

Agenda for the Selectboard Meeting Wednesday, June 26, 2019 6:30 PM
Tracy Hall, Multipurpose Room

1. Approval of Agenda (Action)
2. Public Comments for items not on the Agenda (Discussion)
 - Administrative Matters
3. *Appointments to Open Positions – Advance Transit Bd.; CT River Joint Commissions (Action)
4. *VT DEC Stormwater Grants In Aid – Approval to Participate (Discussion/Action)
5. *2019 Paving Program – Use of Paving Designated Fund (Discussion/Action)
6. Consent Agenda (Action)
 - a. *Correspondence
 1. Energy Committee – quarterly report
 2. Correspondence regarding crosswalk RRFBs, from the following persons: Ann Marie Smith; John Trautlein; Din Seaver; Harry Roberts; Peter DeLong; Vickie Seaver; Sheilah Rahfield; Folger & Liz Tuggle; Rob Nahabedian; Gina Sonne; Rick Greenwald; Robert Lewy; Sandra Dell; Kathleen Foltz; Deirdre Goodrich; Amy Stringer & Jaan Laaspere; Omer Trajman; Susan Jorgensen; Kate & Andy Robinson; Ruth Dixon-Vestal; Charlotte Goodrich; Don Herzberg; Elaine Kennedy; Jon Voelkel; John Olszewski; Jon Kaplan, VTrans; Arwen Northern; Peter White; Lee Steppacher; Allison Katz; Margaret Cheney; Ted Austin; Allison MacCormick; Rob MacCormick; Cindy Shannon; Miranda Bergmeier, Asst. to the Town Mgr.
 3. John Farrell – Norwich Farms signage
 4. Bartlett Leber – resignation from CRJC
 - b. *Accounts Payable / Warrants
 - c. *Minutes – 6/12/2019
7. Town Manager Report (Discussion)
 - a. Fee Schedule Update – handout to be available at meeting (Discussion)
- Policy Matters
8. *2019 TRORC Proposed Regional Plan – Joint Discussion with Planning Commission and Rod Francis, Planning Director (Discussion)
9. *Selectboard Goals Discussion - Affordable Housing (Discussion)
10. Selectboard Policies Status (Discussion/Action)
 - a. *Master Financial Policy – adopt master framework (Discussion/Action)
11. *Verizon 60-day Advance Notice re: Cellular Attachment & Utility Pole Replacement on Lary Lane (Discussion/Action)

12. Future Meeting Agenda Items (Discussion)
13. End of Meeting Debrief (Discussion)
14. Adjournment (Action)

* Indicates agenda items with related documents in meeting packet

Next Regular Meeting – July 10, 2019 at 6:30 PM

To receive email notices of Selectboard meetings and hearings, agendas, minutes and other notices, send an email to manager-assistant@norwich.vt.us requesting to be placed on the Town Email List.

Town of Norwich
P.O. Box 376
Norwich VT 05055-0376
(802) 649-1419 Ext. 101 or 102

APPLICATION FOR BOARDS/COMMISSIONS
(and for those reapplying for continuing appointments)

Name: Demosthenes P Sofronas

Address: 82 Glen Ridge RD

Day phone: 802-829-9133

Evening phone: same

E-mail: demosofronas@gmail.com

Position Applied For: Town Representative to the Advance Transit Board

- 1. If you are re-applying for the same board/commission, how many terms/years have you already served?

Terms: 1

Years: 3

- 2. Would you be available for evening and/or morning meetings?

Evening: (Yes No Morning: (Yes No

Are there other restrictions on your availability? If so, please describe:
none

- 3. Please list any experiences, skills and/or qualifications which you feel would especially suit you for this appointment.

Served on Mutiple boards and my 34 years experience in the US Postal Servic

- 4. Please include service on other municipal or school district Boards, Commissions, or Committees both in Norwich and elsewhere and indicate whether or not any of those appointments are current ones:

Cemetery Commission Current

Board of Civil Authority

Advance Transit Current

5. Education and Current Employment

Name of Company: US Postal Service

Location: Various

Title: Postmaster

Describe your work:

Manage several offices in my 24 years as a Postmaster

6. Pertinent Education and/or Experience:

34 years post office

26 years Military experience

7. Do you feel there could be *any conflict of interest* with your personal beliefs, occupation or employer in serving on this board, commission or committee? (Yes

No). If yes, please explain:

Comments:

Thanks for your consideration and please know that I will always work diligently and well with

Signature



Date

June 14, 2019

Town of Norwich
P.O. Box 376
Norwich VT 05055-0376
(802) 649-1419 Ext. 101 or 102

3

APPLICATION FOR BOARDS/COMMISSIONS
(and for those reapplying for continuing appointments)

Name: Ed Piper

Address: 113 Pattrell Rd

Day phone: 802 299 9195

Evening phone: 649-5241

E-mail: dr.edmund.piper@gmail.com

Position Applied For: Advance Transit Rep for Norwich

1. If you are re-applying for the same board/commission, how many terms/years have you already served?

Terms:

Years:

2. Would you be available for evening and/or morning meetings?

Evening: (Yes No Morning: (Yes No Depending on time & day

Are there other restrictions on your availability? If so, please describe:

3. Please list any experiences, skills and/or qualifications which you feel would especially suit you for this appointment.

I've been an enthusiastic supporter, I've been invested in transportation and grateful for the opportunities and service it has provided over the years for my patients and neighbors

4. Please include service on other municipal or school district Boards, Commissions, or Committees both in Norwich and elsewhere and indicate whether or not any of those appointments are current ones:

Norwich Planning Commission 1979
President North Country Chordsmen 2004-17

5. Education and Current Employment

Name of Company: Edmund Piper Psy.D. LADC

Location: Woodstock VT

Title: *PT practice*

Describe your work:

Social / Psychological intervention on clinical mental health issues & addictions

6. Pertinent Education and/or Experience: *BS, MA, Psy.D*

7. Do you feel there could be *any conflict of interest* with your personal beliefs, occupation or employer in serving on this board, commission or committee? (Yes No) No). If yes, please explain:

Comments: *I have been a big fan and supporter of AT since its inception. I want myself a frog and practical tank and transportation efficiency and costs were crucial issues for my family, patients, and our communities. I am left handed and often can offer novel perspective. I have a 5 PM patient on the 26th and can join you close to 7:00 on Wed*

Signature



Date 6-21-19



INVITATION TO PARTICIPATE IN MUNICIPAL ROADS GRANTS-IN-AID PROGRAM

TO: Selectboard Chair, Town Clerk, Road Foreman, Transportation Advisory Committee, TRORC Commissioners
FROM: Peter G. Gregory, Executive Director *PGG*
DATE: May 30, 2019
RE: Letter of Intent to Participate in the SFY'20 Municipal Roads Grants-in-Aid Program: Funding Assistance in Municipal Road General Permit Compliance

Vermont's Regional Planning Commissions (RPC) and the Department of Environmental Conservation (DEC) invite your municipality to participate in the Municipal Roads Grants-in-Aid Program. This program provides funding for municipalities to implement best management practices (BMPs) in compliance with the DEC Municipal Roads General Permit (MRGP).¹

DEC has set aside \$2.9 million to support BMP implementation under this program. In addition, DEC has allocated \$100,000 for purchase of equipment for communities that have the majority of their land base in the White River, Ottawaquechee-Black and Southern Lake Champlain watersheds. DEC will work with the RPCs to provide grant funds to participating Vermont municipalities that agree to the terms (see attached participation commitment letter). Your municipality must return the signed letter by **July 3, 2019** to be eligible (page 3 of this invitation letter).

DEC will offer funding to participating municipalities to implement BMPs on hydrologically connected road segments that currently do not meet standards.² A municipality's funding award depends upon:

- The number of hydrologically connected roads, sorted into five-mile increments.; and
- The number of participating municipalities. Additional funds may become available should some municipalities decide not to participate. **Refer to the attached DEC Base Offer for your municipality to see the amount of funding you are eligible to receive.**

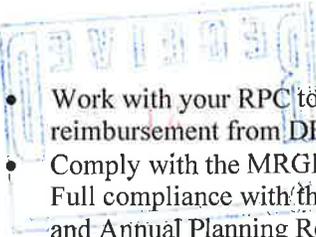
Grants-in-Aid awards will be made available through RPCs, which will submit reimbursement requests to DEC on behalf of municipalities. DEC will reimburse up to 80% of the municipality's documented construction expenses, including in-kind support, for BMPs on hydrologically connected roads.

Participating municipalities agree to:

- Construct the additional BMPs on hydrologically connected roads to bring road segments into full compliance with MRGP standards.
- Complete the project by October 31, 2019, or if more time is necessary, by the final completion date of June 30, 2020.
- Sign the commitment letter to confirm the municipal responsibilities of the program.
- Document project costs and provide a minimum of 20% local match (cash or in-kind) for BMP construction
- Match for BMP construction can include in-kind contributions such as transportation, municipally-owned road equipment, crew labor, municipal staff time and other costs directly related to the BMP construction project. Funds from other federal or state grant programs or local match for those other federal and state grant programs cannot be included as match.
- If eligible, provide 20% local cash match for purchase of equipment.

¹ MRGP available at: <http://dec.vermont.gov/watershed/stormwater/permit-information-applications-fees/municipal-roads-program>.

² Hydrologically connected roads are those that drain directly into surface waters (streams, rivers, ponds, lakes and wetlands) as defined by the MRGP.



- Work with your RPC to select projects, conduct site visits, obtain technical assistance, compile invoices, and seek reimbursement from DEC.
- Comply with the MRGP before requesting reimbursement from the State for construction and equipment costs. Full compliance with the MRGP includes full payment of MRGP fees due by June 1, 2019, required MRGP forms and Annual Planning Report.

DEC agrees to:

- Use a formula, based on the number of hydrologically connected municipal road miles, in determining the monies appropriated to each participating municipality; and
- Make payments on a reimbursement basis, upon receipt of invoices submitted from Regional Planning Commission on behalf of participating municipalities. DEC will reimburse 80% of documented costs for BMP construction and purchase of equipment up to the final award amount for each municipality.

Regional Planning Commissions agree to:

- Assist municipalities by: (a) serving as the municipalities' point-of-contact and fiscal agent for reimbursement of expenses under the program; (b) providing maps of connected road segments to participating municipalities; (c) providing technical assistance to towns in project selection, mapping and construction techniques; (d) posting clean water project signs for publicly visible projects during construction when feasible, and (e) aiding municipalities in tracking and reporting deliverables.
- Securing letters of intent to participate in the program coordinated statewide through the Northwest Regional Planning Commission in St. Albans.

Eligible BMPs according to the MRGP Standards:

- Grass and stone-lined drainage ditches and stone check-dams;
- Turnouts, cross culverts, and other disconnection and infiltration practices;
- Lowering of high road shoulders;
- Installation or replacement of drainage culverts and driveway culverts on non-perennial streams within right of way and installation of culvert headwalls and outlet stabilization;
- Stabilizing conveyance zones;
- Addressing gully erosion on Class 4 roads; and
- Stabilizing catch basin outlets.

If you have any questions about the Municipal Grants-in-Aid Project, please contact **Rita Seto**, rseto@trorc.org / 802-457-3188. For questions about the Municipal Roads General Permit please refer to the DEC website: <http://dec.vermont.gov/watershed/stormwater/permit-information-applications-fees/municipal-roads-program>.

We look forward to hearing from you soon.

Sincerely,

Peter G. Gregory, AICP
Executive Director

**LETTER OF INTENT TO PARTICIPATE IN THE
MUNICIPAL ROADS GRANTS-IN-AID PROGRAM**

(page 3 of 3)

We, the Legislative Body of the Municipality of _____ certify that the municipality will:

- Construct one or more road best management practices (BMPs) to bring connected road segments into full compliance with Municipal Roads General Permit (MRGP) standards, to be completed, if possible, by October 31, 2019, or by the final completion date of June 30, 2020.
- Construct the road BMPs on hydrologically connected road segments – roads that drain directly into surface waters (streams, rivers, ponds, lakes and wetlands). Contact your regional planning commission or refer to the Vermont Department of Environmental Conservation (DEC) map layer for *hydrologically-connected* municipal roads in Vermont. This map layer is available at: <http://anr.vermont.gov/maps/nr-atlas>.
- Consult with the Regional Planning Commission prior to construction of the BMPs to conduct a required site visit and verify the appropriate location of the connected road segment and BMP(s) to meet MRGP standards.
- Post a clean water project sign during construction (select projects only).
- Provide a minimum of 20% local match (in-kind and cash). Match can include quantified in-kind contributions such as transportation, municipally-owned road equipment, crew labor, municipal staff time and other costs directly related to the BMP construction project as part of this program. Funds from other federal or state grant programs or local match for those other federal and state grant programs cannot be included as match.
- Work with the Regional Planning Commission to complete all reporting and invoicing requirements.

