of Norwich advised, through the passing of Article 36, all Town Officials to "take immediate and sustained efforts to gradually and continually reduce the Town's direct use of fossil fuels, beginning at a rate of no less than 5% per year starting in the 2019-20 and continuing until they are eliminated entirely."

This task force will initiate work towards an aggressive and comprehensive work plan to achieve the elimination of direct fossil fuels in the Town of Norwich.

## About the Task Force

### Membership

The task force will be composed of nine (9) members, as follows:

- Two members of the energy committee, recommended by the committee and appointed by the selectboard
- One member of the finance committee, recommended by the committee and appointed by the selectboard
- One member of the planning commission, recommended by the commission and appointed by the selectboard
- One selectboard member, appointed by the selectboard
- One community member recommended by the Town Manager and appointed by the selectboard
- One representative of the school or business community, recommended by the Town Manager and appointed by the selectboard
- One member from an organization engaged in environmental justice work, recommended by the Town Manager and appointed by the selectboard
- One member with a depth of experience in greenhouse gas reduction and climate resilience strategy, recommended by the Town Manager and appointed by the selectboard

**Comment [C1]:** I would propose 1 (one) member of EC based on expectation that Reg EC will pass. I would expect the Reg Energy Coor would be part of the group.

**Comment [C2]:** Would PC member be bringing back the work to PC to full commission to implement? Or is this more advisory member to full PC? Would also wonder if existing committees could propose "giving up" their seat at the table to allow a non-member the seat?

**Comment [C3]:** Would increase to two or three and leave out as recommended by TM. We do not do other appts as a recommendation by TM. Otherwise, TM influence on Task Force may negatively impact work of the TF. "Stacking the deck" so to speak.

**Comment [C4]:** Same argument as above.

**Comment [C5]:** What do you mean? Can you provide an example of an organization engaged in environmental justice work? Argument against TM recommendation same as above.

**Comment [C6]:** As determined by what criteria? There may be many who fit this description but the methods by which he/she would recommend reductions can vary widely. Take this seat and increase community membership to 3 and eliminate TM recommendation as argued above.

The task force will report to the Selectboard and make recommendations jointly to the Board and Town Manager. The Selectboard is the appointing authority and may remove any member for good cause, including unexcused absence from two or more consecutive meetings without cause. Task force members are expected to abide by all Town of Norwich policies.

Members shall serve until the elimination of the Town's direct use of fossil fuels or otherwise by an action of the Selectboard.

## Authorities of the Task Force

The Task Force shall have the authority to:

- 1) Elect one or more members to serve as Chairperson and elect one or more members to serve as Vice Chairperson, who shall serve as Chair in the absence of the Chairperson.
- 2) Organize itself, establish committees or subcommittees, enlist the expertise of others as needed and delegate duties for the performance of its work. Any committee or subcommittee will adhere to the Open Meeting Law by posting agendas, maintaining minutes, posting minutes, and allowing for public participation.
- 3) Adopt rules of procedure for its meetings to accomplish its functions. Provision for public input shall be listed on the agenda of each meeting, and agendas and minutes taken and posted should be in a timely manner in accordance with the Vermont Open Meeting Law.
- 4) Make requests to the Town Manager's office for staff time, expertise, and documents.

**Comment [C7]:** I want to be clear at the outset that non-quorum-no action groups presenting out of meeting work to the full TF do NOT need (but may be encouraged) to comply with public warnings, etc

Meetings that are conducted via Zoom shall include links to the video conference via phone or internet and shall include a telephone number should the Zoom link fail to open for any or all participants. The Town Manager's office will provide assistance in scheduling Zoom meetings and posting minutes.

## Task Force Scope

- 1) Coordinate efforts with the Town Manager's Green Fleet [Group], Energy Committee, Finance Committee, and other Town Committees and Commissions, as needed
- 2) Provide proposals or recommendations for the design of new policies, programs, projects, and staffing, and for modifications to existing policies, programs, projects, and staffing to the Town Manager, Selectboard, ~~for~~ the reduction of direct fossil fuels. These proposal or recommendations may include, but are not limited to:

**Comment [C8]:** Eliminating town dpts doesn't preclude the dpt heads from proposing recommendations

**Deleted:** and Town departments

- a) Organizational capacity
  - i) Help determine role of town staff in benchmarking and tracking energy performance of town-owned buildings and fleet
  - ii) Recommend plans to improve town staff's commitment and awareness to fossil fuel reduction and elimination through training or communication strategies
- b) Measurement and tracking of energy performance
  - i) Create a system for benchmarking and publicly disclosing energy consumption for Town buildings and fleet, including methodologies for creating a Green House Gas inventory and fuel reporting standards
  - ii) Implement and improve the process for collecting data for buildings and vehicles. It may be proposed that benchmarking efforts, facility energy audits, cost of carbon calculations, and other current information regarding building and vehicle data may be managed in a central and accessible location
  - iii) Improve data management for building characteristics, including information regarding building insulation levels and mechanical systems
- c) Improved operations and maintenance for buildings and other capital assets
  - i) Establish guidelines that ensure that investments yield optimal results towards fossil fuel reduction. These guidelines may focus on best practices such as HVAC system controls, scheduling and set points, ventilation loads, and lighting system controls, sensors and scheduling
- d) Capital investments in building and transportation inclusive of analyses and prioritization, which may include:
  - i) Proposals for extensive energy use reductions factoring both up-front and life-cycle costs for fossil fuel reduction
  - ii) Develop a standard methodology for determining the effectiveness of capital projects, which may stem beyond life-cycle costing towards discount rate, inflation and utility rate escalations, and more
  - iii) An exemption process for investments in assets that do not reflect the goals of fossil fuel reduction but accomplish other town-wide goals or needs

- e) Pilot programs, purchases, or technologies to gather more information on certain assets before significant investment
  - f) Recommendations for use of the Climate Emergency Designated Fund
- 3) Submit an initial work plan to the Selectboard by July 1, 2021, and provide an updated annual scope of work, including priorities, goals, outcomes, and key stakeholders to consult, annually thereafter. A July 2021 work plan may contain items as outlined in items 1-3, but must:
- a) Define what constitutes a policy, program, or project that advances the directives of Article 36 as it relates to the Town-owned buildings and fleet.
  - b) Summarize work-to-date on Town's Green House Gas (GHS) emission and provide an analysis. Identify areas of improvement or missing data, if applicable.
  - c) Provide an engagement plan for collaborating with existing committees whose scope aligns with those of the Task Force.

**Comment [C9]:** Is this proposing Indications for use or recommendations on spending funds based on the stated indications for use?

The Task Force charge can be amended or revised by action of the Selectboard.

# Town of Norwich

Town of Norwich, Vermont



CHARTERED 1761

## **SPECIFICATIONS AND REQUEST FOR PROPOSALS FOR Norwich Real Property Reappraisal**

Date of RFP  
February 10, 2021

Proposals are due on May 14, 2021

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## **1. INTRODUCTION**

1.1 "Town" refers to the Town of Norwich. "Contractor" refers to the Contractor making the proposal and conducting the reappraisal. "Project Manager" refers to the ~~Norwich Board of Listers~~.

1.2 The Town is accepting sealed proposals for a Contractor to provide Computer Assisted Mass Appraisal (CAMA) software and conduct a reevaluation of all real property in Norwich. Proposals are to be received at the Lister's Office, 300 Main Street or mailed to P.O. Box 376, Norwich, VT 05055-0376 on or before April 30, 2021. Bids shall be marked "Norwich Reappraisal" on the outside of a sealed envelope. Late bids, electronic bids, or faxed bids will not be accepted.

1.3 Proposers shall submit one signed original and four copies of their bid in hard copy in the Form of Proposal contained in these specifications and an electronic copy of the bid in PDF format.

1.4 A pre-proposal zoom meeting can be made available if requested.

1.5 The Contractor's Reappraisal Plan shall be reviewed and approved by the Town and the Vermont Department of Taxes, Division of Property Valuation and Review before beginning the reappraisal work.

1.6 The effective date of the new real property values shall be April 1, 2022 or a subsequent year to be negotiated with the selected Contractor.

1.7 This request for proposal is subject to the appropriation of the necessary funds by the Town of Norwich depending upon the real property effective date. If the Town does not appropriate sufficient funds for the reappraisal, this request for proposals shall be considered null and void. It should be noted that designated funds are being set aside for this reappraisal.

1.8 When a specific product or deliverable is specified it shall be considered as the minimum acceptable.

## **2. REQUIRED INFORMATION FROM PROPOSERS**

2.1 Proposals shall include the following information.

2.2 Project Proposal Form.

2.3 One page summary of the Contractor's advantages and strengths.

2.4 Name and telephone number of person(s) to be contacted for further information and clarification.

2.5 Listing of all Vermont municipal revaluations completed during the past five (5) years, including client contact, telephone number, and size of municipality indicated by number of real estate parcels, scope of services rendered, and date completed under present corporate entity. Such list ideally will include at least two (2) communities that are similar to and at least equal in size to Norwich.

2.6 Listing of all Vermont municipal revaluations now underway or under contract, including client contact, telephone number(s), size of municipality indicated by number of real estate parcels, scope of services rendered, and date to be completed.

- 2.7 Listing of personnel to be assigned to Norwich's revaluation including years of experience in current position and other revaluation positions, municipalities served and their roles in those revaluations. Resumes of personnel assigned to this project must be included.
- 2.8 Description of the methodologies used for assessing values for residential, commercial, exempt, non-taxable and vacant land parcels.
- 2.9 Description of sales analyses that shall be performed to calibrate and verify accuracy of valuations.
- 2.10 Description of the Contractor's public relations program proposed to be used throughout the revaluation project.
- 2.11 Description of the CAMA software and version and database engine used.
- 2.12 Indication of how many years the Contractor has been engaged as a Contractor, corporation, partnership, or individual specializing in governmental tax revaluation service.
- 2.13 Each Contractor shall provide the name of a person or designated representative of the Contractor to whom all official notices or correspondence shall be sent, and the correct mailing address, email address and telephone number of such individual.
- 2.14 Provide samples of typical progress reports.
- 2.15 Provide a detailed description of the Quality Control Program to be utilized in this Project.

### **3. CAMA SOFTWARE**

- 3.1 The use of any other CAMA software shall be prequalified by sending a copy of the specifications annotated with the proposed CAMA software capabilities by listed item to the Project Manager by the proposal due date. The proposed CAMA software will then be reviewed by the Board of Listers and notification made to the proposing contractor.
- 3.2 Upon transfer of the CAMA database to the Town the CAMA software shall be installed based upon highest and best technology used at the time as operating systems and the final data will reside on a cloud-based server.
- 3.3 The CAMA software shall be able to communicate with State of Vermont grand list administration module used to generate the reports that become the grand list book and change of appraisal notices.
- 3.4 It shall meet all requirements of the State of Vermont and Town of Norwich. Any changes to meet future requirements shall be provided to the Town at no additional cost(s).
- 3.5 It shall have the capability to link to supporting PDF and Microsoft Word documents from the property file.
- 3.6 It shall have the capability to print PDF files without any additional printer drivers.
- 3.7 It shall have the capability for web-based access to property cards including sketches and pictures on the Town's or software suppliers' server. The property cards shall be fully searchable and have the capability to be limited by parcel and by fields displayed and have the capability of being password protected and to track users.

### 3.8 Conversion of Current Database

#### 3.9.1 Current system- AssessPro

3.9.2 Convert all data fields and sketches to the proposed CAMA system and confirm the accuracy of the conversion.

#### 3.10 General Information

3.10.1 The CAMA system shall include valuation capability for all residential, commercial, industrial, condominiums, solar, vacant land, exempt properties, homestead program and land and structures subject to the Vermont Use Value Program.

3.10.2 It shall be maintainable by Town personnel after installation and conversion

3.10.3 User-controlled edit features, including capability to update cost tables, depreciation factors and market adjustments.

3.10.4 Inquiry capabilities on all parcel information by Parcel ID (PID), locatable street address and name of the owner(s).

3.10.5 On-line screen property appraisal card with logical groupings of data on the screen designed to correspond to data elements on the data collection card.

3.10.6 Maintenance function producing audit trail listings and totals showing the before and after status of the data and the person making the change and the date and time of the change.

3.10.7 It shall have report writer capabilities to create a variety of independent reports as developed and required by the Board of Listers by selecting some or all fields that can be sorted as needed and totaled.

3.10.8 Property record printouts shall be on plain paper, not pre-printed forms. The final layout shall provide all of the data used by the Vermont Grand List specifications.

3.10.9 The CAMA system shall be fully integrated so that the user can perform all day-to-day entry and editing functions, including the capability to override any programmed formula numbers.

3.10.10 All major functions shall be performed through on-screen menus. Whenever possible, programs shall be maintained by a "fill-in-the-blanks" or "pull-down" menus process. The systems shall provide for "error" messages. Security devices to restrict access to various fields shall be provided.

3.10.11 All legal information necessary for the tax billing cycle shall be contained in the master file. This shall include: Owner name, exemption codes, exemption amounts, any special or preferential assessments such the Vermont Use Value Appraisal program, etc.

3.10.12 It shall have the ability to generate the Vermont Property Valuation and Review (PV&R) reports to be utilized by the Town for submittal to Vermont Department of Taxes, Division of Property Valuation and Review for tax rate determination.

3.10.13 It shall store and display the data that is collected for each parcel on both screens and in reports. The system shall support the creation and analysis of separate inventory and sales history data files, store and apply factors and values to calculate cost, sales and value comparison estimates of value, generate sales ratio reports and support maintenance activities such as parcel splits, building permits, transfers, etc.

3.10.14 It shall provide for the production of mailing labels.

### 3.11 Valuation Approaches

3.11.1 It shall have the ability to develop and select more than one approach to value including, but not limited to, the cost approach, the income approach and the direct sales comparison approach, multiple regression method, weighted estimate comparable sales technique, etc.

3.11.2 It shall be capable of generating complete and final property values for each parcel by the cost approach, direct sales comparison approach and income approach for commercial and industrial properties and shall consider the pricing of additions and outbuildings by the cost approach, including adding appropriate depreciated values to the already "in place" assessed values in the system.

### 3.12 Cost Approach Module

3.12.1 It shall include data management and cost valuation components for commercial, industrial, and residential properties using Marshall & Swift (CoreLogic) cost data or Town-generated cost data.

3.12.2 It shall produce a valuation document showing the cost calculations on each property, the appraisers' adjustments and final value determination.

3.12.3 It shall access a maintainable table of replacement cost formulas and depreciation schedules that are keyed to a structure code (indicating what the improvement was built as or designed to be used for).

3.12.4 It shall provide for correlation of the cost approach with the sales comparison approach through the use of economic condition factors as well as the standard depreciation for age, condition, and economic and functional obsolescence.

3.12.5 It shall compute value estimates based on standard appraisal methodologies (acceptable by the IAAO, Society of Real Estate Appraisers and the Appraisal Institute) with clear documentation for non-automated value computations and defense of values.

3.12.6 The basic residential classification system shall identify property within construction type, quality class and floor area as a minimum; values must also be responsive to effective age.

3.12.7 The component costs shall be easily understood in terms of cost new, depreciation and land value schedules.

3.12.8 It shall provide for on-line updating of the data management files with cost estimates. This includes the capability to price any additions and new outbuildings over 100 square feet through the cost approach.

3.12.9 It shall be adaptable to time and location indicators as well as possible neighborhood adjustment tables.

3.12.10 It shall be capable of calculating a value estimate by the square foot method.

3.12.11 The land pricing function shall utilize Computer-Assisted Land Pricing tables modifiable by the Town to provide location and land-use indexing for mass updating of land values. Land pricing should also have the ability to utilize preferential unit land values in addition to market-based unit land values and be adjustable by land area.

3.12.12 Computed cost values for key fields shall be stored on a value history file of record. These would include Replacement Cost New (RCN) of each structure, Replacement Cost New Less Depreciation (RCNLD) of each structure, land value, physical depreciation by structure and accrued depreciation by structure. The cost estimates shall be fully integrated with the market approach sub-system to develop reports to study depreciation schedules, land rate schedules, neighborhood factors, date of sale adjustments and certain cost adjustment factors. Cost schedule additions or modifications shall not require program

changes. New codes shall be able to be added by updating cost schedules and data management libraries.

3.12.13 It shall be capable of calculating both the ad-valorem and sales comparison value, and land values for various subcategories of land use, such as, but not limited to, two (2) acre or smaller house sites, miscellaneous (bulk) land, land in multiple ownership, and separate parcels.

3.12.14 It shall be able to print detailed Cost Approach Valuation sheets.

### 3.13 Sales Comparison Approach Module

3.13.1 It shall provide for data screening, segmented analysis, sales comparison model generation, model evaluation, value predictions, comparable selection, and adjustment of comparables and field review reports.

3.13.2 It shall produce sales comparison value estimates using comparable sales.

3.13.3 It shall include the ability to enter, track and extract properties that have sold from the master file and to build a sales history file for sales analysis purposes.

3.13.4 For each improved residential property to be valued, the program shall select from the sales history file three (3) to five (5) comparable properties, which have sold recently. The comparable properties will be selected from the same neighborhood or from an area similar to that of the subject property.

3.13.5 It shall include the ability to analyze sales ratio statistics by neighborhood, class and/or neighborhood groups. This shall provide the necessary information to make comparisons between classes, locations, and to identify categories of significant differences in value level or variance.

3.13.6 It shall allow constraints, if determined by user, otherwise adjustments should be market developed through multiple regression analysis, not based on cost adjustments only.

3.13.7 It shall have the ability to print and display on screen Comparable Sales Approach along with supporting valuation approaches.

3.13.8 It shall be capable of calculating both the ad-valorem and sales comparison approach.

3.13.9 It shall have the capability to build an independent sales file with independent access for research, as well as maintenance of multiple sales for multiple years.

### 3.14 Income Approach Module

3.15 The income approach module shall perform valuation by income capitalization.

3.15.1 Methods of capitalization shall include one or more of the following: mortgage equity, discounted cash flow, gross rent multipliers, overall rates, and residual capitalization.

3.15.2 It shall have the ability to develop and apply Cost Approach and Income Approach to value through cost and income models.

3.15.3 It shall have the ability to develop different types of income models and develop variations to income models within the types, applied by location automatically by system, as defined by user.

3.15.4 It shall allow for adjustments to both the cost and income approaches as determined through analysis, by physical condition and/or accrued obsolescence and should apply this adjustment uniformly and automatically.

3.15.5 It shall allow for adjustments to capitalization rate, vacancy, effective age, and expense ratio, by the reviewer.

### 3.16 Statistical and Data Analysis

3.16.1 It shall be capable of generating statistical reports and assessment

administration studies as follows:

- 3.16.1.1 Statistical analysis by market stratification.
- 3.16.1.2 Ratio studies and coefficients of dispersion and price related differential by total sampling.
- 3.16.1.3 Geo-districts and by property type or use and by age of improvements.
- 3.16.1.4 Measures of Central Tendency.
- 3.16.1.5 Measures of Dispersion.
- 3.16.1.6 Variable Profiles, Charts, and Graphs.
- 3.16.1.7 Uses of Exploratory Data Analysis.
- 3.16.1.8 Sales ratio Studies.
- 3.16.1.9 It shall be able to perform detailed sales ratio reports by a minimum of ten (10) stratifications with a class type, perform analysis such as scatter-grams, histograms, cross tabs, etc., to assist in completely reviewing analyses, final valuation estimates and public presentations.
- 3.16.2 It shall be able to perform administration studies such as total land valuations and total valuations by town-wide, geo-districts and property types or uses, including taxable and exempt properties.
- 3.16.3 It shall be able to develop and apply the final value estimate while displaying the supporting value approach onscreen.
- 3.16.4 It shall provide the capability of comparable sales selections and shall provide a market- tested formula for multiple regressions.
- 3.16.5 It shall have the capability of selecting and adjusting comparable sales for use by the Board of Listers in defense of values derived by all the recognized and accepted approaches to value.
- 3.16.6 It shall have the capability to build an independent sales file with independent access for research, as well as maintenance of multiple sales for multiple years.
- 3.16.7 It shall be capable of creating reports for statistical analysis.

### 3.17 Drawing Program

- 3.17.1 The system shall include a computerized sketching program including:
  - 3.17.1.1 Property labels
  - 3.17.1.2 Area designation
  - 3.17.1.3 Calculated perimeter and area of individual sections and total area
  - 3.17.1.4 Year built
  - 3.17.1.5 Floor to ceiling height
  - 3.17.1.6 Percent complete for each individual section of the structure
  - 3.17.1.7 Links to the cost-based evaluation module
  - 3.17.1.8 GPS coordinates
  - 3.17.1.9 Notes

### 3.18 Digital Imagery

- 3.18.1 The following are requirements for a fully integrated imagery package:
  - 3.18.1.1 The software shall be able to store multiple images and update the most recent image and archive previous image when importing/uploading, but be capable of being overridden.
  - 3.18.1.2 It shall be able to print individual or batches of archived and current images.
  - 3.18.1.3 It shall have the ability to select and print individual documents as well as availability of visual enhancements to assist when reviewing a document screen (e.g., control brightness, contrast, zoom, etc.).

3.18.2 Digital images shall be fully integrated and displayed on the host appraisal system and mobile devices with the ability to display on documents launched from appraisal and mobile devices system including property record cards.

### 3.19 Geographic Information Systems (GIS) Integration

**(NOTE: This section needs TM & ZA Verification)**

3.19.1 It shall integrate with ESRI's ArcView and allow for dynamic data manipulation and discovery and return the data set to the CAMA system to browse the parcels, create a report using only this data set or change the data, recalculate the assessments and redisplay the results.

3.19.2 It shall include:

3.19.2.1 Mass updating functions

3.19.2.2 One-to-many and many-to-one relationship accuracy

3.19.2.3 Tabular data available directly in the GIS Viewer

3.19.2.4 Direct link of selected parcels to the report module

### 3.20 GIS Software Upgrades and Updates

3.20.1 Provide an upgrade from a one-seat ESRI ArcView Version 3.3 GIS software to version 10.x

3.20.2 Provide an upgrade from a one-seat ESRI ArcView Version 9.x GIS software to version 10.x

3.20.3 Provide software maintenance and any upgrades for three (3) years from the date of the installation of the software.

### 3.21 Training Documentation and Maintenance

3.21.1 Provide all documentation for the system and conversion documentation (i.e. mapping, error reports, test results etc.) by no later than the completion date of the project. The documentation shall include comprehensive user manuals, which clearly explain data collection, data entry and all appraisal operations, systems and functions and other support material. A schematic system map shall be provided for the CAMA and related software.

3.21.2 Provide comprehensive training in use, and operation and technical aspects of the system on Town premises. Such training shall be sufficient so that the Town personnel shall be able to operate and understand the use and operation of the system and the appraisal process.

3.21.3 Provide software maintenance and any upgrades for three (3) years from the date of the completion of the reappraisal as part of an on-going system support program.

## 4. REAPPRAISAL

4.1 One member of the Board of Listers and/or the contract assessor may accompany the Contractors personnel on all site inspections.

4.2 The Board of Listers is responsible for determining final real property values and lodging the Norwich Grand List.

### 4.3 Responsibilities of the Town

4.3.1 The Town shall furnish the following:

4.3.2 Maps: The Town shall furnish one (1) hardcopy set of the most recent Norwich Tax Maps showing streets, property lines, and parcel identification (PID) number.

4.3.3 The Town shall provide a copy of the current Town zoning regulations and zoning map.

4.3.4 The Contractor shall at their expense provide to the Listers and the Town's tax map provider all necessary changes to the tax maps during the period of the reappraisal. The form of this communication shall be determined by the Town's tax map provider.

4.3.5 Land Dimensions: The Town shall make available lot sizes and total acreage to the Contractor of all pieces of property. Where the maps or present records fail to disclose measurement or acreage, the Town will also provide assistance during informal reviews to resolve discrepancies due to land sizes, boundaries, etc.

4.3.6 Property Transfers: The Town shall notify the Contractor, on a regular basis, of property splits and transfers as soon as they are received by the Listers' office after recording by the Norwich Town Clerk. The Contractor shall update the CAMA software as necessary.

4.3.7 Zoning (Building) Permits: The Town shall make available copies of all zoning/building permits as soon as they are received by the Listers' office through the course of the revaluation project.

4.3.8 Signing of Communications: The Board of Listers shall sign communications to be mailed at the Contractor's expense, to contact property owners for inspection of the property and for the purpose of obtaining the property owner's income and expense information for the income approach for valuation of commercial properties.

4.3.9 Mailing Addresses: The Board of Listers shall make available the currently known mailing addresses and telephone numbers of all property owners. At such time as the Town learns of changes to this information, it shall be transmitted to the Contractor and said changes shall immediately be entered into the CAMA software.

#### 4.4 Office Space, Hours, Staffing

4.4.1 Office space to carry out the terms of this contract shall be available in the Listers' present office. The office shall be available to the Contractor during the regular hours maintained by the Town of Norwich. During the reappraisal, the Listers' office will be open to the public based upon then current State guidance, so as to meet their needs and responsibilities. The Contractor shall be responsible for its own cell phone charges.

4.5 Tentative Project Schedule – mutually acceptable dates to be determined between Town and Contractor.

4.5.1 \_\_\_\_\_ - Contract signed by Board of Listers

4.5.2 \_\_\_\_\_ - Public Forum(s) on reappraisal with vendor

4.5.3 \_\_\_\_\_ - Training of field personnel

4.5.4 \_\_\_\_\_ - Field work on reappraisal begins

4.5.5 \_\_\_\_\_ through \_\_\_\_\_ - Computer Assisted Mass Appraisal (CAMA) software installed and field work to develop, verify and update property records

4.5.6 \_\_\_\_\_ through \_\_\_\_\_ - Quality assurance and verification of data set

4.5.7 April 1, \_\_\_\_\_ - Assessment date

4.5.8 April 15, \_\_\_\_\_ - Abstract of Grand List lodged and Change of Appraisal Notices sent

4.5.9 April 15, \_\_\_\_\_ through July 1, \_\_\_\_\_ - Web-based access to property cards including sketches and pictures available on the CAMA software supplier's sever. The property cards shall be searchable and have the capability to be limited by parcel and by fields displayed and have the capability to track users.

4.5.10 \_\_\_\_\_ through \_\_\_\_\_ - Informal grievance process

4.5.11 \_\_\_\_\_ Change of Appraisal Notices sent, if different from Abstract.

- 4.5.12 \_\_\_\_\_ Last day to grieve value  
4.5.13 \_\_\_\_\_ through \_\_\_\_\_ -Grievance Hearings  
4.5.14 \_\_\_\_\_ -New Grand List lodged

4.6 Transmittal of Records to the Town shall be as follows:

4.7 Regular periodic delivery of appraisals, as completed, and in accordance with a schedule agreeable to the Listers, shall be turned over to the Listers for review. All appraisals of structures either complete or under construction, shall be valued as of April 1, 2022 or a subsequent year to be negotiated with the selected Contractor.