Comply with the MRGP before requesting reimbursement from the State for construction and equipment costs. Full compliance with the MRGP includes full payment of MRGP fees due by June 1, 2019, required MRGP forms and Annual Planning Report.

Please indicate by checking one of the boxes below the anticipated project completion date (for planning purposes only):

- October 31, 2019
- June 30, 2020

Date: _____

(Duly Authorized Representatives)

This signed letter is due by July 3, 2019

Return signed commitment letter to: Municipal Roads Grants-in-Aid Program, c/o Northwest Regional Planning Commission, 75 Fairfield Street, St. Albans, Vermont 05478 or email to GIA@nrpcvt.com.



**MUNICIPAL ROADS GRANTS-IN-AID PROGRAM
EQUIPMENT PURCHASE**

TO: Vermont Municipal Officials in the White River, Ottawaquechee-Black River and Southern Lake Champlain Watersheds
FROM: Peter G. Gregory, Executive Director *PGG*
DATE: May 30, 2019
RE: Letter of Intent to Participate in the SFY'20 Municipal Roads Grants-in-Aid Program: Equipment Purchase

Vermont Department of Environmental Conservation (DEC) offers select municipalities funding to purchase up to one piece of equipment that will support Municipal Roads General Permit (MRGP) implementation. Your municipality is eligible for equipment funding, as the majority of your municipality is located within the SFY 2020 targeted watersheds: White River, Ottawaquechee-Black River, and Southern Lake Champlain. Equipment eligible for purchase is described below. Municipalities must enroll in the SFY 2020 Grants-in-Aid program and construct best management practices on hydrologically connected municipal road segments in order to be eligible for equipment purchase grants.

DEC will reimburse up to 80% of the municipality's documented equipment cost, up to the State maximum award amount in the table below. If demand exceeds available funding, then equipment purchase will be prioritized for municipalities with the greatest number of hydrologically connected municipal road miles.

Please fill out, sign, and return the Letter of Intent to Participate in the Equipment Purchase program on the following page.

Equipment eligible for purchase:

Equipment type	Total maximum amount	State maximum award (80% of equipment cost)	Cash match required (20% of equipment cost)
Roller compactors for attachment to grader to improve road crown	\$7,500	\$6,000	\$1,500
Leaf blowers to clean leaves and sediment from ditches and reduce culvert plugging	\$5,500	\$4,400	\$1,100
Hydroseeders to accelerate revegetation of disturbed soil areas	\$7,500	\$6,000	\$1,500
Plate or jumping-jack compactors to install drainage culverts	\$4,000	\$3,200	\$800
Tractor-mounted shoulder discs to remove high road shoulders	\$8,000	\$6,400	\$1,600
Haybale shredder to distribute hay mulch on disturbed soil areas	\$7,600	\$6,080	\$1,520

If you have any questions about the Municipal Roads Grants-in-Aid Program equipment purchase, please contact **Rita Seto**, rseto@trorc.org / 802-457-3188. For questions about the Municipal Roads General Permit please refer to the DEC website:

<http://dec.vermont.gov/watershed/stormwater/permit-information-applications-fees/municipal-roads-program>.

Thank you for your attention, and we look forward to hearing from you soon.

Sincerely,

 Peter G. Gregory, AICP
 Executive Director

**LETTER OF INTENT TO PARTICIPATE IN THE
RPC/DEC MUNICIPAL ROADS GRANTS-IN-AID PROGRAM
EQUIPMENT PURCHASE**

We, the Legislative Body of the Municipality of _____ certify that
the municipality will:

- Upon a notice to proceed, purchase up to one piece of equipment, indicated below, to support MRGP implementation and provide 20% cash match for equipment purchase by the final completion date of June 30, 2020. Funds from other federal or state grant programs or local match for those other federal and state grant programs cannot be included as match.
- Indicate piece of equipment of interest for purchase (check one):
 - Roller compactors for attachment to grader to improve road crown (State maximum award amount \$6,000, cash match required \$1500 or 20% of equipment cost)
 - Leaf blowers to clean leaves and sediment from ditches and reduce culvert plugging (State maximum award amount \$4,400, cash match required \$1100 or 20% of equipment cost)
 - Hydroseeders to accelerate revegetation of disturbed soil areas (State maximum award amount \$6,000, cash match required \$1500 or 20% of equipment cost)
 - Plate or jumping-jack compactors to install drainage culverts (State maximum award amount \$3,200, cash match required \$800 or 20% of equipment cost)
 - Tractor-mounted shoulder discs to remove high road shoulders (State maximum award amount \$6,400, cash match required \$1600 or 20% of equipment cost)
 - Haybale shredder to distribute hay mulch on disturbed soil areas (State maximum award amount \$6,080, cash match required \$1520 or 20% of equipment cost)
- Purchase the equipment following relevant municipal procurement practices or policies.
- Submit a signed operation and maintenance agreement to operate and maintain equipment according to manufacturer's specifications for the equipment's design life.
- Work with your local Regional Planning Commission to complete the equipment reporting requirements including the final performance report (refer to page 3 of this letter), invoices/receipts, photos of equipment in use, a copy of the maintenance manual and transfer of ownership letter (template to be provided by the state).

Date: _____

(Duly Authorized Representatives)

This signed letter is due by July 3, 2019

Return signed commitment letter to: Municipal Roads Grants-in-Aid Program, c/o Northwest Regional Planning Commission, 75 Fairfield Street, St. Albans, Vermont 05478 or email to GIA@nrpcvt.com.

**MUNICIPAL ROADS GRANTS-IN-AID PROGRAM
FINAL PERFORMANCE REPORT
EQUIPMENT PURCHASE**

Project Information

Municipality:

Report date:

Grants-in-Aid funding amount:

Total match documented (20% of total project(s) cost):

*For your reference. Do not
return with Letter of Intent.*

Indicate Equipment Type:	
Performance Measure Type	Performance Measure Value
Estimated hours equipment in use per year	
Estimated road miles improved through use of equipment per year (applies to roller compactors, leaf blowers, and tractor-mounted shoulder discs)	
Estimated number of culverts installed through use of equipment per year (applies to plate and jumping-jack compactors)	
Estimated acres stabilized through use of equipment per year (applies to hydroseeders and haybale shredder)	

Please include the following attachments in your submission for reimbursement with this form:

- 1. A copy of the invoice or receipt for equipment purchase**
- 2. A copy of the signed operation and maintenance agreement to operate and maintain equipment according to manufacturer's specifications for the equipment's design life**
- 3. A copy of the equipment maintenance manual**
- 4. Photos of the equipment purchased in use (submit as individual JPG files -minimum resolution 300 dpi)**
- 5. A written request to retain ownership of the equipment at the end of grant term for the same use and intended purpose as outlined in this agreement (template to be provided by the state)**

Submit all required documentation to: **Rita Seto, rseto@trorc.org / Two Rivers-Ottawaquechee Regional Commission, 128 King Farm Road, Woodstock, VT 05091.**

**APPENDIX: Municipal Roads Grants-in-Aid Pilot Project
Hydrologically Connected Roads by Vermont Municipality
(Excerpt from VDEC Data Map Layer: <http://anr.vermont.gov/maps/nr-atlas>)**

	Town	State Estimate: Connected Road-Mile by Range	DEC Grant Offer	20% Local Match (Cash or In-Kind)	Total	Eligible for Equipment Purchase
1	BARNARD	40-45 Connected Road-miles	\$19,600	\$4,900	\$24,500	Yes
2	BETHEL	45-50 Connected Road-miles	\$21,900	\$5,475	\$27,375	Yes
3	BRADFORD	25-30 Connected Road-miles	\$12,700	\$3,175	\$15,875	No
4	BRAINTREE	30-35 Connected Road-miles	\$15,000	\$3,750	\$18,750	Yes
5	BRIDGEWATER	30-35 Connected Road-miles	\$15,000	\$3,750	\$18,750	Yes
6	BROOKFIELD	25-30 Connected Road-miles	\$12,700	\$3,175	\$15,875	Yes
7	CHELSEA	35-40 Connected Road-miles	\$17,300	\$4,325	\$21,625	Yes
8	CORINTH	45-50 Connected Road-miles	\$21,900	\$5,475	\$27,375	No
9	FAIRLEE	10-15 Connected Road-miles	\$5,700	\$1,425	\$7,125	No
10	GRANVILLE	15-20 Connected Road-miles	\$8,100	\$2,025	\$10,125	Yes
11	HANCOCK	5-10 Connected Road-miles	\$3,400	\$850	\$4,250	Yes
12	HARTFORD	60-65 Connected Road-miles	\$28,900	\$7,225	\$36,125	Yes
13	HARTLAND	30-35 Connected Road-miles	\$15,000	\$3,750	\$18,750	Yes
14	NEWBURY	45-50 Connected Road-miles	\$21,900	\$5,475	\$27,375	No
15	NORWICH	45-50 Connected Road-miles	\$19,600	\$4,900	\$24,500	No
16	PITTSFIELD	10-15 Connected Road-miles	\$5,700	\$1,425	\$7,125	Yes
17	PLYMOUTH	30-35 Connected Road-miles	\$15,000	\$3,750	\$18,750	Yes
18	POMFRET	35-40 Connected Road-miles	\$17,300	\$4,325	\$21,625	Yes
19	RANDOLPH	35-40 Connected Road-miles	\$17,300	\$4,325	\$21,625	Yes
20	ROCHESTER	25-30 Connected Road-miles	\$12,700	\$3,175	\$15,875	Yes
21	ROYALTON	40-45 Connected Road-miles	\$19,600	\$4,900	\$24,500	Yes
22	SHARON	30-35 Connected Road-miles	\$15,000	\$3,750	\$18,750	Yes
23	STOCKBRIDGE	25-30 Connected Road-miles	\$12,700	\$3,175	\$15,875	Yes
24	STRAFFORD	40-45 Connected Road-miles	\$19,600	\$4,900	\$24,500	No
25	THETFORD	25-30 Connected Road-miles	\$12,700	\$3,175	\$15,875	No
26	TOPSHAM	40-45 Connected Road-miles	\$19,600	\$4,900	\$24,500	No
27	TUNBRIDGE	40-45 Connected Road-miles	\$19,600	\$4,900	\$24,500	Yes
28	VERSHIRE	30-35 Connected Road-miles	\$15,000	\$3,750	\$18,750	No
29	WELLS RIVER VILLAGE	0-3 Connected Road-miles	\$2,000	\$500	\$2,500	No
30	WEST FAIRLEE	15-20 Connected Road-miles	\$8,100	\$2,025	\$10,125	No
31	WOODSTOCK VILLAGE	5-10 Connected Road-miles	\$3,400	\$850	\$4,250	Yes
32	WOODSTOCK	55-60 Connected Road-miles	\$21,900	\$5,475	\$27,375	Yes
			\$475,900	\$118,975	\$594,875	

TOWN OF NORWICH

P.O. Box 376

Norwich, VT 05055

5

PURCHASE ORDER

DATE: 6/20/19 PO #: _____

VENDOR: Blaktop, Inc.

ITEMS OR SERVICES PURCHASED: Paving section of Union Village

CHARGE ACCOUNT: LESS VTRAKS CLASS 2 PAVING GRANT PAVING DESIGNATED FUNDS

Department Head: Town Manager: [Signature]

AMOUNT: \$211,420 (166,380) TOTAL: \$45,040

Date: 6/20/19

Finance Department Use Only

Herb Durfee

From: Larry Wiggins
Sent: Monday, June 17, 2019 9:05 AM
To: Herb Durfee
Cc: ej@jhutchinsinc.com; Bump, Chris; Donna Flies; Miranda Bergmeier; Roberta Robinson; Stuart@blaktop.com
Subject: 2019 Paving Project Bid recommendation

The Public Works Department received bids for the Town of Norwich 2019 Paving Project at the Town Manager's office on June 14, 2019. Two bids were received. I have reviewed the bids and recommend awarding the contract to Blaktop, Inc. in the amount of \$211,420.00. This is a unit cost contract so this is not a final cost number. The paving will need to be monitored to ensure budgeted funds are not exceeded. Currently the designated paving funds equal \$219,900.67. The bid tabulation is as follows:

		J. Hutchins, Inc.	Blaktop, Inc.
Shim course	1350 tons	\$116,100	\$ 98,820
Wear course	1350 tons	\$116,100	\$ 98,820
Hand method	5 Tons	\$ 1050	\$ 900
Flaggers	220 hrs	\$ 7920	\$ 9680
Performance/Payment bonds	1 LS	<u>\$ 5000</u>	<u>\$ 3200</u>
	Totals	\$246,170	\$211,420

With VTrans and your approval I will contact Blaktop, Inc. and initiate the Agreement, insurance and bonds process.