4.8 All records shall be completed and corrected and shall be turned over to the Listers by April 15 of the same year. The final inspection and review shall take into consideration any known or apparent changes in the individual property since it was first inspected so that the final appraisal of property shall be appraised as of April 1, 2022 or a subsequent year to be negotiated with the selected Contractor.

4.9 It is understood and agreed that the reappraisal of properties covered by this agreement shall conform to the procedures and technical requirements and laws of the State of Vermont, and the Town of Norwich. At least biweekly, an authorized agent of the Contractor shall meet with the Board of Listers to discuss the progress and various other details of the project. Bi-monthly written status reports shall be submitted to the Lister throughout the duration of the project.

4.10 Default

4.9.1 Satisfactory Performance

4.9.1.1 Should the Contractor fail to fulfill, in a manner deemed satisfactory to the Town, its obligations under this Agreement, or if the Contractor should violate any of the material covenants, conditions, or stipulations of this Agreement, which failure or violation shall continue for ten (10) days after written notice of said failure or violation is sent by the Town, the Town shall have the right to terminate this Agreement by giving written notice to the Contractor of such termination and specifying the effective date thereof, at least seven (7) days before the effective date of such termination; the Contractor shall remain liable for the breach of this Agreement by the Contractor. If this termination clause is invoked, the Contractor's agents and employees shall, at the Town's direction, vacate in an orderly fashion the office space provided by the Town, leaving behind all records, properly filed and indexed, as well as other property of the Town. Any funds held by the Town under the Contract shall remain the property of the Town to the extent necessary to reimburse the Town for its costs in obtaining another contractor and supervising the transition. Termination of the Contract and retention of funds by the Town shall not preclude the Town from bringing an action against the Contractor for damages or exercising any other legal, equitable, or contractual rights the Town may possess in the event of the Contractor's breach.

4.9.2 If the Contractor does not pay its debts as they shall become due, or if a receiver shall be appointed for its business or its assets and not voided within sixty (60) days, or if the Contractor shall make an assignment for the benefit of creditors, or otherwise, or if its interest herein shall be sold under execution or if it shall be adjudicated insolvent or bankrupt, then and forthwith thereafter, the Town shall have the right at its option and without prejudice to its rights hereunder to terminate the Contract and withhold any payments due.

4.9.3 The Contractor shall be liable for all costs and reasonable attorney fees resulting

from its breach.

#### 4.10 Defense of Values

4.10.1 For all appeals to the Board of Listers, the Board of Civil Authority, the State of Vermont Tax Department (Director of the Division of Property Valuation and Review) or the Windsor Superior Court for the April 1, Grand List generated as a function of the contract signed between the Town and the Contractor, the Contractor shall work with the Board of Listers in evaluating and defending the values being appealed.

4.10.1.1 Upon completion of the Preliminary Grand List (Abstract) and the mailing of the Change of Appraisal Notices, the Contractor and Board of Listers shall conduct a period of informal review prior to formal grievances. During the period of informal review a knowledgeable representative of the Contractor shall provide assistance to the Board of Listers including participation during this process.

4.10.1.2 During the grievance process with respect to the April 1 Preliminary Grand List (Abstract), qualified Contractor personnel shall provide assistance and make recommendations to the Board of Listers as to whether the valuation should be modified. The Contractor personnel will be available during the grievance hearings including participation in the hearing process.

4.10.1.3 The cost of providing assistance to the Board of Listers during the informal review, grievance process and Board of Civil Authority hearings shall be included in the proposal costs.

4.10.2 Qualified Supervisory Contractor personnel shall be available to provide testimony on appeals of value to the Windsor Superior Court or the State of Vermont Tax Department, Director of the Division of Property Valuation and Review at the hourly rate described in the Proposal.

#### 4.11 Excusable Delays

4.11.1 In no event shall either party be liable to the other for any delay or failure to perform which is due to any Act of God or civil or military authorities, civil disturbance, wars, strikes, fires, natural catastrophes, or other unforeseen similar causes beyond the control and without the fault or negligence of the party claiming excusable delays.

#### 4.12 Town Records

4.12.1 All records, computations, and work product including electronic databases, made by the Contractor in connection with any appraisal of property in the Town shall, at all times, be the property of the Town and, upon completion of all services to be performed under this agreement, or termination of this agreement, immediately shall be delivered to the Town, in good order. The Contractor shall certify to the Town, in writing, that it has delivered all such records to the Town and has no such records in its possession. The Contractor may, with written permission of the Board of Listers, keep records to be used for future support of the Town's data base software. Redacted copies of such records may be retained by the Contractor for statistical studies. Such records and computations shall include all electronic or hard copies of any relevant documents.

4.12.2 The Contractor shall have access to needed Town records during the course of providing services hereunder, and the parties shall cooperate in order to minimize any disruption to the Town. No original record shall be removed from any Town Department.

#### 4.13 Protection of the Town

##### 4.13.1 Indemnification and Insurance

4.13.1.1 The Contractor agrees to defend and save harmless the Town of Norwich, its officers, agents and employees against all claims, demands, payments, suits, actions, recovery, and judgments of every kind and description arising out of the performance of

this Agreement, including personal injury or property damage brought or recovered against it by reason of any negligent action or omission of the Contractor, its agents, or employees and with respect to the degree to which the Town is free from negligence on the part of itself, its employees and agents.

4.13.1.2 The Contractor shall carry Comprehensive Broad Form General Liability Insurance in the amount shown below including protection for bodily injury and property damage.

4.13.2 The Contractor shall also maintain Automobile Liability Insurance providing limits prescribed by the Town and Umbrella or Excess Liability Insurance in the amount shown below. The Workers' Compensation Insurance shall provide coverage pursuant to V.S.A. Title 21, Section 600 et seq.

4.13.3 Prior to the commencement of the work Certificates of Insurance shall be supplied to the Town by the Contractor detailing the required coverage. These Certificates shall be issued by a carrier authorized to do business within the State of Vermont. The Town shall also be named as an additional insured.

4.13.4 The Contractor shall have and require all subcontractors to have and maintain insurance coverage and list the Town as an additional insured in accordance with the minimum amounts listed below. Prior to the start of any work, the Town shall be furnished with an insurance certificate as proof that coverage is in place.

**(NOTE: These amounts need to be reviewed by the TM)**

4.13.5 General Liability - \$1,000,000 per occurrence

4.13.6 Product Liability- \$1,000,000 per occurrence

4.13.7 Property Damage - \$1,000,000 per occurrence

4.13.8 Personal Injury - \$1,000,000 per occurrence

4.13.9 Automotive Liability- \$500,000 per occurrence

4.13.10 Worker's Compensation - Statutory Requirement

4.14 Changes and Subletting of Contract

4.14.1 All changes to this Contract shall be in writing and signed by persons for both parties so authorized.

4.14.2 The Contractor shall not sublet any portion of this Contract or use any independent contractor for this contract without consultation and prior written approval from the Board of Listers. All Contractor personnel assigned to this contract shall be employees of the Contractor.

4.15 Scope of the Revaluation Project

4.15.1 The revaluation project requires the complete appraisal and revaluation of all real taxable, exempt and non-taxable property located within the municipal boundaries of the Town of Norwich, Vermont.

4.15.2 The Contractor shall furnish all labor, and their own computer and shall perform all work for the project in strict accordance with this Request for Proposals (RFP) and written agreement signed by both parties and any amendments thereto signed by both parties.

4.15.3 The values to be determined shall be market value as defined in V.S.A. Title 32 and relevant Vermont Supreme Court decisions. The basis of valuation shall be the recognized methods of appraising real property, as defined by the International Association of Assessing Officers (IAAO). Should a question arise as to the taxable status of any property, the Listers shall make the final decision as to its status.

4.15.4 The effective date of this revaluation project shall be for the Assessment Date as of

April 1, 2022 or a subsequent year to be negotiated with the selected Contractor.. Valuations and appraisal of all taxable, exempt and non-taxable real property shall reflect full market value as of April 1, 2022 or a subsequent year to be negotiated with the selected Contractor.

4.15.5 The number of parcels in the Town as of April 1,2020 was 1750. A more complete description of the properties can be found in the Norwich 2020 Grand List, which is available in the Norwich Listers' office, the Norwich Town Clerk's office or on line at [www.norwich.vt.us](http://www.norwich.vt.us).

#### 4.16 Pertinent Town Data

4.16.1 Last revaluation was effective as of April 1, 2016.

4.16.2 Population as of the 2020 Census for Norwich is approximately 3,400.

4.16.3 The total area of the Town is 28,672 acres.

#### 4.17 General Conditions

##### 4.17.1 Project Award

4.17.1.1 The Town of Norwich reserves the right to reject any, or any part of, or all Proposals; to waive formalities and technicalities; and to accept that Proposal which the Town deems to be in the best interest of the Town. Proposal price shall be a consideration, but the lowest dollar cost Proposal shall not be the sole criterion to be considered. Consideration in the awarding of the contract shall be given to price, experience and competence of the Contractor, project schedule, quality and experience of the Contractor's personnel, the nature and size of the Contractor's organization, the quality of similar projects performed by the Contractor in the past and any other relevant criteria.

#### 4.18 Contractor

##### 4.18.1 Certification

4.18.2 Each Contractor, corporation, partnership, or individual, hereinafter termed Contractor shall be certified in accordance with Title 32 V.S.A Section 4052 prior to the commencement of any and all work.

#### 4.19 Personnel

4.19.1 The Contractor shall provide experienced and qualified personnel in compliance with the requirements of the Equal Employment Opportunity provisions of Federal and State Governments.

4.19.2 The Contractor shall submit to the Town written qualifications of all personnel assigned to this project in the form of a resume. All personnel assigned to this project shall be subject to the approval of the Project Manager and shall be removed from this project by the Contractor upon written demand of the Project Manager. The Project Manager will have the right to interview all personnel before they are assigned to this project. As part of this Contract, the Contractor will provide motor vehicle and criminal background checks of all project personnel upon request.

4.19.3 The Contractor shall assure the availability of all personnel assigned to work on this project.

#### 4.20 Minimum Qualifications

4.20.1 The Contractor shall employ personnel in such positions as necessary to execute the provisions of this work. If not presently certified by PV&R for the purposes of completing a town-wide revaluation, the Contractor, and its personnel shall obtain such approval prior to the commencement of this revaluation.

#### 4.21 Project Supervisor

4.21.1 Once assigned to this project, the Project Supervisor of the Contractor shall not delegate or assign any responsibilities of and for this project to any other person. Should the Town desire, at any time, to have the Project Supervisor replaced on this project, the Town shall notify the Contractor in writing, stating its reason(s).

#### 4.22 Identification

4.22.1 All field personnel shall carry suitable I.D. cards, which will include an up-to-date photograph, supplied by the Contractor. All automobiles used by the field personnel will be registered with the Norwich Police Department, giving license number, make, model, year and color, and ownership of the vehicle.

4.22.2 Each automobile used for field inspections shall display appropriate signs on the driver and passenger sides of the vehicle at Contractor expense. The signs shall include information sufficient to inform the public of the purpose of the occupants and in addition shall display the words "Norwich Listers Office," and telephone number "(802) 649-1419 Ext 6". The final design shall be subject to the approval of the Board of Listers.

#### 4.23 Conflict of Interest and Communication

4.23.1 No Town of Norwich employee, elected official or immediate relative/spouse shall be employed by the Contractor. All written and electronic communication, subsequent to the awarding of this contract, shall be to the Norwich Board of Listers. No private communication with individual Town employees or Norwich elected officials shall be permitted other than the bi-monthly meetings with the Project Manager and the property inspections as set forth above.

#### 4.24 Staff Meetings

4.24.1 Prior to the issuance of any field assignments, the Contractor shall hold a joint meeting with the Norwich Board of Listers in order that all duties and assignments can be explained to all individuals who are to be performing tasks in conjunction with this project. All staff members for the Contractor shall receive a copy of the executed agreement for the revaluation. It is the Town's intent that all individuals who are to participate in this project should be fully informed as to the contract requirements. Any individual assigned to the project after the initial staff meeting shall also receive a copy of the executed agreement and shall attend a similar meeting held between the Project Supervisor and the Project Manager. Subsequent staff meetings will be held on a bi-weekly basis, with the Contractor and at least two (2) Listers present.

#### 4.25 Confidentiality

4.25.1 The Contractor agrees not to disclose to anyone except to the Board of Listers any new values, for any purpose, or to permit anyone except the Board of Listers to use or peruse any of the data on file in connection with the revaluation.

#### 4.26 Assessment Date

4.26.1 The completed appraisals, upon approval of the Board of Listers shall be the basis for the Assessment Date April 1, 2022 or a subsequent year as negotiated with the selected Contractor. All data contained in the Appraisal Cards, and the Town's new CAMA Software shall reflect the Assessment Date of April 1, in the year agreed upon\_\_\_\_\_.

#### 4.27 Payment Schedule

##### 4.27.1 Progress Payments

4.27.1.1 Payments shall be made in the following manner: Thirty (30) days after the execution date of this contract, and thereafter within ten (10) days of the end of each succeeding month, the Contractor shall forward to the Norwich Board of Listers an invoice of work completed during the preceding thirty (30) day period. Such request for payment shall itemize and accurately indicate the extent and nature of work performed by volume, street, and category or in any other manner required by the Project Manager. The Listers will review and approve all invoices and submit for payment to be rendered within thirty (30) days of receiving an invoice for work completed.

#### 4.28 Responsibilities of the Revaluation Contractor

##### 4.28.1 Good Faith

4.28.1.1 The Contractor shall use good faith in its efforts to assist the Assessor and Board of Listers in determining accurate and proper market valuations, and shall not undervalue or overvalue any land, building or other property to avoid or minimize its responsibilities.

##### 4.28.2 Public Relations

4.28.2.1 The parties to this project recognize that a good public relations program is required in order that the taxpayers of the Town may be informed as to the purpose, benefits, and procedures of the revaluation.

4.28.2.2 The Contractor shall provide reasonable assistance to the Assessor and Board of Listers in conducting a program of public information through media, electronic means and public meetings.

#### 4.29 Conduct of Contractor Employees

4.29.1 As a condition of this contract, the Contractor's employees shall at all times treat the residents, employees, and taxpayers of the Town with respect and courtesy.

#### 4.30 Records

##### 4.30.1 General Provisions

4.30.1.1 The Contractor's entire work product shall comply with the administrative rules and regulations of the Vermont Department of Taxes, Division of Property Valuation and Review (PV&R) and the Vermont Statute Annotated.

##### 4.31 Town's Property Records

4.31.1 The Contractor shall use a system approved by the Board of Listers for the accurate inventory of all records and maps, copies of which may be made from the files of the Board of Listers' office, in connection with appraisal work. All such records and maps shall be returned before the end of the contract and prior to final retainage paid. Copies only of the Board of Listers' records shall be taken outside of the corporate limits of the Town with prior permission of the Listers.

##### 4.32 Data Collection

4.32.1 The Contractor shall collect data that will be entered into the CAMA system either electronically or on hard copy. In either case the original data collected shall be preserved so it can be retrieved by the Listers' office. In the case of electronic data collection, the electronic data collection device(s) shall become the property of the Town at the termination of the project.

4.32.2 The Contractor shall complete field record cards, commonly referred to as "Data Collection Cards," or by electronic means identified by map and lot number. The map and lot number as indicated on the Town of Norwich tax maps shall be considered the Parcel Identification Number (PID) for all parcels. The signature or other identifying symbol of the Contractor's employee collecting the property data and the property owner's or representative's signature shall be noted on each property record card or electronic file

4.32.3 The information collected in the field during property inspections (that will be entered into the CAMA software) will include, but is not limited to, information as to location of property and locatable address if a structure appears on the parcel, classification as to usage, owner of record, size, shape and physical characteristics of land, with a breakdown of open and wooded acreage as applicable, public utilities available, public improvements, and zoning designations in effect as of assessment date. Locatable addresses are assigned by the Town of Norwich Planning and Zoning Administrator.

4.32.4 All physical improvements to the properties (including any man-made structures greater than one hundred (100) square feet in footprint) and their existing conditions shall be measured comparing present conditions to the Town's data. All attributes of the various structures interior and exterior shall be noted as required by the CAMA software. Structures with a floor area of less than 100 square feet shall be noted but not valued unless very unique. All attributes that apply to the software of the land shall be noted. Digital drawings shall accurately indicate the footprint of all floor areas of all structures over 100 square feet in size. Special attention shall be paid to structures that are heated or have electricity or running water. All detached structures including tennis courts, swimming pools or other significant improvements shall be indicated by footprint dimensions or electronically drawn to scale. All additional information required by the CAMA program for every structure shall be noted. The drawings shall be labeled, organized, and electronically filed according to the protocols developed by the Norwich Board of Listers. The Board of Listers shall offer one day of training to the Contractor's personnel regarding this protocol. There shall be one hard copy of each drawing attached to its appropriate Property Card.

4.32.5 Digital photos of all improved properties shall be taken and dated showing at least four sides of each major structural improvement and any significant site improvements such as ponds, pools, tennis courts, landscaping, etc. Detached structures shall be photographed from as many views as necessary to depict the use and configuration of the improvements.

4.32.6 The Board of Listers shall offer one day of training to the Contractor's personnel regarding this protocol. Electronic copies of all photographs shall be filed according to the protocol extant in the Listers Office at the time of the signing of an agreement.

#### 4.33 Preliminary Assessment Notices

4.33.1 At the close of the revaluation and as shown in the tentative project schedule a Change of Appraisal Notice, hereinafter called the booklet, shall be sent by the Board of Listers, by first class mail to each owner of record as of April 1, of the year of the assessment, setting forth the valuation that has been placed upon the property identified in the notice. The Contractor shall provide the needed information, ready for printing, for the notice five (5) business days prior to the mailing date of April 15. The notice shall comply with Title 32 of Vermont Statutes Annotated regarding notification to taxpayers of a change in appraisal. Also enclosed shall be a letter specifying the dates, times and place of the informal public hearings, informal grievance procedure, formal grievance

procedures, URL of web-based property record cards and a valuation booklet, printed in a minimum of nine-point Arial font listing all taxable properties in the Town of Norwich. The format of the booklets shall be approved, prior to printing, by the Board of Listers and shall include the name of each property owner, the Parcel Identification Number and the old and the new valuations. The booklet shall be arranged in the alphabetical order of the names of property owners. One-hundred additional copies shall be printed for use of the Listers. The Contractor is responsible for the printing and first-class mailing costs including a USPS certificate of mailing.

4.33.2 As of the date of the mailing of the booklet and until the Grand List is lodged web-based access to property cards including sketches and pictures shall be available on the CAMA software suppliers' server. The property cards shall be searchable and have the capability to be limited by parcel and by fields displayed and have the capability to track users.

#### 4.34 Informal Public Hearings and Informal Grievance Process

4.34.1 Between April 15, and a date to be negotiated with Contractor, the Contractor shall hold two public informational forums at a time and place approved by the Board of Listers. The Contractor's personnel shall explain the manner and methods of arriving at the new valuations. Following the mailing of the Change of Appraisal Notice, the Board of Listers and employee(s) of the Contractor shall hold informal grievance meetings so that errors with respect to individual properties (size of buildings or land, number of bedrooms, bathrooms, etc.) can be resolved. Any information offered by the taxpayer shall be given consideration, and adjustments shall be made where warranted with the approval of the Listers.

#### 4.35 Grievance Hearings

4.35.1 The grievance process is set forth in Title 32 of Vermont Statutes Annotated. During the grievance hearings the Contractor along with at least two of the Board of Listers shall hear grievances and re-inspect each grieved property with the Board of Listers to reach a decision as to revised valuations. Following the grievance process, the Contractor shall make itself available to attend Norwich Board of Civil Authority hearings to defend any and all appraisals.

#### 4.36 Appeals to Property Valuation and Appeal and/or Superior Court

4.36.1 The Contractor agrees to make available the services of a qualified supervisory representative to support the values established for the revaluation tax year upon appeals to Property Valuation and Review or Windsor Superior Court.

#### 4.37 Project Plan

4.37.1 Within thirty (30) days of execution of this Contract with the Town, the Contractor shall provide a project plan for the project, which will also show the resource requirements necessary to complete this project on time. Once the revaluation project has commenced, the Contractor shall submit progress reports on a bi-monthly basis, relating such progress to the project plan. A tracking Gantt chart would meet these requirements.

#### 4.38 Appraisal Specifications

#### 4.39 Sales Study

##### 4.39.1 Sales Analyses

4.39.2 Prior to the field data collection phase of this project, an analysis of all recent

sales within categories of properties defined by PV&R shall be performed as a means of determining schedules. The Town shall make available to the Contractor a copy of all property transfers in its possession.

4.39.3 A sales analysis shall be conducted using accepted measurement methods in order to determine land, building and total real property values. Such accepted methodology shall include the consideration of all sales given by the Town to the Contractor and their inclusion in the sales analyses report with appropriate notations for those sales not used in the correlation of values.

4.39.4 The sales analyses report shall be conducted by a qualified appraiser supervisor.

4.39.5 All recent property sales shall be included in the sales analyses report by photocopy or printout of the property assessment record card and a photograph of the principal buildings shall be attached thereto.

#### 4.40 Land Sales Study

4.40.1 Unimproved land values shall be determined from land only sales whenever possible. In the absence of an adequate number of land sales, the appraiser may use the abstraction method or the land residual technique to assist in the determination of land values. The analysis shall show the sale price, adjustments made and final value as of the effective date of the revaluation.

4.40.2 The completed sales Analyses Report showing the sales used to determine property values shall be delivered to the Town for approval and shall at all times be the property of the Town.

#### 4.41 Land Value Study

4.41.1 Land value shall be determined on the basis of highest and best use for each parcel.

4.41.2 The Contractor shall conduct a sales study using accepted IAAO standards for developing land sales, and shall provide all information to the Board of Listers and enter all data affecting value into the chosen CAMA system.

4.41.3 The land value schedule must include separate values for up to two (2) acre site values, bulk land and land where all of the rights have been transferred to non-owners so as to comply with requirements of Property Valuation and Review and Vermont Statutes Annotated. In addition all site improvements shall be inventoried and valued accordingly for all zoning lots that comprise a parcel.

#### 4.42 Land Inspection

4.42.1 The Contractor shall be responsible for familiarizing itself and inspecting the physical attributes of each parcel (and any sub-parcel). This shall include but is not limited to topographical features, such as high banks and steep slopes, category of road access (road classification ( Class 1, 2, 3, 4, private, etc.)), proximity to utilities, approximate elevation, views, wetlands, soil conditions, irregular shapes, presence of utility easements or other factors which may influence the value of the land. Each abutting parcel listed under a single ownership shall be separately appraised as if it were a stand-alone parcel and the sum shall become the total of the owner's land value.

#### 4.43 Land Value Map

4.43.1 The Contractor shall delineate the land value units on all streets and acreage in the Town and enter it into the Town's updated ArcView mapping software. The Contractor shall identify all recent sales and all land values in the Town in the CAMA software and mapping program in such a way that maps and charts of the values can be produced.

#### 4.44 Appraisals

4.44.1 The Contractor shall make a careful study and investigation of the current costs of residential, commercial and industrial construction in Norwich, based upon delivered material costs and prevailing wage rates in the building trades. The Contractor shall develop direct unit costs, and shall include all indirect costs such as architects and engineer's fees and contractor's overhead and profits.

#### 4.45 Residential Property Cost Schedules

4.45.1 The Contractor shall use unit costs as the Cost Approach basis of appraisal for residential properties. Schedules shall consist of unit base prices from defined specifications for houses of various types and quality of construction and reflect the building customs and practices in Norwich. The schedules shall be complete for various story heights and calculation of improved ground area, exterior measure, adequate for the valuation of all types of houses with tables for additions and deductions for variations from base specifications including schedules for garages and all other accessory structures larger than 100 square feet.

#### 4.46 Appraisal of Residential Buildings And Structures

4.46.1 All structures shall be inspected, classified, delineated as to homestead (as defined by the State of Vermont), and reviewed to include the listing of physical construction details on each parcel in the Town. The measurements shall be to the nearest foot.

4.46.2 The Contractor shall prepare comparative unit cost schedules. Said schedules shall reflect the square foot (heated and unheated) areas of buildings. In structures over one-story usable areas will be calculated using a 5' drop as the vertical sideline of the wall structure. If, during the course of the market study, the basis for cost trending is noted, the resulting index numbers should be included in the study.

4.46.3 These schedules shall be used in computing the comparative unit cost in the Town for all residential construction.

4.46.4 All documentation utilized in compilation of the cost schedules shall at all times be the property of the Town. The Contractor shall submit written explanations of all formulae and calculations utilized in the project.

#### 4.47 Manual of Appraisal

4.47.1 The cost data and schedules developed for the valuation of residential, commercial, industrial, manufactured housing, land and exempt properties shall be entered electronically and with hard copy for use as an appraisal manual. Upon completion of the revaluation, the Contractor shall deliver two (2) bound copies of the manual to the Board of Listers. The Contractor shall instruct the Board of Listers in the use and interpretation of the manual.

#### 4.48 Types of Cost Schedules

##### 4.48.1 Residential

4.48.1.1 The Contractor shall implement a quality control program, approved by the Project Manager, to ensure that the data collection, entry and verification procedure is being conducted properly.

##### 4.48.2 Interior Inspection

4.48.2.1 The Contractor and the Board of Listers shall discuss and agree on the method of setting up inspections of individual properties. The Contractor shall make every effort to obtain permission to enter each parcel. The minimum number of telephone attempts shall be two and shall be accounted for on a daily log. The telephone calls that do not produce an inspection result shall be followed by a letter (from the Board of Listers) that shall be sent by certified, return receipt mail and shall contain language that will

explain the system of assessment when done without interior inspection and requesting a time and date for inspection. A similar letter (from the Board of Listers) shall be sent to all property owners who refuse interior inspections of their properties. It may be necessary for the Contractor to make these telephone calls during evenings and weekends and must take into account Federal, State and school holidays. The Contractor shall make a careful inspection of the complete interiors of 100% of all structures excluding those where permission to enter has been refused or cannot be obtained.

4.48.2.2 When entrance to a structure for a scheduled inspection is refused, or no authorized person was available at the time of the inspection the Contractor shall make note of the fact and within two (2) working days notify the Project Manager of the fact in writing, giving the facts as to the time of the visit and if possible, the name of the party refusing entrance and other pertinent information. The Project Manager shall review the situation, and if he shall be unable to gain the cooperation of the party involved, he shall so notify the Contractor, and they shall proceed to estimate the value of the building on the basis of facts ascertainable without entry and make adequate notations of the lack of cooperation, and the manner of arriving at value.