6a1

NORWICH SELECTBOARD
Committee/Commission Reports

Committee: Norwich Energy Committee, 1st Quarter 2019

Members: Rob Gere, Linda Gray (chair), Susan Hardy, Aaron Lamperti, Suzanne Leiter, Norm Levy (vice-chair), Charlie Lindner.

Committee/Commission/Appointee Charge (including subcommittees and workgroups):

The Norwich Energy Committee (the "NEC") shall make recommendations to the Selectboard regarding Town policy on energy-related matters. The NEC works with residents, businesses and the Town to promote energy education and awareness, reduce energy consumption, improve the efficiency of energy used, promote renewable energy generation, and reduce greenhouse gas emissions on both an absolute and per capita basis. (Approved by the Selectboard 6-27-18)

Current Projects (including work by subcommittees and workgroups) with proposed deadlines):

Transportation: The NEC collaborated with Vital Communities on a campaign to reduce barriers to using different modes of transportation, with weekly posts on the listserv and weather-proof posters around town that advertised travel training and showcased Norwich residents who ride the bus). Transportation links were added to the NEC web site and an info table was set up at Town Meeting with transportation materials. An NEC member is attending the monthly meetings of the Upper Valley Transportation Management Association.

Solarize: the NEC promoted a community solar project to residential users, for the 150kW solar project permitted for the Becky Cook parcel (along Union Village Road). The deadline to identify a minimum number of residential users was mid-April. Contacts included email to solar prospects from earlier years, posts to the list serv, and several Q/A presentations with Norwich Solar Technologies, the project developer.

Weatherize: The NEC continued working with Efficiency Vermont (EVT) on the "Button Up Hero" program (modeled on the 2017 program with Vital Communities). 52 households signed up to have a home energy walk-through with a contractor and then receive a free professional recommendation on the best ways to save. (The usual energy audit costs ~\$400 and this cost is an obstacle for homeowners.) Homeowners have until 4/22 to decide whether to pursue the recommendations.

Tracy Hall energy improvements: NEC members and Town staff met with a representative of an energy performance contractor, which can be hired to do an audit of all the Town buildings and make recommendations for energy improvements (such as integrated building controls, changes to heating/cooling, insulation, lighting, solar energy). The majority of such projects have no tax impact, because the cash outlay equals the energy savings. Developing a Request for Qualifications will be the first step in pursuing this.

EV Charging Stations: The state grant was awarded to the Town, providing 90% funding for an EV charging station at the park-and-ride lot at Huntley Meadow.

Enhanced Energy Planning Workgroup: Group members worked with Planning Director Rod Francis on a revised Energy Chapter of the Town Plan, with the goal of turning the draft over to the Planning Commission in the spring.

Future Projects:

Transportation: the NEC is working on additional bike racks in town, bike fix-it stations, and a loaner program for electric-assist bikes.

Solarize: 8th campaign planned for April-May, to enable residents to go solar before a decreases in 1) the net-meter adder as of 7/1/19 and 2) the federal tax credit as of 12/31/19.

Weatherize: Efficiency VT will do outreach to rental unit owners, offering consultations on efficiency improvements to their buildings.

Tracy Hall energy improvements: work with the Town Manager on developing a Request for Qualifications for an energy performance contractor.

EV Charging Station: work with the Town Manager on the details of installation, including an RFP for an electrical contractor and purchase of the charging equipment.

Support Needed from the Selectboard:

Continued annual budget for NEC (\$1500)

Continued budget support for Tracy Hall reserve fund

From: Ann Marie Smith <birdseye95@comcast.net>
Sent: Friday, June 07, 2019 11:14 AM
To: Miranda Bergmeier
Subject: Blinking cross walks

Let us not be tricked into believing there is no expense for the blinking crosswalk signs. They are not Free, there are costs. They will cost us in character, of our village and of ourselves. Nothing is free..... and even if you think it is, it doesn't mean you have to take it.

We are not Hanover, a college town with thousands of students among their local population, who routinely walk across the streets without looking or any concern for the cars on the road, on 'their' campus. The blinking crosswalks there were intentionally to address that issue. We do not have that issue. Pride of courtesy, of looking out for one another, is something that we do have,,,,, or did. It is ridiculous to me that something like this seems necessary in a small village, in a town of slightly more than 3000 people.

I wish the Select Board would stop and take a breath, review this decision and its true necessity and its consequences. There is no true need, and just because we can have it..... doesn't mean should or that we need it.

Ann Marie Smith

6a2

Herb Durfee

From: John Trautlein <john@trautlein.com>
Sent: Friday, June 07, 2019 2:28 PM
To: David Ormiston
Subject: Flashing Crosswalks

Hi,
Please add me to your list of residents who don't believe these are good for our town.
I have noticed that in Hanover that some folks step out into the street without even taking their eyes off their phone.
I think it actually gives a FALSE sense of safety.. Not to mention the drivers who disregard them, either purposely or not.
John Trautlein

602

From: din seaver <dinseaver@gmail.com>
Sent: Tuesday, June 11, 2019 8:46 AM
To: Miranda Bergmeier
Subject: NO to Crosswalk Warning Lights

Dear selectboard,

i completely agree with rob's sentiments asposted in the listserv (see below) and would like to add that i had no idea that these lights were under consideration a year ago. granted, i don't often go to selectboard meetings but this seems like something that could have gotten more public review/input given that it does have a fairly visible impact.

din seaver

X-original-from: ROB NAHABEDIAN <musicwleb@aol.com>
From: "ROB NAHABEDIAN via norwich list" <norwich@lists.vitalcommunities.org>
To: norwich@lists.vitalcommunities.org
Subject: [Norwich] NO to Crosswalk Warning Lights
Date: Tue, 11 Jun 2019 11:56:57 +0000 (UTC)

I wholeheartedly agree with those voicing their opposition to adding crosswalk warning lights to multiple locations along main street.

I see in todays Valley News that select board member John Langhus said "The point to disagree with that" (installing the lights) was two years ago".

(when the grant was submitted) He goes on to say, "I certainly have no interest in revisiting that".

I believe that when enough townspeople speak up about an issue, those serving those townspeople on the select board should listen, and definitely revisit the issue.

If you feel the same speak up by contacting those on the board.

Rob Nahabedian
Norwich



Virus-free. www.avg.com

6a2

From: Harry Roberts <harry@trailgroom.com>
Sent: Tuesday, June 11, 2019 8:12 PM
To: norwich@lists.vitalcommunities.org
Cc: Miranda Bergmeier
Subject: crosswalk safety

Until I went back and viewed the 6/13/18 tape of the Selectboard meeting on CATV, I did not realize that the Town was planning to install four separate sets of flashing yellow beacons for our cross walks, and now the Valley News article on Tuesday confirms that.

So there are four sets of these lights planned for: 1) Tracy Hall, 2) Elm Street across Main ST., 3) Church Street from the Congregational Church, and 4) at the Norwich Inn.

In my opinion, we have a solution looking for a problem, e.g. lights paid for partially by a grant and we need some place to put them.

Safety First....so I suggest a good first measure for crosswalk safety would be to re-paint the existing crosswalks onto the pavement, they're currently worn off and not visible.

Harry Roberts

Miranda Bergmeier

6a2

From: maximilian wallenberg <mount1batten@yahoo.com>
Sent: Wednesday, June 12, 2019 6:03 AM
To: Miranda Bergmeier
Subject: Crosswalk lights

Count me as another strong "NO" on crosswalk warning lights.
Peter DeLong
102 maple hill RD

From: Vickie Seaver <pugpp104@icloud.com>
Sent: Wednesday, June 12, 2019 6:48 AM
To: Miranda Bergmeier
Subject: Crosswalk lights.

I would like to say I don't want to see crosswalk lights in Norwich. We are not a big city and should NOT need to have bright big flashing lights in the historic part of the town. You regulate what paint folks can use for their houses, WHY would you degrade the Main Street with bright flashing lights that could interrupt sleeping people in their houses and would just not add any true protection or safety to our towns people. I have never had an issue with people not stopping when I want to cross any street in Norwich. sincerely, Vickie Seaver.

Sent from my iPad

ba2

From: Sheilah Rahfield <srahfield@gmail.com>
Sent: Wednesday, June 12, 2019 7:06 AM
To: Miranda Bergmeier
Subject: Flashing lights

Select board,

I am opposed to installing flashing lights at the crosswalks. The character of our town is based on respect and caring for each other. We show each other this respect daily by stopping at the crosswalk for our neighbors. The existing system creates an opportunity to make old fashioned eye contact, smile and wave. These small gestures strengthen our community. Crossing the street safely without the aid of flashing lights is a basic life skill we should be teaching our children. Personally, my daughter loves to wave and smile as she crosses. The crossing guards do a great job of watching over our children at MCS. Let's not dumb ourselves down in the name of safety.

With respect,

Sheilah Rahfield
156 Mitchell Brook Road
Norwich

6a2

From: Folger Tuggle <folger.tuggle@gmail.com>
Sent: Wednesday, June 12, 2019 7:25 AM
To: Miranda Bergmeier
Subject: NO to crosswalk warning lights

Dear Selectboard members,

Please refrain from installing the crosswalk warning lights in Norwich. Some have offered how the "process" has brought us here, Others have provided feedback on other options; ie fresh coat of paint on the existing crosswalks.

Some actions of the Selectboard are difficult to keep track of in a "timely" manner. For that, we apologize.

These walker activated crosswalk lights are unsightly and unneeded.

Despite the comments of some I would argue that over time there have been a series of items that fit this unnecessary and unneeded category which taken collectively all impact the visual landscape of a small village--each taking its collective toll.

Please reverse your decision and opt not to install these cross walker activated lighting structures.

Thank you.

Folger & Liz Tuggle
Waterman Hill Road

From: Rob Nahabedian <musicwleb@aol.com>
Sent: Wednesday, June 12, 2019 7:49 AM
To: Miranda Bergmeier
Subject: NO to crosswalk warning signs

This was posted on the Norwich list serve and I wanted to make sure the select board ,most importantly, read what I had to say.

I wholeheartedly agree with those voicing their opposition to adding crosswalk warning lights to multiple locations along main street. I see in todays Valley News that select board member John Langhus said "The point to disagree with that" (installing the lights) was two years ago". (when the grant was submitted) He goes on to say, "I certainly have no interest in revisiting that".I believe that when enough townspeople speak up about an issue, those serving those townspeople on the select board should listen, and definitely revisit the issue. If you feel the same speak up by contacting those on the board.

Rob Nahabedian
96 Hawk Pine Road
Norwich

Rob

Excuse typos, sent from my iPhone

Subject: FW: crossing lights

From: gina sonne [mailto:waltzlover@gmail.com]
Sent: Wednesday, June 12, 2019 7:54 AM
To: Miranda Bergmeier
Subject: crossing lights

Selectboard,

It sounds like there is quite a lot of opposition to the proposed crossing lights. I, for one, am opposed. It would seem to be the most community minded approach to take to review this decision with our town. This installation will drastically affect the feel and aesthetic of the town. I urge you to open this up to the town once again. Mostly importantly, I feel the board's responsibility is to foster an open, careful conversation with the community about their concerns. We live in a time when it is a challenge to keep up with all the news and concerns of our local community. It seems to me that to be willing to wait(even tho' this was passed) on this issue and listen to everyone's concerns, would be the wisest next step.

Sincerely,

Gina Sonne

24 Dorrance Dr

if you need a faster response please send a text
802 649-7062 cell primary

Google Hangout phone number 802 765-0478

From: Rick Greenwald <rgreenwald@simbex.com>
Sent: Wednesday, June 12, 2019 7:55 AM
To: Miranda Bergmeier
Cc: 'Ann Greenwald'
Subject: Crosswalk Warning Lights

Dear Norwich Selectboard,

I'd like to add our names to the growing list of those who would ask you to re-visit and re-consider the implementation of the crosswalk warning lights. While it is correct that this would have been better argued two years ago when funding was applied for, we were also not aware this topic was being discussed publicly. Together with the many added signs on Turnpike Road in recent years, it seems like a bit more than might be needed for Norwich. If appropriate, we would appreciate the Selectboard get a feel for what the majority of the town wants in this regard before moving ahead with installation of these lights.