4.48.2.3 The data collection card shall indicate the initials of the Contractor's inspector and at least one Lister, if part of the inspection team, and the date(s) and time of the inspection. Every attempt shall be made to have the property owner or his/her representative sign the data collection card.

#### 4.48.3 Exterior Inspection

4.48.4 The perimeter of all improvements shall be carefully measured. The Contractor is responsible for the accuracy of all exterior information.

4.48.5 All residential buildings and improvements shall be measured to the nearest foot. In many instances the Contractor shall encounter structures where greater accuracy is required so as to account for parts of a structure that do not meet at a 90° angle. Story heights and configurations of the various sections and subsections shall be noted on the data collection card.

4.48.6 An outline sketch, with dimensions noted, or electronically prepared to scale, shall be made. This sketch shall contain enough information for entry into the CAMA software drawing program.

4.48.7 Other man-made improvements to the property shall be similarly measured and accounted for.

#### 4.49 Review

4.49.1 All data collected at the inspections shall be reviewed by the Contractor and the Assessor/Board of Listers. Should deficiencies in accuracy be discovered, the Contractor shall reinspect the property.

#### 4.50 Pricing and Valuations

4.50.1 Pricing and valuations of all land and buildings must reflect the fair market value as of April 1, 2022 or a subsequent year as negotiated with the selected Contractor and shall be done from and in accordance with the approved manuals and schedules.

4.50.2 The final valuation shall be the market value of the structures plus the market value of the land including site improvements plus any premium for improved parcels, water access/view, or other external reasons including but not limited to zoning restrictions or ease of access. In arriving at the assessed value of the property, all applicable valuation methods shall be used.

#### 4.51 Appraisal Of Commercial Properties And Those That Will Require Additional Attention

#### 4.51.1 General

All structures other than residential shall be inspected, classified, valued and reviewed in the same manner as residential properties as set forth above. The dimensions of all buildings shall be to the nearest foot and recorded on the data collection card. The height of each building story shall also be recorded on the data collection card.

4.51.1.1 All structures shall be identified and described as to component parts of construction, size, area, age, usage, and present occupant(s) on the property forms, as prescribed in this Request for Proposal.

#### 4.52 Commercial

##### 4.52.1 Cost Approach

4.52.1.1 Commercial building cost schedules shall be prepared in comparative unit costs of materials in place and charted on a per square foot basis, and shall be prepared for various story heights and contain all the additions and deductions for construction components from base specifications.

##### 4.52.2 Income Approach

4.52.2.1 The Board of Listers shall perform an income approach as a verification of the cost approach results calculated by the Contractor and the appropriate value as determined by the Board of Listers will be used for the assessed value.

4.52.3 The Contractor shall develop gross rent multipliers by interviews with local bankers, buyers/sellers, investors, realtors, and appraisers.

#### 4.53 Special Purpose Properties

##### 4.53.1 Definition

4.53.1.1 This section includes, but is not limited to, those properties that are special purpose in nature and will be assessed using the cost approach.

##### 4.53.2 Exempt Parcels

4.53.2.1 Properties that are not taxable and properties that are exempt (in whole or in part) from real estate taxes either by statute or vote shall be valued as all other like properties without regard to their exemption status.

##### 4.53.3 Farms

4.53.3.1 Cost schedules for farm structures regardless whether or not they are enrolled in the Vermont Use Value Appraisal Program shall be prepared for square foot costs for various types of buildings, including, but not limited to: barns, sheds, milk houses, silos, trenches, coops, etc.

##### 4.53.4 Condominiums and Other Like Real Estates

4.53.4.1 The Sales Comparison Approach shall be used to value residential and commercial condominiums where applicable. Sales Comparison Approach shall include only recent Norwich sales unless few or none exist. If necessary and with permission of the Board of Listers sales values from outside Norwich town boundaries may be included. For each complex there shall be a computer record and hard copy of that record, showing the project diagram, percent of ownership, phase of development, common ownership amenities, and assessed values for each unit type and each unit as well as individual computer record for each unit. The Contractor shall familiarize itself with the applicable declaration of condominium, covenants and restrictions.

4.53.4.2 Where real property is owned by several owners holding joint or undivided interest, that property shall be valued as a separate parcel and listed as one parcel under the names of the various owners. The Board of Listers shall determine which of the

owners of record shall receive the property tax bill and that owner shall be recorded as primary owner in the CAMA software.

#### 4.54 Depreciation Schedules

4.54.1 The preferred method is an overall depreciation schedule abstracting estimated land value and site improvements value from the sale property and then comparing the residual improvement value to a comparable new improvement.

#### 4.55 Contractor Deliverables

4.55.1 The Contractor shall provide two hard copies and a digital copy of the Tax Maps updated to reflect the current conditions as of the date of the reappraisal.

4.55.2 The Contractor shall provide updated CAMA software that reflects the current ownership and assessed values for each parcel to reflect the current conditions as of the date of the reappraisal.

4.55.3 The Contractor shall provide a digital (text) copy of those fields necessary to create the Grand List for the year of the reappraisal and shall assist the Town, State, and/or other personnel in making the data transfer for a period of three years including the year of the reappraisal.

4.55.4 The Contractor shall provide the Town with a prepaid license agreement for the CAMA software and any supporting software for a period of three years from the date of the lodging of the reappraisal Grand List with the Norwich Town Clerk.

4.55.5 The Contractor shall provide the digital photographs as described above arranged by address in a separate digital file that shall be maintained on the Town's server/cloud.

#### 4.56 Plat Plan

4.56.1 A plat plan shall be made of all commercial properties where more than one building is present showing the location of and identification of the buildings and yard improvements.

#### 4.57 Site Improvements

4.57.1 All site improvements shall be listed and valued separately. Site improvements shall include but are not limited to paving, retaining or decorative walls and fences, plantings and other landscaping, underground utilities, grading.

4.57.2 The Contractor shall make an inventory of all ponds in Norwich, the parcel(s) on which each occurs, an approximation of each pond's acreage and contributory value to the overall land value.

#### 4.58 Review

4.59 All data collected at the inspections shall be reviewed by the Contractor and the Board of Listers. Should deficiencies in accuracy be discovered, the Contractor shall re-inspect the property.

#### 4.60 Control And Quality Checks

##### 4.60.1 Field Checks

4.60.2 The Board of Listers shall spot check in the field, properties picked at random by him or her, with or without an agent of the Contractor being present.

#### 4.61 Building Permits and Changes in Property Lines

4.61.1 The Town shall provide to the Contractor copies of all building permits issued during the course of the revaluation to allow the inclusion of all new construction, additions, and remodeling in the Contractor's appraisals. Similarly the Town shall provide changes in property line locations as soon as they come available.

#### 4.62 System Recovery And Backup

4.62.1 The Contractor shall maintain its own off site data and CAMA software backup procedure and assist the Town in completing a system recovery if necessary at no additional cost. At the completion of the contract the Contractor shall, by affidavit, certify that any and all analysis and data has been turned over to the Town of Norwich.

**4.63 Incomplete Construction**

**4.63.1** The Contractor shall designate on its data collection card and electronically in the chosen duly licensed software, each property which has incomplete improvements on the April 1<sup>st</sup> Grand List. The final card shall show the percentage of completion and reflect the percentage of completion in the valuation as of that date.

**5. PROPOSAL FORMAT**

Date:

Town of Norwich  
Board of Listers  
300 Main Street  
P.O. Box 376  
Norwich, VT 05055-0376

We propose and agree to furnish, conditioned upon your acceptance of our Proposal and the execution and approval of a contract, appraisal services fully complying with this Request for Proposals and written agreement signed by both parties and any amendments thereto signed by both parties entitled "Town-wide Reappraisal for the Town of Norwich, Vermont" dated

\_\_\_\_\_, \_\_\_\_ for the amount of \$ \_\_\_\_\_. Any modifications and "as equal" substitutions are fully listed and detailed in the attached sheets entitled Modifications and Substitutions to Specifications.

**1. Stages of Completion: Fee Breakdown**

The Contractor shall prepare a proposed fee schedule for each of the items listed in the following table. The schedule shall show the amount in dollars that when added together the dollars shall sum to the total of the contract.

**2. Total Contract Price**

The proposal price shall be based on the number of active and inactive zoning lots which comprise the parcels in the 411, which is attached hereto. Excluded parcels include the following: \_\_\_\_\_. Adjustments to the contract amount will be made based on the number of parcels on the lodged Grand List based on the 411. The per-parcel costs shall include all elements of the specifications including informal grievance meetings, formal grievance hearings, Board of Civil Authority hearings, CAMA software, GIS software, guarantees and warranties and all items necessary for completion of the project.

**3. Cost of Additional Options (If applicable)**

Description	Cost

**4. Personnel**

The personnel from supervisory positions to data collectors, who may be assigned to this project are as follows:

Name	Position	Percent of Time on this Project

Attach resumes for all personnel listed above.

This Proposal shall remain in full force and effect for one hundred and twenty (120) calendar days from May 14, 2021.

Attached to this Proposal is a detailed description of the proposed town-wide reappraisal services to be furnished for the Town of Norwich, Vermont.

Attach all information requested in the Request for Proposals.

**5. Modifications and Substitutions**

Paragraph	Clarifications or Exceptions (Attach additional sheets if necessary.)

SIGNATURE PAGE

Submitted by:

\_\_\_\_\_  
Signature

\_\_\_\_\_  
Name

\_\_\_\_\_  
Title

\_\_\_\_\_  
Street Address

\_\_\_\_\_  
City, State Zip

\_\_\_\_\_  
Telephone Number

\_\_\_\_\_  
Fax Number

\_\_\_\_\_  
Email

## 6. FORM OF CONTRACT

### CONTRACT

CONTRACT made this \_\_\_\_\_ day of \_\_\_\_\_, 2021, by and between the TOWN OF NORWICH (hereinafter called "TOWN") and \_\_\_\_\_ whose mailing address is \_\_\_\_\_ (hereinafter called "CONTRACTOR").

The TOWN and CONTRACTOR, in consideration of the mutual undertakings hereinafter set forth, agree as follows:

**1. The Project:**

A full revaluation of all taxable, tax exempt and non-taxable zoning lots which comprise the parcel(s) for tax assessment purposes, in accordance with the standards set forth in the laws and regulations of the State of Vermont.

**2. Contract Documents:**

This Contract shall consist of the following contract documents:

- This executed Contract
- Exhibit A - Specifications and Request for Proposals for Norwich Real Property Reappraisal
- Exhibit B, the Contractor's Proposal dated \_\_\_\_\_, \_\_\_\_\_ in response to the Town's Request for Proposals For Norwich Real Property Reappraisal
- Exhibit C, any modifications to Exhibits A and B.

**3. Payment:**

The Town of Norwich agrees to purchase and pay for the subject project for the total sum of \$\_\_\_\_\_ in the following manner:

The Town shall make progress payments to Contractor towards the total balance due on the Contract less ten percent (10%) of the material and labor amount which shall be withheld by the Town. The remaining ten percent (10%) of the total Contract amount will be paid by the Town to the Contractor within thirty (30) days following the satisfactory completion of all terms of the Contract. Satisfactory completion of all terms of the Contract shall not be unreasonably withheld.

**4. Satisfactory Completion:**

Satisfactory completion of all terms of the Contract shall include delivery of all terms of this Contract as specified up to and including defense of values before the Town of Norwich Board of Civil Authority. Each appeal taken beyond the Board of Civil Authority shall be defended by the Contractor and invoiced at the completion of each appeal. The Town shall not owe interest to the Contractor on any retained amounts of money due to the Contractor under this Contract.

**5. Insurance:**

The Contractor and subcontractors shall have insurance coverage as required by these Specifications and Request for Proposals.

**6. Identification:**

Name of Town: Town of Norwich, Vermont  
Address of Town: 300 Main Street  
Post Office Box 376  
Norwich, VT 05055-0376  
Telephone: (802) 649-1419 Ext. 6

Contracting Officer for the Town: Norwich Board of Listers

Name of Contractor: \_\_\_\_\_  
Address of Contractor: \_\_\_\_\_  
Telephone: \_\_\_\_\_

**7. Name and Title of Contractor Signor:**

The Contractor shall furnish to the Town, town-wide reappraisal services and hardware (if any) as set forth in the Request for Proposals, Bid Form and Proposed Contract for Town-Wide Reappraisal Services for the Town of Norwich, Vermont dated \_\_\_\_\_, \_\_\_\_\_ a copy of which is attached hereto and the terms of which are incorporated herein, and the Contractor's written Proposal accepted by the Town, a copy of which is attached hereto and the terms of which are incorporated herein.

**8. Commencement:**

Commencement of services, as specified shall begin on \_\_\_\_\_, \_\_\_\_\_.