Regards

Rick and Ann

Rick Greenwald
Ann Greenwald
63 Douglas Hill

Simbex
10 Water Street Suite 410
Lebanon, New Hampshire 03766
603-448-2367
www.simbex.com

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From: Robert Lewy <rl10@caa.columbia.edu>
Sent: Wednesday, June 12, 2019 8:10 AM
To: Miranda Bergmeier
Subject: Crosswalk

Dear Selectboard:

As a long time resident, off and on, since 1971, I am writing to express my opposition to installing the flashing lights on crosswalks throughout town. I am all for public safety, but do not feel as though these obnoxious flashing lights will contribute to a safer environment. They certainly will contribute to a more urban and congested atmosphere around town.

I oppose.

Thank you for listening.

Robert Lewy

176 Upper Turnpike Road

From: Sandra Dell <sandradellrealestate@gmail.com>
Sent: Wednesday, June 12, 2019 8:20 AM
To: Miranda Bergmeier
Subject: Crosswalk lights

Thank you for all you do for our town. However, sometimes doing more isn't a good thing. I'm referring to placing crosswalk lights on Main St. They will be an eyesore we don't need. Motorists do stop to let people cross at the crosswalks. They often stop even when people cross other places. And spending our tax dollars on these signs is wasteful.

Respectfully,

Sandra Dell

--

Sandra Dell
Realtor NH and VT



802-356-7786 - Cell
603-653-8490 - Office
3 Lebanon St., Hanover, NH 03755
www.Sandradell.masiello.com

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From: Kathleen Foltz <knfoltz@gmail.com>
Sent: Wednesday, June 12, 2019 8:31 AM
To: Miranda Bergmeier
Subject: No Crossing Lights

Dear Selectboard Members,

Firstly, thanks for the tremendous job you do for our town. I really appreciate how much you care and think about our wellbeing.

But, (you knew there was a "but" coming, right?) I am also a naysayer to the lights. I have lived here for 39 years, brought up my three children here and watched with dismay as the amount of signage on Main Street seemed to grow and grow. I appreciate the crosswalks but I really think the lights are one step too many.

I am sorry that I did not let you know sooner but I wasn't aware of the issue which is my own fault.

In any case, I just wanted you to know how I felt.

Thanks for listening-
Kathleen Foltz

6a2

From: Deirdre <deirdre.goodrich@comcast.net>
Sent: Wednesday, June 12, 2019 8:40 AM
To: Miranda Bergmeier
Subject: Opposed to new crosswalk lights

Dear selectboard members,

Thank you for your service to the town.

I would like to voice my opposition to the crosswalk lights. I have lived in Norwich for 27 years, and I'm married to a Norwich native whose roots go back to the town's first residents. All three of our children were born here and attended Marion Cross School. Most days they took the school bus, even when in kindergarten. The school bus is the safest form of transportation to school, but seems to be an unpopular choice.

As a family, we do a significant amount of walking in town and we have never experienced safety issues with crosswalks.

Anecdotally, just this morning I followed a car down Main St and noticed the car dropped off a child at the gazebo crosswalk. The car could just as easily have dropped the child at the manned Tracy Hall crosswalk, more safe for the child but less convenient for the parent. Seems that convenience for the parent trumped the child's safety in that decision.

I am happy to participate in a task force to educate residents on pedestrian safety.

Respectfully,

Deirdre Goodrich
474 Main St.

From: Amy Stringer <vermont6633@gmail.com>
Sent: Wednesday, June 12, 2019 8:46 AM
To: Miranda Bergmeier
Subject: Flashing walk lights

To the Selectboard-

We, like many others, first learned of the proposed flashing walk lights on the listserv over the past few weeks. Like everyone else who has posted, we were disappointed at this prospect. It seems entirely unnecessary in a town of this size to have such eyesores. Have we ever had even one injury in a crosswalk?

We have questions:

1. Why is it too late to question this proposition? (So disappointing to see the Langhus quote in the Valley News. The comment felt dismissive.)
2. What is the town's out-of-pocket cost for these lights?
3. What happens to the job of crossing guards (surely the best protection for our children)?
4. How much are the crossing guards paid?
5. Wouldn't keeping the crosswalk visibly painted with a cone in the middle have a strong positive effect concerning safety?

We wrote to the town manager last week, but have not yet received a response. We hope to hear from you with answers to our questions.

We suggest a police crackdown on drivers using hand-held devices would offer the greatest protection to motorists and pedestrians alike.

We would also suggest you adopt a better way of communicating with townspeople about changes to the status quo so we can all be better informed. Perhaps a posting on the listserv with updates on such topics and, for individuals who have expressed an interest in a particular topic, emails sent to individual addresses would be helpful.

Thank you-

Amy Stringer
Jaan Laaspere
Norwich

(602)

Herb Durfee

From: Omer Trajman <otrajman@gmail.com>
Sent: Wednesday, June 12, 2019 8:57 AM
To: John S Pepper; Herb Durfee
Cc: Lily Trajman
Subject: Crosswalk lights

Unfortunately I can't make the SB meeting tonight due to travel. In case there are more conversations about the crosswalk lights, I'd hoped to share a few points regarding the added need for safe pedestrian crossing on Main St.

A few things have changed in the village that we don't notice day to day. In the past 30 years, the average daily traffic has doubled, and with more cars competing for the same amount of road, it's more dangerous to cross the street. In the past 10 years, drivers have become more distracted, both because of the increase of traffic and because of cell phones. In the next 10 years, we'll also see the dangers of silent electric cars. The lighted signs are a responsible and gentle reminder for everyone to slow down and pay attention.

-Omer

From: Susan Jorgensen <jorgensen.susan@gmail.com>
Sent: Wednesday, June 12, 2019 9:24 AM
To: Miranda Bergmeier
Subject: Crosswalk lights

Good morning,
Please add me to the list of Norwich residents opposed to the plan to install crosswalk lights. It's overkill .. so to speak.. massive overkill.

If you're looking for something to spend money on, how about some NO PARKING signs on the upper end of Turnpike Road, from the Fire Tower Trail parking area to the end of the Class 3 part of the road? That's something that's *needed*.

Thank you,
Susan Jorgensen
1737 Turnpike Rd.
Norwich

6a2

From: andy robinson <andrewrobinson0656@gmail.com>
Sent: Wednesday, June 12, 2019 9:48 AM
To: Miranda Bergmeier
Subject: crosswalk

I am writing to voice strong opposition to the proposed crosswalk modification. This seems to be a giant overreach and a "solution" to a problem that doesn't exist. We think that a much simpler way to go would be to purchase 2 "lollipop" stop signs (like the crossing guards use) for each crosswalk and have them available at either end of the crosswalk. The pedestrian could hold a sign, if he or she wishes, while crossing and deposit the sign on the other side. Simple, low cost and effective.

Respectfully,

Kate and Andy Robinson

Miranda Bergmeier

6a2

From: Ruth Dixon-Vestal <hummingbird642@yahoo.com>
Sent: Wednesday, June 12, 2019 10:27 AM
To: Miranda Bergmeier
Subject: Crosswalk lights

I strongly oppose blinking crosswalk lights anywhere within the town of Norwich! Please withdraw this as an option . Perhaps the money could go toward something else. It is not too late Mr. Langus.
Thank you. Ruth

Sent from Yahoo Mail for iPad

6a2

From: Charlotte Goodrich <charlottegoodrich@gmail.com>
Sent: Wednesday, June 12, 2019 10:36 AM
To: Miranda Bergmeier
Subject: Blinking Crosswalk Lights

Good morning,

I would like to respectfully ask the select board to reconsider installing four sets of new blinking crosswalk lights in downtown Norwich. They would disrupt the peace, pleasantry, and aesthetics of Main Street. I lived in Norwich for my entire childhood. During my school years, I either took the bus or walked, and I never had any safety issues. As a driver, I have always respected painted crosswalks, and I've seen other drivers do the same.

The safety grant money could be used on a more beneficial alternative such as adding sidewalks on Beaver Meadow Road or repainting the painted crosswalks, which have faded.

Thanks for all you do for Norwich and thank you in advance for your consideration of this request. Please feel free to reach out to me with any questions.

Best regards,

Charlotte Goodrich
(603) 306-2868

6a2

Miranda Bergmeier

From: Donald Herzberg <don.herzberg@outlook.com>
Sent: Wednesday, June 12, 2019 10:42 AM
To: Miranda Bergmeier
Subject: crosswalk light

Add my NO to the list of people NOT wanting the lit sign.

Don Herzberg

From: Elaine <e2kennedy@gmail.com>
Sent: Wednesday, June 12, 2019 11:44 AM
To: Miranda Bergmeier
Subject: NO to Crosswalk Warning Lights

PLEASE STOP...our little town does not need these and they would ruin what we have not to mention more tax money for what is NOT need.

Not everyone can attend the Select board meetings due to many reasons, it would be wonderful to have a way to inform the town of ideas of changes before decisions are made.

Thank you.
Elaine Kennedy

From: Jon Voelkel <voelkel@jaguarstones.net>
Sent: Wednesday, June 12, 2019 12:24 PM
To: Miranda Bergmeier
Subject: Flashing crosswalk lights

Dear Selectboard,

I would like to add my voice in the opposition to the proposed flashing crosswalk lights. Having seen the ones in Hanover - which are clearly an unnecessary eyesore. The last thing the town needs is to add to the almost continuous parade of signs and flashing lights already on Main Street. Please desist from the madness.

Thank you,

Jon Voelkel
351 Main Street.

Herb Durfee

6a2

From: johnlanghus@gmail.com
Sent: Wednesday, June 12, 2019 12:50 PM
To: Mary Layton; rogerarnold@gmail.com; cbrochu30@gmail.com; John Pepper
Cc: Herb Durfee; Miranda Bergmeier
Subject: Fwd: crosswalk lights

Fyi. Was asked to pass this along.

Sent from my iPhone

Begin forwarded message:

From: John Olszewski <john@53tech.com>
Date: June 12, 2019 at 12:11:05 PM EDT
To: John Langhus <johnlanghus@gmail.com>
Subject: Re: crosswalk lights

Hi John, Thanks for your quick response ;-). Yeah, please share with the rest of the board and thanks. I know this can be a pain.

John

On Wed, Jun 12, 2019 at 12:02 PM John Langhus <johnlanghus@gmail.com> wrote:
Thanks John. I do think the horse has sort of left the barn on this one, but we will obviously be discussing this tonight given the recent listserv traffic. Would you like me to share this with the Board at the meeting, though?

On Wed, Jun 12, 2019 at 11:59 AM John Olszewski <john@53tech.com> wrote:
Hi John,

Hope all is well. I just wanted to add my voice to those against the crosswalk lights. Granted I sometimes don't pay enough attention to what is going on at Selectboard meetings and if you feel that 2 years ago this decision received adequate public exposure/input, then I missed my chance to speak up and should not complain now. That said, if you feel there's a chance that the public didn't understand the impact back then, then I'd appreciate you and the board reconsidering your decision. I feel in certain contexts these lights can be very helpful to pedestrians, but in my opinion they are complete overkill for Norwich Village.

Thanks for your consideration,

John Olszewski
463 Main St

--

John Langhus
(802) 369-4415 (cell)

602

Herb Durfee

From: Herb Durfee
Sent: Wednesday, June 12, 2019 2:03 PM
To: Claudette Brochu; John Langhus; John Pepper; Mary Layton; Roger Arnold (rogerarnold@gmail.com)
Cc: Miranda Bergmeier; Larry Wiggins; Roberta Robinson; Donna Flies
Subject: FW: Norwich ST BP17(23) & Norwich ST BP18(25)
Importance: High

The state's response to my inquiry about the possibility of altering RRFB locations.

Herb
Herbert A. Durfee, III
Town Manager
Town of Norwich
PO Box 376
Norwich, VT 05055
802-649-1419 ext. 102
802-698-3000 (cell)
802-649-0123 (fax)

From: Herb Durfee
Sent: Wednesday, June 12, 2019 2:01 PM
To: 'Kaplan, Jon'
Cc: Bohl, Tina; Scribner, Sue; 'Kenison, Derek W'
Subject: RE: Norwich ST BP17(23) & Norwich ST BP18(25)

Thx, Jon. The adjustments, if allowable, would have been to entirely different crosswalks. As such, I understand your reply is "no". I appreciate your getting back to me in such a timely fashion. I will continue to keep you posted on the status of the two grants.