**9. Deliverable Products:**

All documents, records, data, and other material, in either manual, mechanized, or electronic form, procured or produced in the performance of this project shall be the sole property of the Town at all times, as determined by the Board of Listers. There shall be no distribution or disclosure without prior written approval of the Board of Listers. Upon completion of all services to be performed under this agreement, or termination of this agreement, all documents, records, data and other materials without limitation, shall immediately be returned to the Town as follows:

- Documentation of procedures used throughout the project,
- All training materials and manuals used in any phase of the project,
- The data collection and valuation manuals that shall enable the municipality to maintain and update values,
- Detailed valuation manuals, including tables and formulas to be used in applying the cost, sales comparison, and income approaches to appraisal problems.
- Source information used in the development of cost, sales comparison, and income approach schedules, source information for individual property valuations,
- A property field inspection card (field data source document used by data gatherers) and a final computer-generated property record card for each parcel,
- All manual and computerized reports supporting valuation formulas and values for vacant land and improved properties,
- All sales ratio studies used in the project,
- Field review documents reflecting preliminary values, adjusted preliminary values, and any notes relative to informal review actions,

- Electronic files of all properties, their identification, improvement inventories, computer-generated valuation schedules, and technical documentation describing data file organization and record layout to facilitate user understanding,
- A report of all informal hearings held with the Contractor's representative showing the number of hearings, the number of values changed by more than ten percent (10%) and the number of values unchanged.
- A full complaint resolution report relating to all aspects of the revaluation.

**10. Discretionary Arbitration:**

- A. Any dispute arising from this Contract or potential breach thereof may be submitted, by agreement of both parties to this Contract, to arbitration.
- B. If the arbitration process is agreed upon to resolve the dispute or possible breach regarding the Contract, the parties shall select an arbitrator. In the event the parties are unable to agree on the arbitrator then either (1) application can be made to the Windsor Superior Court to appoint an arbitrator; or (2) a panel of three arbitrators may be chosen with each party choosing a member and those two members choosing the third member. The arbitrator(s) shall adopt appropriate arbitration rules similar to the American Arbitration Association or other customary arbitration proceeding.
- C. Unless otherwise agreed to in writing by both parties, the place of arbitration shall be in Norwich, Vermont and the arbitration hearing shall be held within forty-five (45) days after acceptance by the arbitrator or as early a hearing date as is practicable, which the parties may postpone only for good cause shown.
- D. Any lawful decision of said arbitrator(s) shall be binding upon the parties and, subject to review by the Windsor County Court, shall be enforceable as part of this Contract.

**11. Amendment of Contract:**

This Agreement, including all Amendments, embodies the entire understanding between the parties relating to the subject matter contained herein and merges all prior discussions and agreements between them. No agent or representative of the Contractor has the authority to make any representations, statements or agreements not expressed herein. All modifications or amendments of this Agreement must be in writing and must be signed by an authorized representative of each party.

**12. Assignment:**

This Agreement and the duties of the Contractor hereunder shall not be assigned or subcontracted.

**13. Severability:**

In the event any provision of this Agreement is held to be invalid, illegal, or unenforceable in any respect, such invalidity, illegality, or lack of enforceability shall not affect any other provision, and this Agreement shall, in all other respects, be valid and enforceable.

**14. Applicable Law:**

This Contract is made and executed in the State of Vermont and shall be construed and interpreted in accordance with the laws of Vermont.

**15. Headings:**

The topical headings used in this Agreement are for convenience only and shall not be construed as having any substantive significance or as indicating that all of the provisions of this Agreement relating to any topic are to be bound in any particular section.

**16. Agreement Signature Page:**

In witness thereof the Town of Norwich has caused to be signed by the Board of Listers thereunto lawfully authorized and caused its seal to be affixed by its Town Clerk and [the Contractor] has caused the same to be signed by its lawfully authorized representative and its seal (if appropriate) to be hereto affixed on the date and year first above written.

**TOWN:**

Town of Norwich  
P.O. Box 376  
Norwich, VT 05055-0376

**CONTRACTOR:**

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

By: \_\_\_\_\_  
Signature

By: \_\_\_\_\_  
Signature

\_\_\_\_\_  
Printed Name

\_\_\_\_\_  
Printed Name

\_\_\_\_\_  
Title

\_\_\_\_\_  
Title

[CORPORATE SEAL]

[CORPORATE SEAL]

1. **Planning Commission** last meeting held January 28<sup>th</sup>. Major work includes ongoing discussion of the MCS wastewater situation and what might be an appropriate PC response.
2. **Development Review Board** Next meeting is scheduled for February 4 with another scheduled for February 18. Matters under consideration include a new application for two daycare facilities at 251 Route 5, a subdivision and an appeal of the Zoning Administrator decision concerning a Black Lives Matter banner at 236 Main Street. My decision was that the banner is exempt, the complainant is appealing that decision.
3. **Planning Director:** I am preparing RFQ documents for the grant funding research into the density factor in the subdivision regulations. I am providing further support to the PC on the issue of wastewater assessment for the village area and working on other Town Plan action items including the bridge over Bloody Brook and CRS certification (for which I have training February 23 and 24, 2021).
4. **Zoning Administrator:** There is one active appeal of ZA decision, a very high zoning enquiry load and strong level of permitting activity.



From: Alexander Northern JD, MPA  
Town of Norwich Fire Chief  
Deputy EMD  
Deputy Fire Warden

To: Town of Norwich Selectboard  
Town Manager

Re: January 2021 Department Report

Date: February 2, 2021

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Part of my responsibility as the town Fire Chief is to consider master planning for fire defense. Fire department master planning is the compilation of a department's operational plans (i.e., action plans and short-, medium-, and long-range plans) as well as its strategic plans; each is a necessary component of the master plan. Fire department master planning differs from community master or comprehensive planning. Each type of operational plan is defined differently in the fire department model than the same-named models associated with for-profit business plans. The major difference is the time horizon of each type of plan. Operational plans are SMART:

- Specific.
- Measurable.
- Achievable.
- Results/outcomes are defined.
- Time-limited.

EMS CALLS	7
FIRE CALLS	12
FIRE MUTUAL AID	3

*Alexander Northern*

Town of Norwich Fire Chief



# NORWICH POLICE DEPARTMENT



CHIEF OF POLICE  
JENNIFER FRANK

P.O. Box 311 ~ 10 Hazen Street ~ Norwich VT 05055 ~ 802-649-1460 ~ FAX 802-649-1775 ~ E-MAIL [jennifer.frank@vermont.gov](mailto:jennifer.frank@vermont.gov)

01/28/2021

REF: Chief Frank Resignation letter – Accepted position as Chief of WPD

TM Durfee,

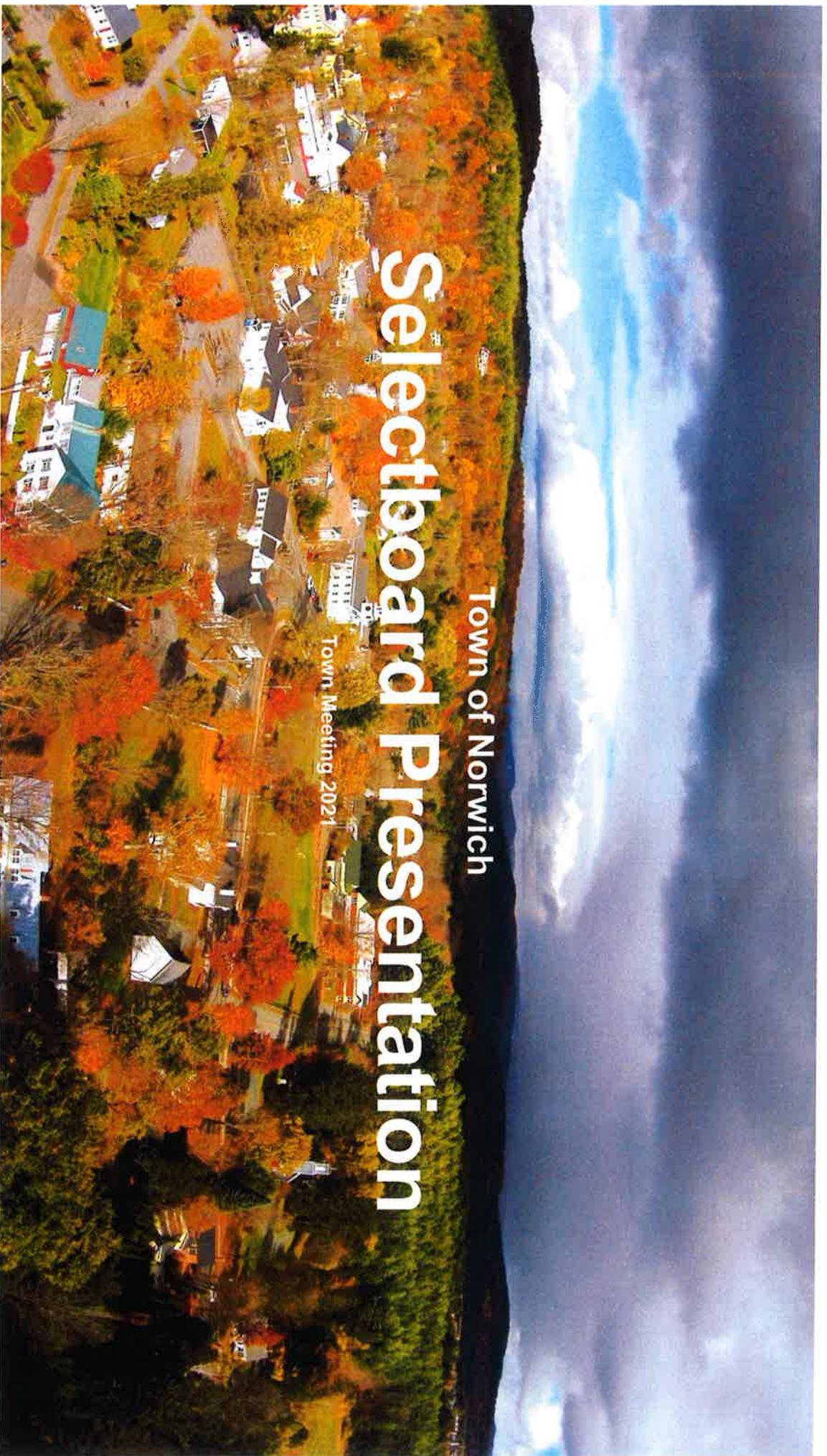
It has been an honor and a privilege to have had the opportunity to serve as the Chief of Police for the Norwich Police Department. I have been privileged to work with a contingent of officers that are fair, just, professional, well trained, committed to excellence, and to serving the residents of the community.

Over the last three years, we have accomplished much together, building off of the successes of the officers and Chiefs that came before us, while furthering new initiatives and advancing community policing. While it is difficult to summarize all that we have accomplished together, a few highlights stand out:

- Drafted and updated 25 Standards Operating Procedures to ensure that they are compliant with current legislation and best practices.
- Obtained more than \$200,000 in grants covering equipment and training expenses that would have otherwise been expended out of the operational and capital budgets.
- Brought NPD equipment up to current standards purchasing new: Ballistic helmets and vests, cruiser, duty-phones, firearms – handguns, in-car camera systems, mobile data computers, public safety building security system, and tactical entry kits to name a few.
- Officers received specialized training, attending courses provided by multiple agencies to include the: Department of Safety, Federal Bureau of Investigation, Federal Emergency Management Agency, Federal Law Enforcement Training Center, Homeland Security, National Criminal Justice Training Center, PACIF, and the Vermont Criminal Justice Academy.
- Increased community policing efforts offering a variety of programs to include: Civilian response to active shooters, Babysitting certification courses, Bike Safety Day, Cadet Program, CHILD ID Safety Kits, Coffee w/ a Cop, Drug Take Back Day, Impaired driving prevention, Junior Detective Challenge, Operation Safety Cone, Police Pen-Pal Program, Ride-A-Long Program, Seatbelt Challenge, Self-defense, Senior Safety School, Student Safety Patrol, and Touch-A-Truck.

Recently, I was offered a position by the Windsor, Vermont Police Department to assume the role of Chief of Police their agency. While I will deeply miss the officers in this department, the town staff, and the community at large; I am excited for this new challenge and the opportunities it presents. After careful consideration of my current position, and the opportunity to assume the role of Chief of Police for the Windsor Police Department, I have accepted their offer of employment and will be officially resigning from employment with the Norwich Police Department on 03/02/2021. When I began my employment with this department on 03/01/2018, I promised to commit three years to you, and to the agency. I could not have imagined three years ago how tremendous an opportunity and an experience it would be. Thank you for your leadership, and for your guidance over the last three years. It has been an honor and a privilege to work with you.