Herb
Herbert A. Durfee, III
Town Manager
Town of Norwich
PO Box 376
Norwich, VT 05055
802-649-1419 ext. 102
802-698-3000 (cell)
802-649-0123 (fax)

From: Kaplan, Jon [mailto:Jon.Kaplan@vermont.gov]
Sent: Wednesday, June 12, 2019 12:52 PM
To: Kenison, Derek W
Cc: Herb Durfee; Bohl, Tina; Scribner, Sue
Subject: RE: Norwich ST BP17(23) & Norwich ST BP18(25)

Short answer about moving locations of funded projects around is "no". The selection committee reviewed and ranked the selected projects based on the case that was made in the application for that particular location and grants were

awarded based on the merit presented. If it's minor adjustments in location, that is one thing, but if it is a completely different crosswalk that was not originally proposed, we can't support that. I hope that makes sense.

The Town may contact me if they have more questions about this.

Jon Kaplan, P.E.
VTrans Bicycle and Pedestrian Program Manager
Municipal Assistance Bureau
1 National Life Drive
Montpelier, VT 05633

Phone: 802-498-4742 – NOTE: NEW NUMBER!!

VTrans Bicycle and Pedestrian Program Web Site
Go To: <http://vtrans.vermont.gov/highway/local-projects/bike-ped>

Municipal Assistance Bureau Sharepoint Site
Go To: <http://tinyurl.com/yddtuwaw>

From: Kenison, Derek W <Derek.Kenison@vermont.gov>
Sent: Wednesday, June 12, 2019 11:54 AM
To: Herb Durfee <HDurfee@norwich.vt.us>; Kaplan, Jon <Jon.Kaplan@vermont.gov>
Cc: Miranda Bergmeier <MBergmeier@norwich.vt.us>; Larry Wiggins <LWiggins@norwich.vt.us>; Roberta Robinson <RRobinson@norwich.vt.us>; Donna Flies <DFlies@norwich.vt.us>; Claudette Brochu <cbrochu30@gmail.com>; John Langhus <johnlanghus@gmail.com>; John Pepper <johnpepper91@gmail.com>; Mary Layton <marydlayton@gmail.com>; Roger Arnold (rogerarnold@gmail.com) <rogerarnold@gmail.com>; Bohl, Tina <Tina.Bohl@vermont.gov>
Subject: RE: Norwich ST BP17(23) & Norwich ST BP18(25)

Hi Herb,

I'll refrain from commenting on the possibility of amending the locations until I speak with Jon, but I wanted to clarify that both the 2017 & 2018 grants were subject to environmental clearance. There were some administrative changes that occurred during the period between when each grant was awarded, but each individual grant was subject to environmental clearance (see attached email for 2017 environmental clearance documents). With that said, if there is any 'wiggle' room for amending the locations of the assemblies, the new locations would be subject to the same environmental clearance process that the 2018 grant just went through.

I'll try to catch up with Jon this afternoon on the other issue.

Thanks,

Derek Kenison
P: 802.595.4316

From: Herb Durfee <HDurfee@norwich.vt.us>
Sent: Wednesday, June 12, 2019 11:31 AM
To: Kaplan, Jon <Jon.Kaplan@vermont.gov>; Kenison, Derek W <Derek.Kenison@vermont.gov>
Cc: Miranda Bergmeier <MBergmeier@norwich.vt.us>; Larry Wiggins <LWiggins@norwich.vt.us>; Roberta Robinson <RRobinson@norwich.vt.us>; Donna Flies <DFlies@norwich.vt.us>; Claudette Brochu <cbrochu30@gmail.com>; John

Langhus <johnlanghus@gmail.com>; John Pepper <johnpepper91@gmail.com>; Mary Layton <marydlayton@gmail.com>; Roger Arnold (rogerarnold@gmail.com) <rogerarnold@gmail.com>
Subject: Norwich ST BP17(23) & Norwich ST BP18(25)

Jon (& Derek),

Without getting into a long-winded background, is it possible to obtain contract amendments for either of the two above-referenced projects (both related to RRFB's)? The specificity would relate to whether or not Norwich would be allowed to change locations for one or more of the RRFB locations identified in the grant awards. We're on the precipice of installing the two sets related to the 2017 grant award, but several residents have raised some issues. I think during the Selectboard meeting tonight the board will be reaffirming the Town's decision to install the RRFB's. But, the only item that I could see them "wiggle" on relates to the locations and the possibility of moving them.

If so, what process would the Town have to adhere to? I'm assuming at least a written request along with the Town's justification for such move(s). Also, I would assume that the 2018 grant award would have to re-visit the Environmental Clearance (EC) process. The 2017 grant award did not have to undergo the EC process, but would any requested amendment to the original location trigger the EC process?

Thanks in advance for your insight. It's appreciated.

Herb

Herbert A. Durfee, III
Town Manager
Town of Norwich
PO Box 376
Norwich, VT 05055
802-649-1419 ext. 102
802-698-3000 (cell)
802-649-0123 (fax)

From: Arwen Northern <arwenlnorthern@gmail.com>
Sent: Wednesday, June 12, 2019 2:26 PM
To: Miranda Bergmeier
Subject: Crosswalk lights

Dear Selectboard members,

I feel compelled to send this email in favor of the crosswalk lights after reading a few days of Norwich residents voicing their unfavorable opinions on the listserv. I understand where the residents are coming from and respectfully disagree. I am the mom of a 2 year old, we live in town and have been daily pedestrians in Norwich for the past 2 years. Prior to being home with my daughter I didn't walk downtown as frequently as I do now. Residents who are not using the crosswalks with the frequency that I do may not be aware that there is a problem. I was surprised to discover how much traffic there is on Main Street and how many cars don't see me or do and choose not to stop. Walking with a stroller or pushing a toddler on a bike is very visible and I am always disappointed by drivers who seem to ignore or not see us. Soon I will have a toddler on her own bike, then a child who will want to walk to school on her own..

The crosswalks in Norwich at this time are not safe. I feel very grateful to the selectboard for recognizing this issue, seeking out grant money and moving forward with the project to address it. I'm sure it is difficult to weigh all the varying opinions of residents and I appreciate the time and effort you all spend doing so.

In appreciation,

Arwen Northern

Miranda Bergmeier

6a2

From: Peter White <peterwhitevt@gmail.com>
Sent: Wednesday, June 12, 2019 10:02 PM
To: Miranda Bergmeier
Subject: crosswalk lights

For the record, I'm against the crosswalk warning lights in Norwich.

Peter White
850 Newton Lane
Norwich

From: Lee Steppacher <shepstep@comcast.net>
Sent: Wednesday, June 12, 2019 5:13 PM
To: Miranda Bergmeier
Subject: flashing lights at crosswalks

I have been reading the conversation in the list serve about these lights to be installed to enhance crosswalk safety. I was totally unaware of these plans, and am appreciative of the list serve for bringing the issue up. I have not heard a specific problem articulated but think 4 pairs of flashing lights might be overdoing it. Even one pair will certainly change the nature of our town. Being motivated by 'free' money is also not the right incentive if it is not right for the town. Perhaps the funds could be used to paint the crosswalks or otherwise highlight the crosswalks, without a flashing light.

Thank you for your reconsideration of this issue,

Lee Steppacher

From: Allison Katz <allisonjo@gmail.com>
Sent: Wednesday, June 12, 2019 2:38 PM
To: Miranda Bergmeier
Subject: blinking lights

I am very sorry I did not know about the proposed blinking lights that are being considered for downtown Norwich until the recent publicity.
I am sorry I did not know when the original discussion at the Select board took place approving these lights. I have lived in Norwich for forty years, and I have seen many changes; some for the better, some not. I strongly believe that these proposed lights will not be for the better. They will take away from our village life in Norwich and will not provide any safety benefit to an already very safe downtown.
I realize that the decision about the lights has already been made, but I am urging you to reconsider.
Thank you very much,
Allison Katz
Tilden Hill, Norwich

Miranda Bergmeier

6a2

From: Cheney, Margaret <Margaret.Cheney@vermont.gov>
Sent: Thursday, June 13, 2019 12:19 PM
To: Miranda Bergmeier
Cc: David Ormiston
Subject: Flashing crossing lights

Please reverse your decision to install the electronic crossing infrastructure. Norwich does not need it; Norwich does not want it. Despite any offset from a grant, this is an unnecessary expenditure, out of character and out of scale for our village – a blight on Main Street.

Margaret Cheney
608 Bragg Hill Road

From: Ted Austin <tedaustinv@gmail.com>
Sent: Thursday, June 13, 2019 4:37 PM
To: Miranda Bergmeier
Subject: NO WAY to Crosswalk Signs / Strobe Lights / Regalia

This action is unacceptable. What information or case history even calls for such an apparatus?

Agreed (to list serve posts) & Absurd!

A classic of a 'solution looking for a problem' (that in this case, does not remotely exist). The strobe lights that accompany these traffic 'calming' apparatus are sufficiently obnoxious that you could land a plane on Main St. if installed. And just because the expense of such overkill is offset by a 'grant' does not justify the installation.

Lastly the same on Hanover's Wheelock St. has to be a function of (one has to assume - why else would anyone install them?) a population density of better than 6,000 college students blithely walking around w/ cell phones front and center in their filed-of-view and oblivious to anything in a three dimensional world while additionally taking no responsibility for their own welfare.

To act as though this decision is final and has already passed is insulting.

Ted Austin
1616 Beaver Meadow Road
802-649-2840

602

Miranda Bergmeier

From: Allison MacCormick <allisonmaccormick@gmail.com>
Sent: Friday, June 14, 2019 8:30 AM
To: Miranda Bergmeier
Subject: Crossing light

Hello,

I would just like to express my opinion that a blinking crosswalk is not a necessity in Norwich. I don't feel like there is a need and just because it is free doesn't mean we need it. I love seeing the crossing guards when school is let out and at other times there doesn't seem to be the pedestrian traffic to warrant such and obnoxious light.

Thank you for your time with regard to all matters of the town!
Have a great weekend.
Best,
Allison MacCormick
365 Chapel Hill Rd

From: Rob MacCormick <rob.maccormick@gmail.com>
Sent: Saturday, June 15, 2019 10:44 AM
To: Miranda Bergmeier
Subject: Norwich cross walk - blinking lights near Marion Cross

Greetings and salutations!

If you read this - THANKS!

I don't like the notion of an additional man made intrusion on such a beautiful and currently functional town green area. Regardless of mode of transport - Everyone just needs to pull their head out of their (phone, music, newspaper, make up application, and/or posterior) and take on the shared responsibility for safely traveling through town. That responsibility includes complying with Existing controls.

Delegating this responsibility to blinking lights is a mistake.

I'm in favor of more enforcement over more blinking lights.

Thanks for being part of running a great town!

Respectfully,

Rob MacCormick
365 Chapel hill road

From: Cindy Shannon <cshannonvt@gmail.com>
Sent: Monday, June 17, 2019 8:48 AM
To: Miranda Bergmeier
Subject: Selectboard@norwich.vt.us

To Select Board members,

Like many other Norwich citizens, I am deeply opposed to the installation of crosswalk lights. However, I am even more upset that the decision to install these lights was not made in a democratic manner. The Select Board needs to organize a vote on this issue, so that citizens may weigh in on this proposed change to our village center.

I moved to Norwich three years ago because it was a community that looked out for each other and paid attention to each other as we walked, shopped and drove through the village. I thought we were a little pocket of "democracy in action" in a nation where decisions are more and more made only by those at the top. The proposed new lights will affect every single person in Norwich who walks or drives through the village center. This project must be voted upon by our citizens.

There was a lot of publicity about hiring a community nurse before a decision was to be made. Information was provided, a survey was taken, etc. Not everyone will be using that service, but we were all asked to weigh in. Hiring a community nurse would be expensive, so it was wise to involve the public.

Installing crosswalk lights will not be as expensive as a community nurse. But it will impose a different and weighty cost on our village. Those blinking lights will degrade the beauty and simplicity of our village center. They will weaken the message our village center has always communicated: that we are a community that watches out for each other, a community that refuses to allow the "Big Brother" of technology to infect our village culture as it is already infecting our all lives in so many other ways.

Please reconsider the manner in which the decision to install crosswalk lights has been made. The Select Board has the opportunity and the responsibility to show our citizens that our village lives out America's democratic values. Fulfilling this responsibility is especially important now, as our national democracy is eroding. You can remind us that here in Norwich, at least, the people have a voice.