  
Chief Jennifer Frank, Norwich, VT Police



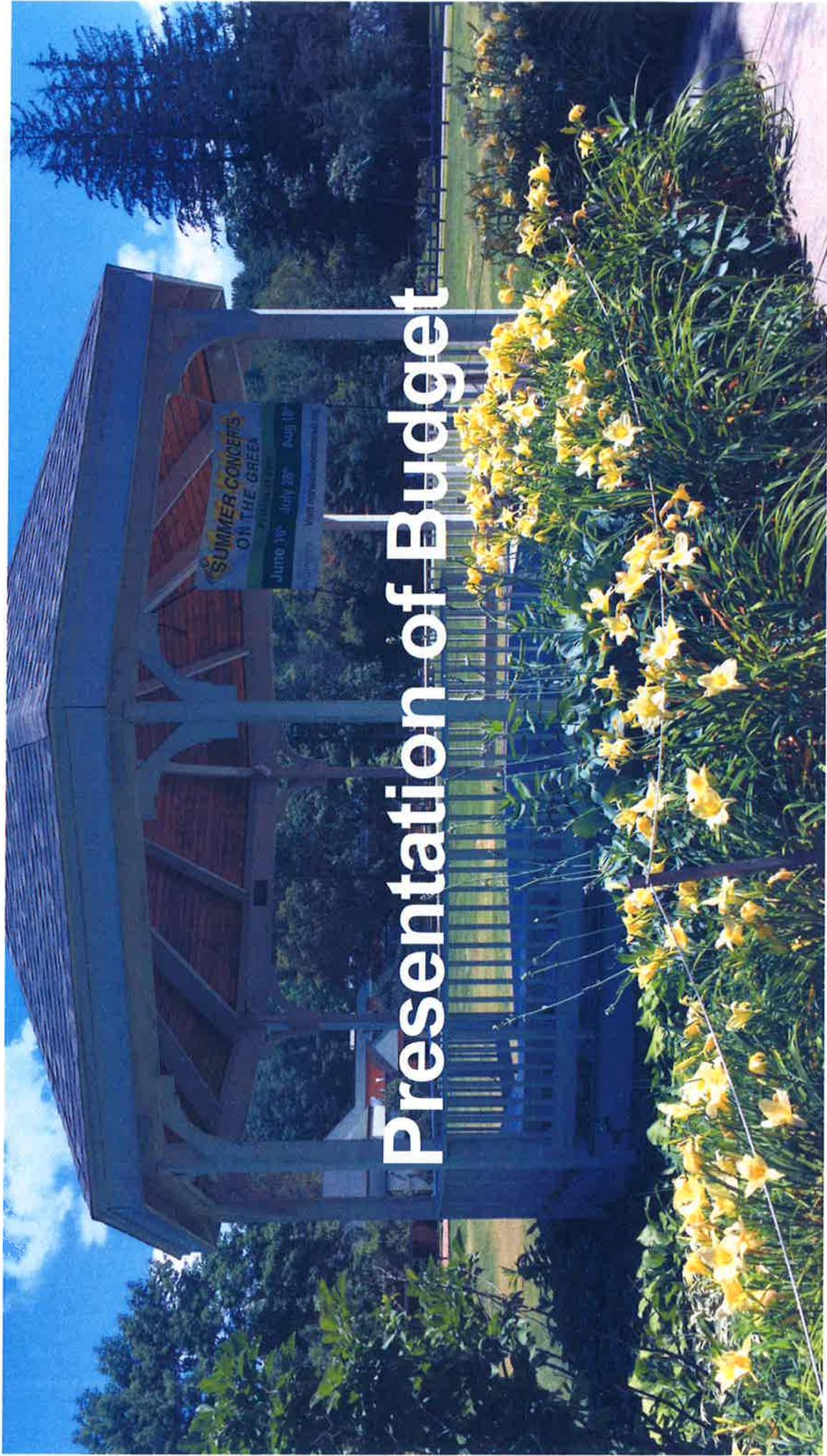
Town of Norwich

# Selectboard Presentation

Town Meeting 2021

# Town Meeting 2020

- Presentation of FY21-22 budget
- Information on monetary articles

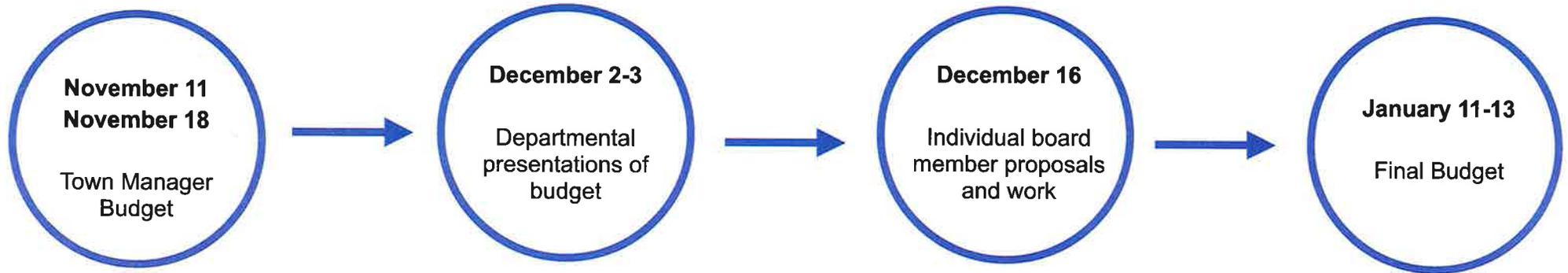


# Presentation of Budget

# Article 8

Shall the voters of the Town of Norwich approve a **gross spending general Town budget of \$4,780,866** plus state and federal grants and gifts consistent with budgeted programs for the period July 1, 2021 through June 30, 2022?

# Budget Process



*The Town Manager prepared an operational budget with a 20.99% increase, with individual department budget increases ranging between 9% - 264%*

*The Selectboard began a careful review of this proposed budget. Board asked questions and sought clarification from Department Heads*

*Individual board member input had a range of individual determinations, with from 4-17% increases.*

*Town Manager adjusted appropriations to paving and bridge funds following line item budget review from Board*

**FY21-22 operating budget has  
a proposed **increase of 6.4%**  
from the previous fiscal year.**

# Budget Priorities

- The impact of COVID-19 on the community
- Improve IT infrastructure, work stations, and server
- Allocate additional funds to properly staff finance office
- Adequately fund future infrastructure projects and/or informational studies
- Create long-term stabilization of the tax rate

## Article 8: Budget Priorities

Board sought to minimize spending to the extent possible in a year of unprecedented unemployment, rising health care costs, and concerns over property tax revenue. Board members were informed not only by Department of Labor statistics, but also stories from community members.

## **Article 8: Budget Priorities**

Based on an outside professional assessment, areas for improving our outdated IT infrastructure include: [updating software no longer serviced by vendor, hardware improvements, improving website functionality](#)

## **Article 8: Budget Priorities**

The Selectboard is committed to funding this office at a level necessary to attract and retain a qualified Finance Director. Despite five rounds of advertisement, we have yet to fill this critical fund and have worked with our Town Manager to develop interim solutions.

## Article 8: Budget Priorities

To better understand long-term infrastructure needs, gather third-party data, and maximize spending efficiency, there are [allocations to the Department of Public Works for traffic studies and road side safety management studies.](#)

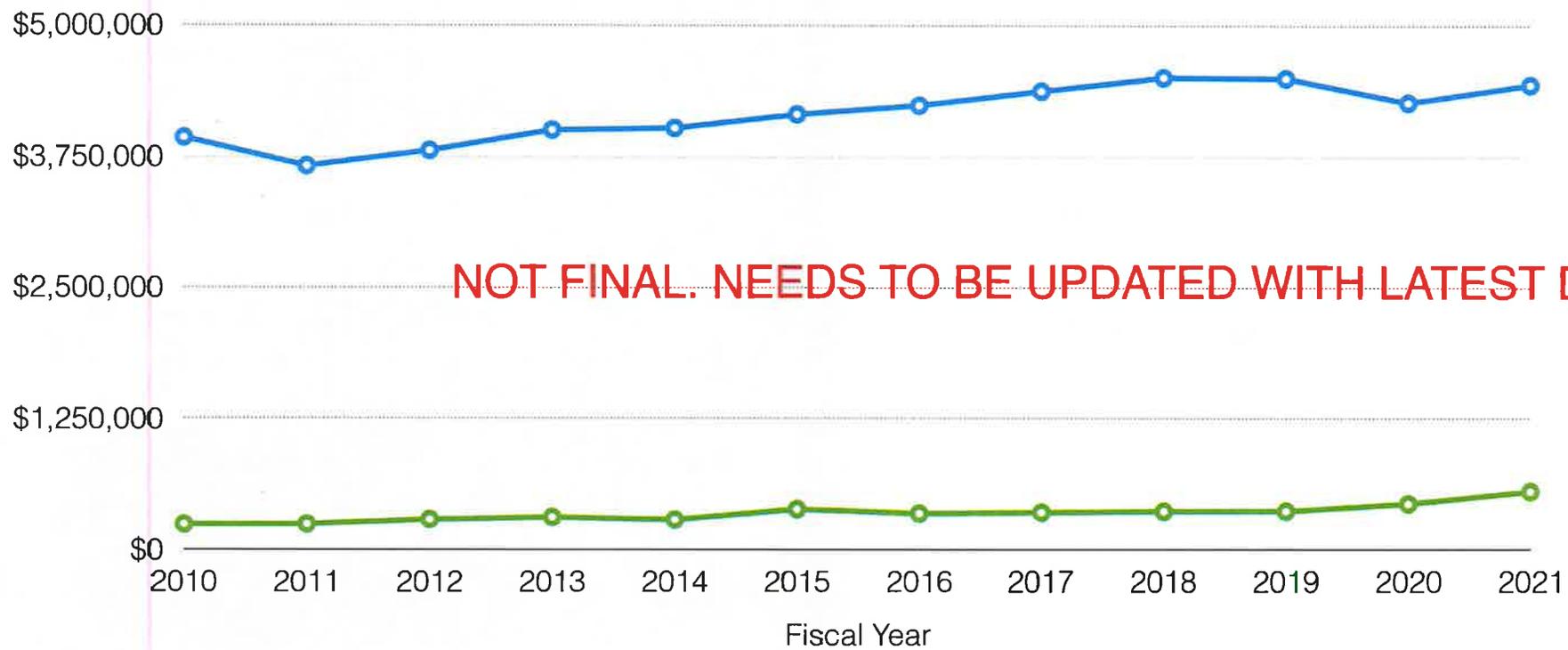
**Salaries and  
benefits  
represent  
tk% of this  
budget.**

Employee salary and benefits remain core ongoing outlays to the operational budget.

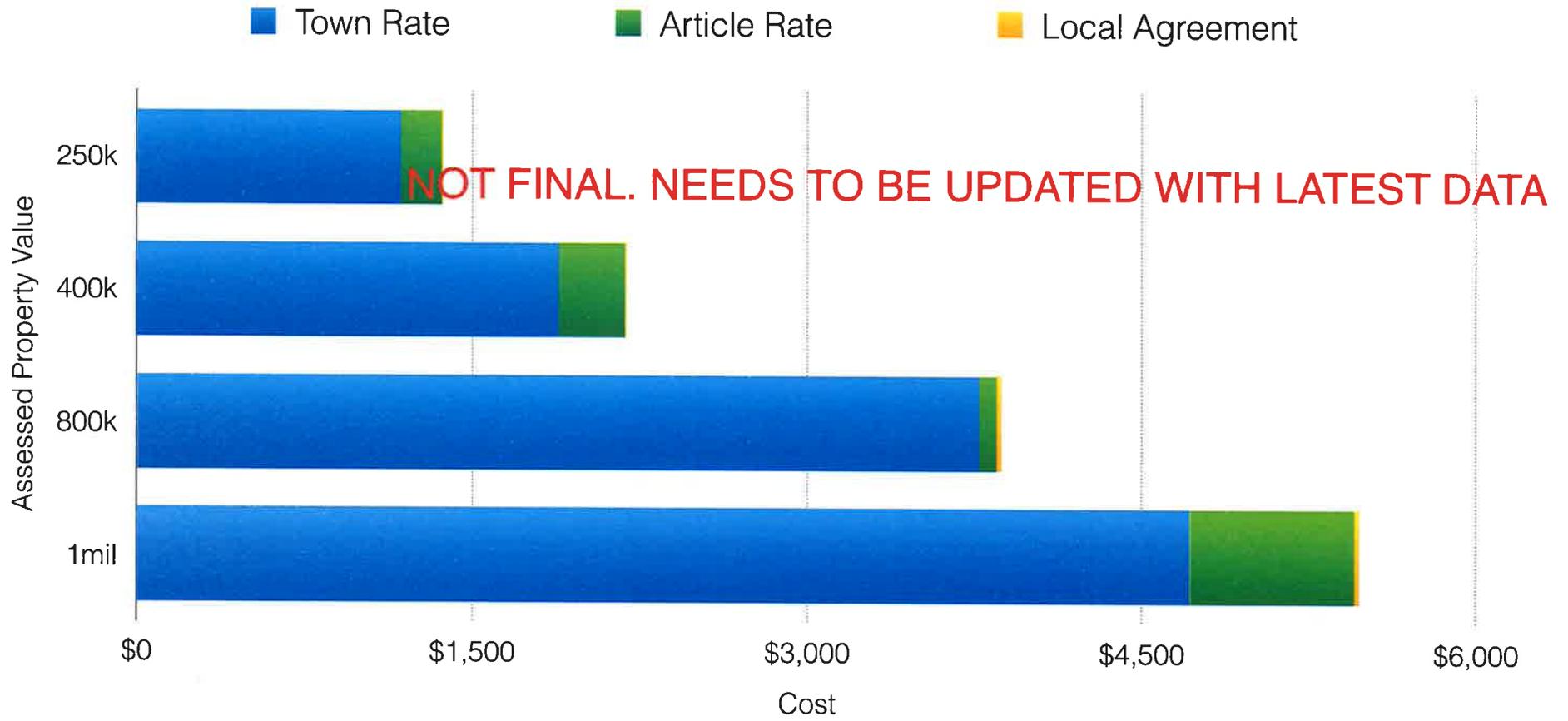
**[A point  
about the  
budget]**

Are there other budget items  
that Board members would like  
to point out?