Thank you for taking the time to read this email.

Cindy Shannon
829 Union Village Road

Miranda Bergmeier

602

CORRESPONDENCE
1252 6/26/14

From: Miranda Bergmeier
Sent: Wednesday, September 26, 2018 2:28 PM
To: 'norwich@lists.vitalcommunities.org'
Subject: Outdoor Projects at Tracy Hall

Exciting things are happening at Tracy Hall! More specifically, there are 3 SEPARATE projects that require digging:

1. The work to install the new flagpole and the new monument is well underway.
2. Two new sets of pedestrian crossing signal light systems will be installed (in front of Tracy Hall and between the Historical Society and the Town Green). This has been marked out with spray paint in the area where the current pedestrian crossing street signs are located.
3. The underground fuel oil storage tank has to be dug up for inspection. This area has been marked by DigSafe.

Please excuse our appearance while we address these separate projects. If you have questions, feel free to reach out to the TM's office.

6a3

June 21, 2019

John M. Farrell

PO BOX 403

Norwich, VT

05055

To: Select Board Norwich, Vermont

Cc: Pat Moulton, President, VTC

Subject: Norwich Farms sign change

I would like to bring to your attention a signage change at 723 Turnpike Road, Norwich, Vermont. The group of buildings comprising NORWICH FARMS had a sign that simply read NORWICH FARMS which, I believe, is the correct and legal name for this group of buildings. Recently the owners of Norwich Farms Creamery have changed the sign to read NORWICH FARMS CREAMERY which indicates that ALL the buildings are Norwich Farms Creamery which is simply not true.

I would like to request that someone on the select board find out if this is legal and notify me of the results of the inquiry.

Thank you very much,

John M. Farrell

Norwich resident

624

Bonnie Munday

From: bartlett leber <bartlettleder@gmail.com>
Sent: Monday, June 17, 2019 11:20 AM
To: Bonnie Munday
Subject: CRJC resignation
Attachments: CRJC resignation.pdf

Hi Bonnie — attached is my letter of resignation from the CRJC — I was one of the two reps with Melissa Horvitz. Regrets, I just don't have the time to do a decent job and you can find a better volunteer! Will drop off a hard copy later in the day.

Many thanks for the opportunity!
Best, B

Will drop off a hard copy at Tracy hall.

06/21/19

Town of Norwich Accounts Payable

66

11:28 am

Check Warrant Report # 19-26 Current Prior Next FY Invoices For Fund (General)

DFlies

All Invoices For Check Acct 03(General) 06/26/19 To 06/26/19

Vendor	Invoice Description	Invoice Date	Invoice Number	Account	Amount Paid	Check Number	Check Date
ADVANCE	ADVANCE AUTO PARTS	05/29/19	DPW-WIPER BLADES 00041	01-5-703403.00 PARTS & SUPPLIES	19.48	7762	06/26/19
ADVANCE	ADVANCE AUTO PARTS	05/24/19	DPW-BATTERY FARM TRUCK 09912	01-5-703403.00 PARTS & SUPPLIES	185.78	7762	06/26/19
ADVANCE	ADVANCE AUTO PARTS	06/03/19	DPW-BATTERY RETURN 15283	01-5-703403.00 PARTS & SUPPLIES	-27.00	7762	06/26/19
ADVANCE	ADVANCE AUTO PARTS	06/14/19	DPW-BRAKES & U JOINT 60486	01-5-703403.00 PARTS & SUPPLIES	374.36	7762	06/26/19
ADVANCE	ADVANCE AUTO PARTS	05/23/19	DPW-TRACTOR HYD FLUID 73175	01-5-703403.00 PARTS & SUPPLIES	49.00	7762	06/26/19
ADVANCE	ADVANCE AUTO PARTS	05/29/19	DPW HYDRAULIC CUPLERS 73238	01-5-703403.00 PARTS & SUPPLIES	83.00	7762	06/26/19
ADVANCE	ADVANCE AUTO PARTS	06/17/19	DPW-FRONT CALIPER 73820	01-5-703403.00 PARTS & SUPPLIES	192.78	7762	06/26/19
ADVANCE	ADVANCE AUTO PARTS	05/03/19	DPW-GLOW PLUG/GASKET 85980	01-5-703403.00 PARTS & SUPPLIES	141.32	7762	06/26/19
ADVANCE	ADVANCE AUTO PARTS	05/14/19	DPW-OIL FILTER 86331	01-5-703403.00 PARTS & SUPPLIES	25.96	7762	06/26/19
ADVANCE	ADVANCE AUTO PARTS	05/29/19	B&G -HYDRAULIC OIL 86619	01-5-704403.00 PARTS & SUPPLIES	99.50	7762	06/26/19
ADVANCE	ADVANCE AUTO PARTS	06/06/19	DPW-OIL SEAL 86810	01-5-703403.00 PARTS & SUPPLIES	23.99	7762	06/26/19
ANYTIME	ANYTIME CARPET CARE & CLE	06/02/19	PD-TAR ON CARPET 964757	01-5-500501.00 ADMINISTRATION	135.00	7763	06/26/19
ANYTIME	ANYTIME CARPET CARE & CLE	06/02/19	PUB SAF-CLEANING 964761	01-5-485302.00 REPAIRS & MAINTENANCE	360.00	7763	06/26/19
BAYSTATE	BAY STATE ELEVATOR CO.	06/01/19	TH-MAY 19 ELEVATOR 504813	01-5-706107.00 ELEVATOR MAINTENANCE	261.26	-----	- / - / --
CASELLA	CASELLA WASTE SERVICES	06/01/19	TS-MAY RECYCLING 0490597	01-5-705305.00 RECYCLING	3699.87	-----	-- / -- / --
CASELLA	CASELLA WASTE SERVICES	06/01/19	TS-MAY RECYCLING 0490597	01-5-705308.00 FOOD WASTE DISPOSAL	562.90	-----	- / - / --
CASELLA	CASELLA WASTE SERVICES	06/01/19	TS-MAY TRASH 0490598	01-5-705306.00 C & D WASTE DISPOSAL	1330.91	-----	-- / -- / --
CASELLA	CASELLA WASTE SERVICES	06/01/19	TS-MAY TRASH 0490598	01-5-705303.00 MUNICIPAL SOLID WASTE	4396.48	-----	-- / -- / --
COMCAST	COMCAST	06/01/19	PD/FD PHONE INTERNET 27121 JUNE19	01-5-500501.00 ADMINISTRATION	200.91	7764	06/26/19
COMCAST	COMCAST	06/01/19	PD/FD PHONE INTERNET 27121 JUNE19	01-5-555625.00 TELEPHONE & INTERNET	135.90	7764	06/26/19
COMCAST	COMCAST	05/20/19	DPW TRIPLE PLAY 28756 MAY 19	01-5-703505.00 TELEPHONE	137.64	7764	06/26/19
COMCAST	COMCAST	06/06/19	TH FIXED IP 5945 JULY 19	01-5-275632.00 SERVER MAINTENANCE	19.95	7764	06/26/19
COTT	COTT SYSTEMS INC	06/18/19	TC HOSTING 128876	01-5-100613.00 SOFTWARE	260.00	-----	-- / -- / --
CRICKET'S	CRICKET'S PAINT & AUTO PA	06/12/19	DPW-SHOP HOSE 744044	01-5-703511.00 REPAIRS & MAINTENANCE	22.99	7765	06/26/19
CRYSTAL	CRYSTAL ROCK, LLC	05/28/19	TS/GARAGE WATER 052819	01-5-703507.00 SUPPLIES	78.60	-----	-- / -- / --

Vendor	Invoice Date	Invoice Description Invoice Number	Account	Amount Paid	Check Number	Check Date
DEWOLFE	05/15/19	COM CON ENGINEERING 19-028019111	01 5-650630.00 TRAILS	315.00	7766	06/26/19
EVANSMOTO	05/28/19	DPW- 380 G DIESEL 661926	01-5-703405.00 PETROLEUM PRODUCTS	884.37	-----	--/--/--
FOGGS	06/04/19	DPW-DIG SAFE PAINT 856054	01-5-703403.00 PARTS & SUPPLIES	81.48	7767	06/26/19
FOGGS	06/10/19	DPW-CONTRACTOR TRASH BAGS 856459	01-5-703403.00 PARTS & SUPPLIES	13.99	7767	06/26/19
FOGGS	06/13/19	B&G-NORWICH SIGN 856830	01-5-704403.00 PARTS & SUPPLIES	22.98	7767	06/26/19
FOGGS	06/14/19	DPW-UV MAILBOX///ADAM 856998	01-5-703209.00 CULVERTS & ROAD SUPPLIES	16.93	7767	06/26/19
FREIGHTNH	05/30/19	DPW-ELECTRICAL SHUT OFF LP291330	01-5-703403.00 PARTS & SUPPLIES	81.04	7768	06/26/19
FREIGHTNH	06/03/19	DPW-CREDIT LP291268 LP291473	01-5-703403.00 PARTS & SUPPLIES	-25.33	7768	06/26/19
FREIGHTNH	06/13/19	DPW-AIR CAN PRESSURE RELI LP292010	01-5-703403.00 PARTS & SUPPLIES	16.41	7768	06/26/19
GUVSAMD	06/13/19	HCCR PUNCH CARDS N-PC19-75	01-2-001125.00 DUE TO GUVSAMD-COUPONS	3225.00	7769	06/26/19
HARTFORD	04/08/19	PD DISPATCH SERVICES 10056	01-5-500536.00 DISPATCH SERVICES	14013.31	-----	--/--/--
HAUN	06/01/19	DPW-OXYGEN U214689	01-5-703507.00 SUPPLIES	15.58	-----	--/--/--
JOESEQUIP	05/21/19	B&G-FILTER FOR TRIMMER 2-126814	01-5-704403.00 PARTS & SUPPLIES	1.95	-----	--/--/--
MIS 1	05/30/19	PD-CHIEF BADGES PD 715	01-5-500582.00 UNIFORMS	268.62	7770	06/26/19
MIS 2	06/18/19	LIST-FRUIT BASKET 6.19.19	01-5-300610.00 OFFICE SUPPLIES	34.99	7771	06/26/19
NEDIESEL	05/31/19	DPW-INJECTION PUMP SEALS 4981	01-5-703403.00 PARTS & SUPPLIES	234.74	-----	--/--/--
NORTHWOOD	06/10/19	DPW MOVE GRADER 16849	01-5-703401.00 OUTSIDE REPAIRS	450.00	7772	06/26/19
PATHWAYS	05/31/19	FEMA PROJECT ENGINEER 21771	01-5-703703.00 FEMA GRANT	1660.00	7773	06/26/19
RIVERROAD	11/30/18	PD STRAY CAT 334024	01 5-500201.00 ANIMAL CONT/LEASH LAW	297.00	7774	06/26/19
SABIL	05/31/19	FD INSP AND BRAKE WORK 84860	01-5-555528.00 FIRE TRK R & M	543.73	7775	06/26/19
SOLAFLECT	06/18/19	JUNE MTHLY ALLOCATION JUNE 19	01-5-706101.00 ELECTRICITY	783.60	-----	--/--/--
SOLAFLECT	06/18/19	JUNE MTHLY ALLOCATION JUNE 19	01-5-703501.00 ELECTRICITY	64.53	-----	--/--/--
TENCO	06/13/19	DPWSEALS 6625205	01-5-703403.00 PARTS & SUPPLIES	63.55	-----	--/--/--
TROTTIER	05/31/19	B&G-TRACTOR REPAIR 01-52874	01-5-704403.00 PARTS & SUPPLIES	137.36	7776	06/26/19
TROTTIER	06/03/19	B&G-MOWER PARTS 01-53030	01-5-704403.00 PARTS & SUPPLIES	218.67	7776	06/26/19

Vendor	Invoice Date	Invoice Description Invoice Number	Account	Amount Paid	Check Number	Check Date
UNIFIRST	06/03/19	DPW-UNIFORMS 035 4379109	01-5-703311.00 UNIFORMS	198.33	-----	--/--/--
UNIFIRST	06/03/19	B&G-UNIFORMS 035 4379487	01-5-704311.00 UNIFORMS	71.71	-----	--/--/--
UNIFIRST	06/10/19	DPW-UNIFORMS 035 4381350	01-5-703311.00 UNIFORMS	258.28	-----	--/--/--
UNIFIRST	06/10/19	B&G-UNIFORMS 035 4381726	01-5-704311.00 UNIFORMS	71.71	-----	--/--/--
UNIFIRST	06/17/19	DPW-UNIFORMS 035 4383576	01-5-703311.00 UNIFORMS	268.68	-----	--/--/--
UNIFIRST	06/17/19	B&G-UNIFORMS 035 4383951	01-5-704311.00 UNIFORMS	71.71	-----	--/--/--
UVEQUIPME	05/17/19	UPPER VALLEY EQUIPMENT RE DPW-ENGINE HOIST RENTAL 095173	01-5-703403.00 PARTS & SUPPLIES	80.00	7777	06/26/19
VERIZWIRE	06/04/19	VERIZON WIRELESS TM, PD, DPW, FD-CELL PHONES 9831463123	01-5-005532.00 T MNGR CELL PHONE	56.31	-----	--/--/--
VERIZWIRE	06/04/19	VERIZON WIRELESS TM, PD, DPW, FD-CELL PHONES 9831463123	01-5-500501.00 ADMINISTRATION	109.60	-----	--/--/--
VERIZWIRE	06/04/19	VERIZON WIRELESS TM, PD, DPW, FD-CELL PHONES 9831463123	01-5-555625.00 TELEPHONE & INTERNET	25.42	-----	--/--/--
VERIZWIRE	06/04/19	VERIZON WIRELESS TM, PD, DPW, FD-CELL PHONES 9831463123	01-5-703505.00 TELEPHONE	189.95	-----	--/--/--
VLCT	06/11/19	VERMONT LEAGUE OF CITIES TADMIN-HR WORKSHOP 2019-20678	01-5-005615.00 DUES/MTS/EDUC	60.00	7778	06/26/19
WILSONTIR	05/29/19	WILSON TIRE, INC DPW-TIRE CHANGE 485348	01-5-703401.00 OUTSIDE REPAIRS	108.80	7779	06/26/19
Report Total				37760.58		

To the Treasurer of Town of Norwich, We hereby certify that there is due to the several persons whose names are listed hereon the sum against each name and that there are good and sufficient vouchers supporting the payments aggregating \$ ****37,760.58
Let this be your order for the payments of these amounts.

FINANCE DIRECTOR 
Donna Flies

TOWN MANAGER: 
Herb Durfee, Town Manager

SELECTBOARD:

John Langhus Roger Arnold Claudette Brochu John Pepper, Chair Mary Layton

06/21/19
02:17 pm

Town of Norwich Accounts Payable
Check Warrant Report # 20-01 Current Prior Next FY Invoices For Fund (General)
For Check Acct 03(General) All check #s 07/03/19 To 07/03/19

66

Vendor	Invoice Description	Invoice Date	Invoice Number	Account	Amount Paid	Check Number	Check Date
BCBS	BLUE CROSS/BLUE SHIELD OF	06/04/19	JULY HEALTH INSURANCE	01-5-005123.00	4422.16	7780	07/03/19
			JULY 2019	HEALTH INSUR			
BCBS	BLUE CROSS/BLUE SHIELD OF	06/04/19	JULY HEALTH INSURANCE	01-5-100123.00	2458.69	7780	07/03/19
			JULY 2019	HEALTH INS			
BCBS	BLUE CROSS/BLUE SHIELD OF	06/04/19	JULY HEALTH INSURANCE	01-5-350123.00	1137.26	7780	07/03/19
			JULY 2019	HEALTH INS			
BCBS	BLUE CROSS/BLUE SHIELD OF	06/04/19	JULY HEALTH INSURANCE	01-5-425123.00	2211.08	7780	07/03/19
			JULY 2019	HEALTH INS			
BCBS	BLUE CROSS/BLUE SHIELD OF	06/04/19	JULY HEALTH INSURANCE	01-5-555123.00	1604.40	7780	07/03/19
			JULY 2019	HEALTH INSURANCE			
BCBS	BLUE CROSS/BLUE SHIELD OF	06/04/19	JULY HEALTH INSURANCE	01-5-704123.00	2083.55	7780	07/03/19
			JULY 2019	HEALTH INSURANCE			
BCBS	BLUE CROSS/BLUE SHIELD OF	06/04/19	JULY HEALTH INSURANCE	01-5-703123.00	6953.06	7780	07/03/19
			JULY 2019	HEALTH INSUR			
BCBS	BLUE CROSS/BLUE SHIELD OF	06/04/19	JULY HEALTH INSURANCE	01-5-500123.00	4186.41	7780	07/03/19
			JULY 2019	HEALTH INS			
LINCOLN	LINCOLN FINANCIAL GROUP	07/01/19	DISABILITY & LIFE INSURAN	01-5-005124.00	72.09	7781	07/03/19
			JULY 2019	DISABILITY/LIFE INSUR			
LINCOLN	LINCOLN FINANCIAL GROUP	07/01/19	DISABILITY & LIFE INSURAN	01-5-100124.00	58.75	7781	07/03/19
			JULY 2019	DISABILITY/LIFE INS			
LINCOLN	LINCOLN FINANCIAL GROUP	07/01/19	DISABILITY & LIFE INSURAN	01-5-200124.00	69.74	7781	07/03/19
			JULY 2019	DISABILITY/LIFE INS			
LINCOLN	LINCOLN FINANCIAL GROUP	07/01/19	DISABILITY & LIFE INSURAN	01-5-350124.00	37.61	7781	07/03/19
			JULY 2019	DISABILITY/LIFE INS			
LINCOLN	LINCOLN FINANCIAL GROUP	07/01/19	DISABILITY & LIFE INSURAN	01-5-425124.00	35.96	7781	07/03/19
			JULY 2019	DISABILITY/LIFE INSUR			
LINCOLN	LINCOLN FINANCIAL GROUP	07/01/19	DISABILITY & LIFE INSURAN	01-5-704124.00	48.84	7781	07/03/19
			JULY 2019	DISABILITY/LIFE			
LINCOLN	LINCOLN FINANCIAL GROUP	07/01/19	DISABILITY & LIFE INSURAN	01-5-555124.00	34.99	7781	07/03/19
			JULY 2019	DISABILITY/LIFE INSURANCE			
LINCOLN	LINCOLN FINANCIAL GROUP	07/01/19	DISABILITY & LIFE INSURAN	01-5-703124.00	806.13	7781	07/03/19
			JULY 2019	DISABILITY/LIFE			
LINCOLN	LINCOLN FINANCIAL GROUP	07/01/19	DISABILITY & LIFE INSURAN	01-5-500124.00	115.90	7781	07/03/19
			JULY 2019	DISABILITY/LIFE INS			
PITNEY	PITNEY BOWES	05/30/19	GENADM-POSTAGE METER	01-5-275536.00	171.21	7782	07/03/19
			3308925922	POSTAGE METER RENTAL			
POSTMASTE	POSTMASTER NORWICH	06/30/19	PD-12-20 BOX RENT	01-5-500501.00	92.00	7783	07/03/19
			BOX311 19-20	ADMINISTRATION			
POSTMASTE	POSTMASTER NORWICH	06/30/19	GEN ADMIN-ANNUAL BOX RENT	01-5-275538.00	92.00	7783	07/03/19
			PO BOX 376	POSTAGE			
VLCTERB	VLCT EMPLOYMENT RESOURCE	06/20/19	3RD QTR UNEMPLOYMENT	01-5-800517.00	639.00	7784	07/03/19
			28683-Q3	UNEMP INS RATE ASSMT			
VLCTPACIF	VLCT PROP & CASUALTY INTE	06/20/19	LIABILITY & WORKERS COMP	01-5-800520.00	67642.00	7785	07/03/19
			2019 CONTRIB	WORKER'S COMP INS			
VLCTPACIF	VLCT PROP & CASUALTY INTE	06/20/19	LIABILITY & WORKERS COMP	01-5-800518.00	35576.50	7785	07/03/19
			2019 CONTRIB	PROP & CAS INSURANCE			
WINDSORCL	WINDSOR COUNTY TREASURER	06/20/19	1/2 COUNTY TAX BOND	01-2-001128.00	9617.00	7786	07/03/19
			12-20 BOND	DUE TO WINDSOR COUNTY			
WINDSORCL	WINDSOR COUNTY TREASURER	06/20/19	1/2 COUNTY TAX	01-2-001128.00	18547.00	7787	07/03/19
			19-20 TAX	DUE TO WINDSOR COUNTY			

06/21/19
02:17 pm

Town of Norwich Accounts Payable
Check Warrant Report # 20-01 Current Prior Next FY Invoices For Fund (General)
For Check Acct 03(General) All check #s 07/03/19 To 07/03/19

Vendor	Invoice Date	Invoice Description Invoice Number	Account	Amount Paid	Check Number	Check Date
Report Total				158713.33		

To the Treasurer of Town of Norwich, We hereby certify that there is due to the several persons whose names are listed hereon the sum against each name and that there are good and sufficient vouchers supporting the payments aggregating \$ ***158,713.33
Let this be your order for the payments of these amounts.

FINANCE DIRECTOR 
Donna Flies

TOWN MANAGER: 
Herb Durfee, Town Manager

SELECTBOARD:

John Langhus Roger Arnold Claudette Brochu John Pepper, Chair Mary Layton

(6c)

DRAFT Minutes of the Selectboard Meeting of
Wednesday, June 12, 2019 at 6:30 pm

Members present: John Pepper, Chair; Claudette Brochu, Vice Chair; Roger Arnold; John Langhus; Mary Layton; Herb Durfee, Town Manager; Miranda Bergmeier, Assistant to the Town Manager.

There were about 8 people in the audience.

Also participating: Cheryl Lindberg, Christine Pinello, Sally Matless, Elizabeth Perry, Fran Walls, Niri (unknown last name), Colleen Fox, Jeff Reed, Doris Nelson, Kris Clement, and Peter Warner.

1. Approval of Agenda. Selectboard (SB) members agreed to proceed with the agenda as drafted.
2. Public Comment. John Pepper talked about parking signs at Squam Lake that might be examples for what to post at Gile Mountain parking area.
3. Animal Control Update. Durfee gave an update about the town's animal pound, saying that he and the Police Chief met with Christine Pinello recently. Based on their discussion, Durfee drafted a Memorandum of Understanding (MOU) and sent it to Pinello for her review and comment. Pinello said that she was glad to have met with Durfee and the Police Chief and she appreciated Brochu's suggestion that they have a meeting. Pinello then said that she want to be paid for her services, regardless of the cost, rather than agree in an MOU to a "not-to-exceed" amount. SB members agreed that Pinello should be paid for whatever services she provides. Arnold asked Durfee if he wants the SB to give input on a review of the leash law. Sally Matless said that it is important to spay and neuter your pets and Elizabeth Perry spoke in support of Pinello.
4. Solar-Powered RRFBs. Durfee said that Norwich was awarded 2 separate grants for two sets of Rectangular Rapid Flashing Beacons (RRFBs). One grant was awarded in 2017 and one in 2018, for RRFBs at four crosswalks: Tracy Hall, Town Green, the Norwich Inn, and the Church Street school crossing. Durfee said that the subject of RRFBs has been addressed from time to time in SB packets since 2017. Fran Walls said that she doesn't agree with installing RRFBs. Niri, a student, said that she uses the crosswalks a lot and she thinks it will be much safer with RRFBs in place. She has had the experience of being caught in the crosswalk when drivers do not stop. Colleen Fox said that she is in favor of installing RRFBs because they will help remind drivers to stop at crosswalks. Cheryl Lindberg suggested that the town wait before it installs the second set of RRFBs. Jeff Reed asked if the RRFBs issue had been publicly warned and discussed prior to the grants being applied for. SB members answered that yes, it had. Reed asked why the issue is being discussed again after the decision has been made. Pepper said that SB members have gotten a large volume of comments about the RRFBs, and so they wanted to be sure to give time during the meeting for people to speak to the SB. Langhus said that he would like to see the SB hold a special meeting with committee/commission members and members of the public to review procedures and regulations around public meetings and public bodies. Doris Nelson said that she opposes RRFBs and thinks that our police should enforce speed limits. Kris Clement said that she wants Norwich police to issue more tickets, rather than putting up flashing signs. Brochu said that she challenges people to sit at Tracy Hall and watch drivers speed, roll through stop signs, and drive through crosswalks not stopping for pedestrians. She said that RRFBs will help drivers see pedestrians in low visibility situations. We haven't had a death yet, but that is because we have been lucky. Peter Warner said he thinks there are a lot of people in town who support RRFBs. Warner believes that the SB is doing the right thing and we shouldn't wait for someone to get hurt. Colleen Fox said that RRFBs will allow

visibility for pedestrians crossing without having to have streetlights, thus minimizing light pollution. Arnold asked why the state had decided to award the grant. Durfee said that the state considered the factors of traffic volume, number of pedestrians, presence of a school, and public buildings/businesses that draw people. Langhus said it's fine for people to offer input, but it's not fair to criticize the Town Manager for getting grants. The SB sets policy goals and overall spending priorities, and the Town Manager looks to implement those in the ways most advantageous to the town. Fran Walls asked for a tally of emails in favor and against RRFBs. Pepper said that he reached out to the 5th grade class at MCS for their opinions about RRFBs. They took a straw poll and the majority of students were in favor of them. Durfee summarized a large number of citizen emails about RRFBs, the large majority of which were against. [*Copies of all emails not included in the 6/12/19 packet will be made part of the 6/26/19 SB packet.] Kris Clement said that she keeps hearing that the speed of traffic is a problem, and she wants the Norwich police to ticket drivers because they don't issue enough tickets. Sally Matless said that she thinks that rural character is very important in Norwich and keeping speed low is part of that.