○ Town      ○ Articles  
Norwich Town Expenditures



# Tax Impact



# Tax Impact

## Combined Town and Article Rate



NOT FINAL. NEEDS TO BE UPDATED WITH LATEST DATA

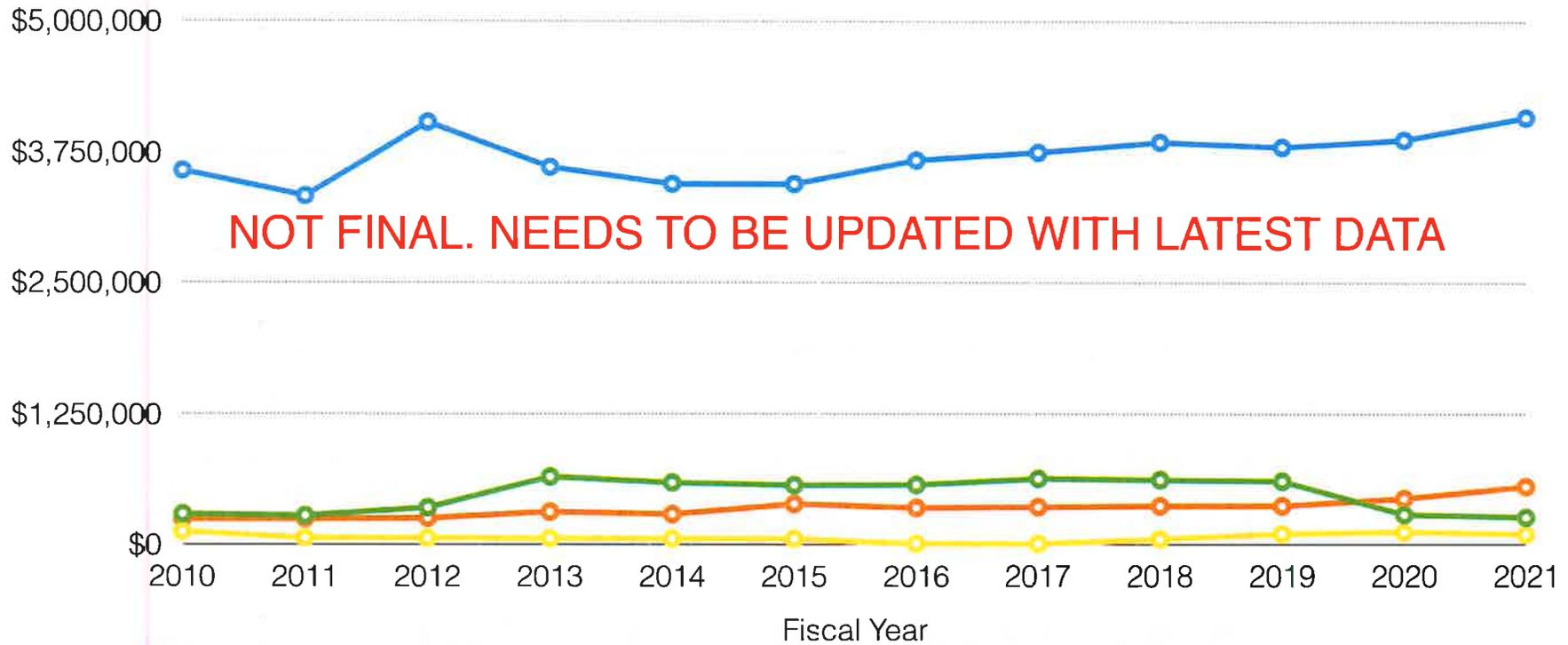
# Future Budget Governance

For the continual stabilization of the tax rate, the Selectboard will continue to work with Town Manager and other appointed and elected officials to understand the [important relationship between the Operating Budget, our Designated Fund Balances, and Capital Budgeting.](#)

# Future Budget Governance

Operations Designated Funds Appropriations Debt Service Articles

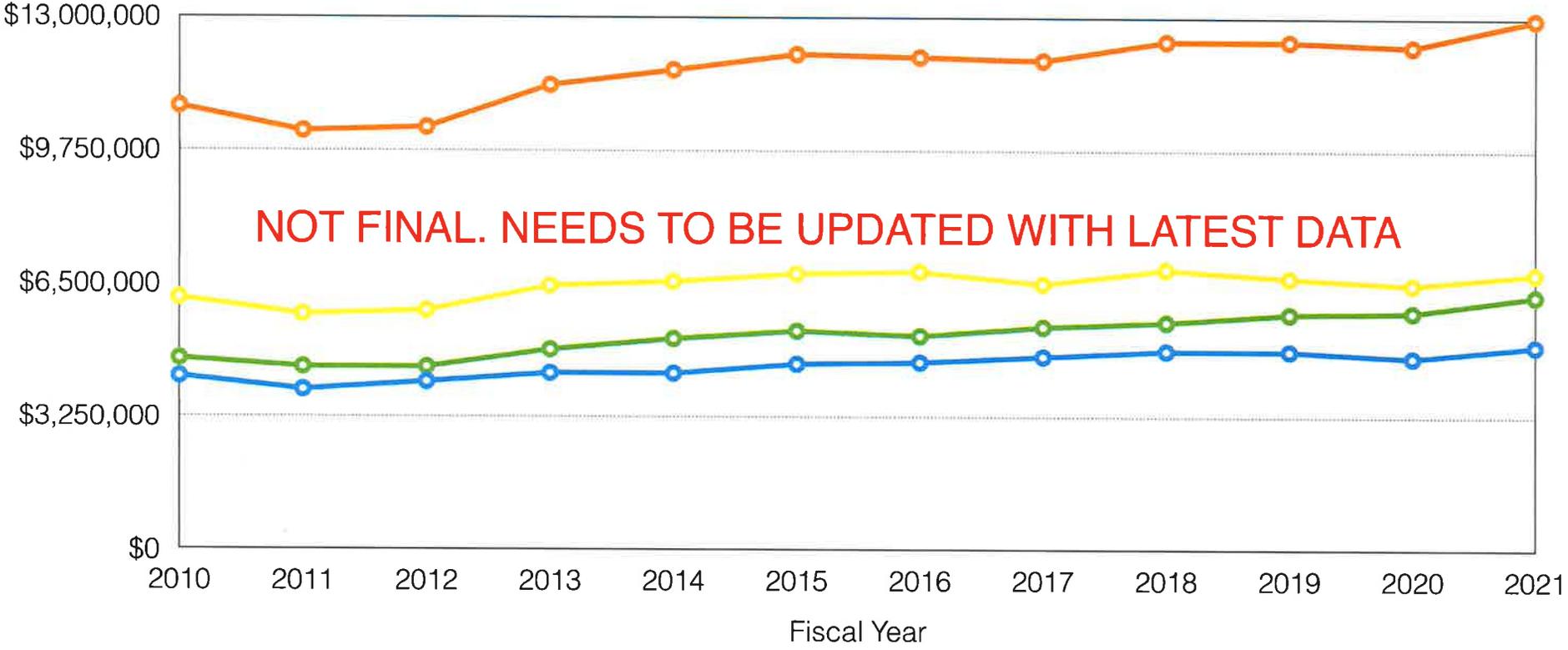
## Norwich Town Expenditures



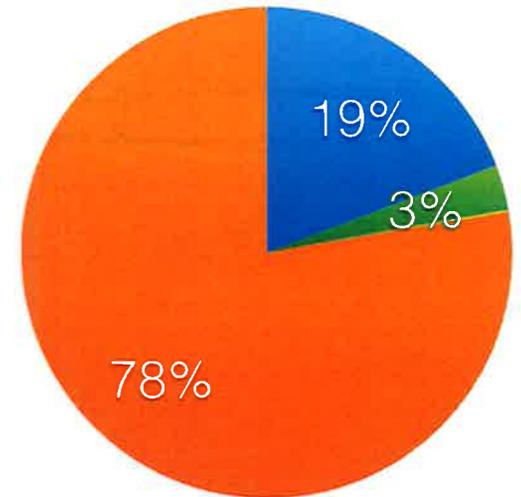
# Future Budget Governance

○ Total Town (with Articles)    ○ Marion Cross    ○ Dresden    ○ Total School

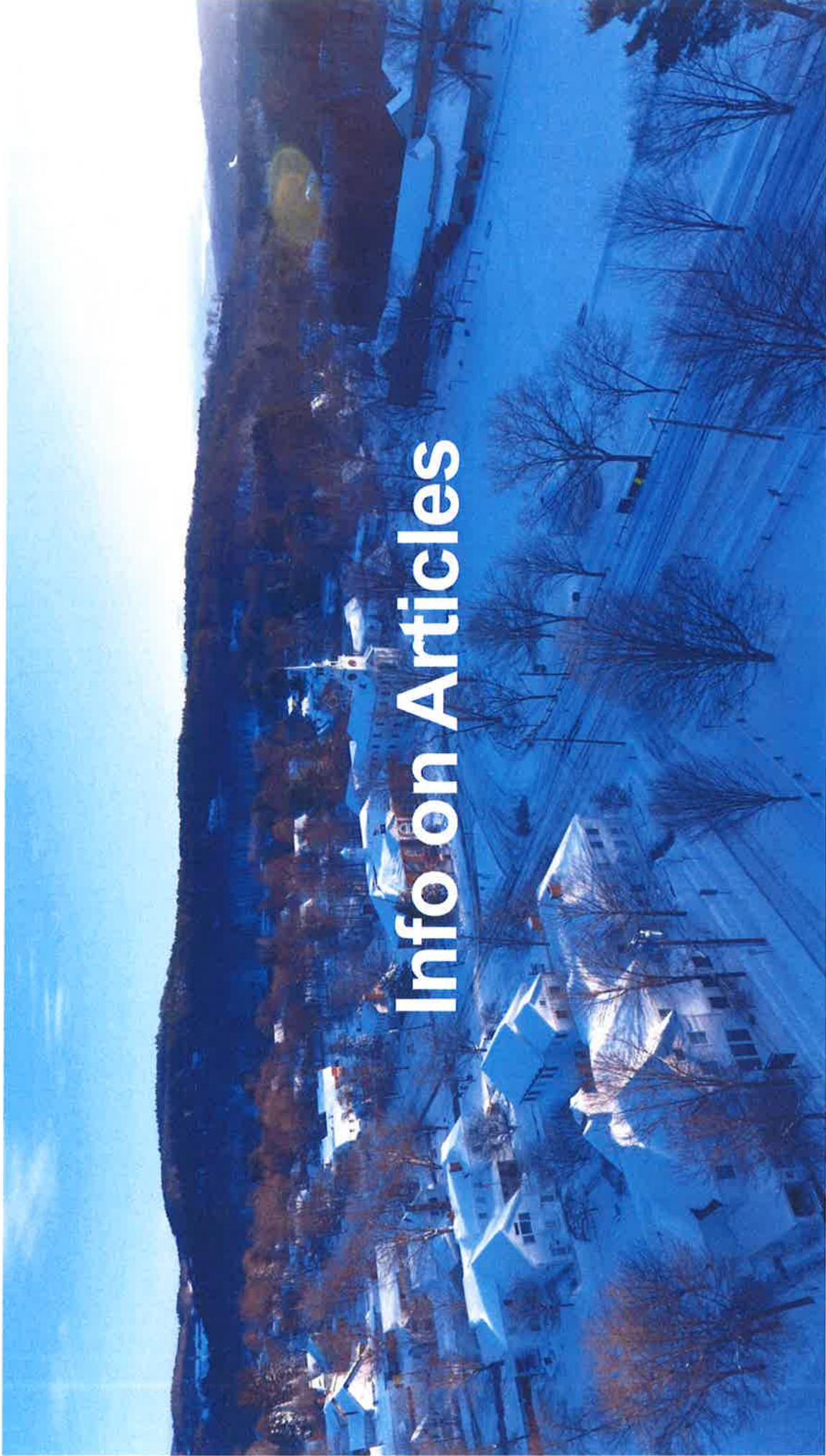
## Norwich Town & School Expenditures



# Tax Impact



For every dollar paid in taxes, approximately .78 cents goes to the State Education Fund and .22 cents is within the Selectboard's control.



# Info on Articles

# Article 9

Shall the voters of the Town of Norwich appropriate \$30,669.51 for Norwich's share of a Regional Energy Coordinator position serving 7 municipalities and based at the Two Rivers Ottawaquechee Regional Commission (TRORC) for a period of one year beginning July 1, 2021?

## Article 9: Regional Energy Coordinator

Article asks [Two Rivers Ottauquechee Regional Commission \(TRORC\)](#) to administer a shared Energy Coordinator for Norwich, Fairly, Sharon, Strafford, Thetford, and Woodstock.

Norwich's share is \$30,669.51, proportional to our town's Equalized Education Grand List ("EEGL") amongst the seven towns, estimated 26.24% of the cost.

[Voters authorized the regional energy coordinator position at last year's town meeting](#), but then-unresolved questions at the start of the program did not allow for Norwich's timely participation.

# Article 11

Shall the voters of the Town of Norwich authorize the Selectboard, pursuant to 24 VSA § 2741, to enter into contracts, as application is made, for alternate energy generating plants: by fixing and maintaining the rate of tax applicable to such property, with the intent to ensure, among other things and as applicable, that the municipal tax treatment of off-site solar installations is equal to the municipal tax treatment of on-site solar installations?

## **Article 11: Authorization for Selectboard to enter into contracts to stabilize taxes of solar equipment**

There are three situations where solar equipment can be tax exempt under statute 1) it's small, defined as less than 50kw, 2) the town votes to exempt solar equipment from taxes and 3) the town votes to allow its Selectboard to enter into contracts to stabilize taxes on solar equipment

In 2007, the town voted to exempt solar equipment for solar generation but only solar "used on the premises."

This article does not define which off-site solar projects should receive tax-stabilization contracts, the level of tax stabilization, or how long a contract should run, but rather asks voters to authorize the Selectboard to consider all details and make determinations.



Questions? Please ask them!