5. Expanded Plastics Recycling Update. Durfee said that he has met with Norwich Transfer Station staff members and they are getting geared up to accept additional plastics for recycling. They are looking at three options: 1) Town staff transports materials to Barre; 2) Town staff transports materials to Hannaford; or 3) current hauler, Casella, collects materials and transports to Hypertherm. Durfee is working on a cost analysis of the options. Also, Durfee is putting together educational flyers for citizens, which explain how the new recycling will work. We will need Town staff to monitor for contaminants in the recycling stream. Durfee said that he will be talking with Langhus about the recycling options.

6. Consent Agenda. Brochu said that she would like to pull out of the consent agenda the correspondence from Bonnie Munday regarding delinquent dog licenses. Langhus moved (2nd Brochu) to approve the consent agenda, absent delinquent dog owners. **Motion passed unanimously.** SB members agreed to ask the Town Manager to work with the Police Chief on the issue of unlicensed dogs.

7. Town Manager Report...

7d. Authorize Town Manager to Apply for VTrans 2019 Bike & Pedestrian Grant. Peter Warner and Colleen Fox, residents on Beaver Meadow Road, said that they and a group of other residents recently met with the Town Manager and Police Chief about looking to the possibility of extending the sidewalk on Beaver Meadow Road. Langhus and Layton thanked the residents for their efforts and encouraged them to gather as much public input as possible. Durfee said that the VTrans grant would fund a scoping analysis to look into possibly extending the sidewalk, and this would include seeking public input. Brochu asked if the town would have to repay funds if we decide we don't want to go through with constructing a sidewalk. Durfee said that this grant would not provide funds to build the sidewalk, so we would not be committing to doing that. Fox said that her neighbors and she are generally in favor of constructing a sidewalk and they are in the beginning stages of reaching out to other town residents for input. Warner said that he doesn't want to be waiting for something bad to happen before doing a sidewalk project. Layton asked what the timeline might be on such a project – four years? Durfee said possibly yes. Linda Cook said that this is a repeat of the beginning of the Church Street sidewalk project that failed. Cook said that we should open up public discourse now, so that everyone can offer input now. Cook asked about how much money would be the town's share of the scoping analysis cost. Durfee said it would be approximately \$6,000. Pepper asked if the SB's motion should include a dollar amount to be clear about how much money the town will commit to spending. Durfee said that, after the town finds out whether they will be offered a grant, they can decide if they want to accept the grant and how much money they will commit to spending. Langhus

asked the Beaver Meadow Road residents if they are ready to reach out to neighbors to solicit input about a potential sidewalk – both in favor and against the project. The residents said that yes, they are. Langhus **moved** (2nd Arnold) to authorize the Town Manager to apply for a VTrans 2019 Bicycle and Pedestrian Grant to conduct scoping analysis inclusive of soliciting public input and preparing conceptual options for a possible sidewalk along Beaver Meadow Road connecting the existing sidewalk at Bridge 39 with the existing sidewalk at Moore Lane. **Motion approved unanimously.**

7a. RFP for Bank Loan. Durfee said that he has put out an RFP for a bank loan, per the terms of Article 11 of the 2019 Town Meeting. Brochu asked if another bank had a lower bid than Mascoma. If so, why would we choose Mascoma? Brochu said she doesn't necessarily object to choosing Mascoma, but asked Durfee to request that they match the other bank's lower interest rate. Durfee said that he would do that. Langhus **moved** (2nd Pepper) to authorize the Town Manager to accept the loan bid dated May 9, 2019 submitted by Mascoma Bank, such bid having been solicited pursuant to Article 11 approved by Norwich voters at 2019 Town Meeting and recommended for acceptance by the Norwich Finance Director and Town Manager. **Motion approved unanimously.**

7b. 2019 PACIF Grant for Fire Dept. Equipment. Brochu **moved** (2nd Pepper) to authorize the expenditure of up to \$6,102.20 from the Fire Department Equipment Designated Fund to pay for the town's 50% cost share under the terms of a grant awarded by VLCT –PACIF. **Motion approved unanimously.**

7c. Reauthorize Line of Credit (Storm Loan). Durfee explained that the town will need additional money after June 30 2019 when the current line of credit will expire. Langhus **moved** (2nd Layton) to authorize the Town Manager to sign loan documents reauthorizing the line of credit with Mascoma Bank, originally instituted in connection with July 2017 storm repair expenses and as specifically reauthorized at the 2019 Town Meeting. **Motion approved unanimously.** Cheryl Lindberg asked about the Listers budget - specifically the \$20,000 projected surplus. Lindberg said that the Listers would like to have that money put into the Listers designated fund. Durfee said that at this time it is premature to set aside that money, because the town budget needs to be looked at as a whole to see what surplus we might or might not actually have. When the SB builds the next fiscal year's budget in the fall, that will be the time to make that request. Lindberg said the Listers had 15-16 grievances this time and just had their hearings today. Brochu asked about the cell antenna pole information that was included in the SB packet. Durfee said that AT&T has given the town notice of their application to install a pole. The town can choose whether or not to chime in on the issue. SB members agreed to put the issue on their next agenda. Durfee said that he has drafted a repair work form for use by town departments. The town has gone out to bid for gravel and for paving. Norwich was awarded paving and structures grants. Durfee is working with the Norwich Historical Society on a proposal for an historical marker sign regarding the former town pool. Durfee has been working on Illsley Road issues and the interested parties are in agreement thus far about a plan for installing a culvert. Durfee has started the early stages of working on a possible granite bench installation on Main Street near Tracy Hall on private property. Durfee has completed final sign off on the submission for \$1.4 million in FEMA funds for 2017 storm damage. Brochu said that she had asked about why the town is buying premium gasoline. Durfee said that Police Chief Frank and DPW Director Wiggins both said that non-premium gas is OK for everything except the grounds keeping equipment. DPW is looking at options for supplying non-ethanol gas for the equipment that needs it. The town may need to buy a storage tank.

8. Distribution of Reusable Shopping Bags Update. Brochu said that Mascoma Bank has donated canvas shopping bags for town citizens in need. Linda Cook will help to distribute the bags. SB members publicly thanked Mascoma for their donation.

9. Selectboard Policies Status. SB members discussed options for continuing their work on updating SB policies.

10. Future Meeting Agenda Items. SB members agreed to the following agenda items for their next meeting:

Proposed AT&T Cell Antenna

Town Fee Schedule Update

Affordable Housing

Regional Plan – Develop Town’s Comments w/Planning Commission and Planning Director

Appoint Advance Transit Rep.

At 9:46 pm, Langhus **moved** (2nd Brochu) to adjourn. **Motion passed unanimously.**

Meeting adjourned at 9:46 pm.

By Miranda Bergmeier

Approved by the Selectboard on _____, 2019

John Pepper
Selectboard Chair

Next Meeting – June 26, 2019 – Regular Meeting at 6:30 PM

PLEASE NOTE THAT CATV RECORDS ALL REGULAR MEETINGS OF THE NORWICH SELECTBOARD.



TO: Herb Durfee, Town Manager
FROM: Rod Francis, Planning Director
RE: Draft TRORC Town Plan
DATE: 06/20/2019

KEY POINTS

The 2019 Draft Regional Plan:

- Deletes the mapped interchange area (I-C Area) and specific text describing the Norwich interchange area
- Converts the Lewiston neighborhood from I-C Area to Rural
- Adds a Mixed Use Area along Rte. 5 S
- Relies on definitions and policies for Rural and Mixed Use Areas to provide development parameters. Neither of these regional land use categories correspond closely with existing Norwich zoning
- Increases potential for residential development

INTRODUCTION

In Vermont a Regional Planning Commission (RPC) has the statutory authority to make regional land use policy. They create this policy in a similar way to towns when they draft town plans. Statute requires both bodies to use essentially the same planning process which meets specified minimum standards of data collection and analysis, and public and stakeholder outreach, including the opportunity to comment on a draft of the plan in duly warned public hearings. The Town of Norwich has an interest in how the proposed changes to the TRORC Regional Plan effect potential development in the subject areas. As a stakeholder, the Town of Norwich can comment on the proposed 2019 TRORC Regional Plan (TRORC Plan) by July 11.

The focus of this memorandum is on the Land Use chapter of the proposed plan, specifically Mixed Use (see TRORC Plan p35) and Rural Areas (see TRORC Plan, p42). The key difference between the current plan and the proposed plan is that Norwich, along with some other towns previously covered by the interchange policy are no longer covered by the interchange policy (see TRORC Plan, p36). In Norwich these two regional land use designations replace the 'interchange policy' of the previous plan. The new plan contains policies and definitions of terms (such as 'rural area', 'mixed use' and 'principal retail'). Broadly speaking there is now more scope for housing development in the subject area than the existing plan provided for.

EXISTING POLICY

Norwich has a mapped Interchange area with a prescribed range of land uses. This policy is applied to some, but not all, interstate interchanges in the TRORC region. The policy describes land uses focused on transportation/travel services (although a town could be more restrictive in terms of specific uses). Key is the exclusion of 'principal retail' as a use. The purpose of this policy is to direct most retail to other land use districts such as designated village centers and growth centers (Hartford is a designated growth center).

PROPOSED POLICY

There is no longer specific language or a mapped "interchange area" for Norwich. Rather, the approach in the 2019 TRORC Plan is to identify a new "Mixed Use" district running south from Route 10 along Route 5 (see attached map). The Lewiston neighborhood is re-assigned to "Rural Area" as is the land on the western side of Route 5 South from approximately King Arthur Flour to just south of Hopson Rd. Of note is that Foggs lumberyard and hardware (the most southerly parcel in Norwich on the western side of Route 5) has been added to Hartford Growth Center Area (see map), although as a lumberyard it is a type of commercial establishment permitted in a mixed use area.

The TRORC Plan contains the following definitions:

MIXED USE AREA — the future land use area ... with a mixture of existing uses that is served by state highways, and is appropriate for recreational facilities, higher intensity residential, light industrial/manufacturing, land intensive commercial uses and use not appropriate for the core of downtowns and villages, such as lumberyards, nurseries, warehouses and kennels. Principal retail is not allowed in this area.

RURAL AREA — future land use area identified as such in the Regional Land Area map and is a regional land use designation.

PRINCIPAL (PRIMARY) RETAIL — the supply of merchandise or wares to the end consumer for use off site. Examples include (but are not limited to) supermarkets, hardware stores (without lumberyards), dry good stores, pharmacies, big box stores etc. Principal retail does not include online sales with no product on site, land intensive and resource based commercial uses, restaurants, retail as a home occupation, or secondary retail.

Existing Norwich Zoning Districts have not corresponded closely to these definitions, or the associated policies. The Lewiston neighborhood is zoned village Residential. Lands to the east of Rte. 5 S are zoned Commercial/Industrial. Lands to the west are zoned Rural Residential. Under statute towns *may* have more restrictive development controls (i.e. a narrower set of permitted and conditional uses) than regional future land use areas but *cannot* be more permissive.

In larger-scale projects where there is a statutorily defined regional impact RPCs have party status to Act 250 proceedings. Under Act 250 the future land use policies, map and definitions prevail over local plans and zoning regulations when there is a substantial regional impact. TRORC uses eight criteria that are reviewed to determine if a development will result in substantial regional impact (see TRORC Plan p273-275).

NORWICH RESPONSE

The Town could pursue one or more of the following possible options:

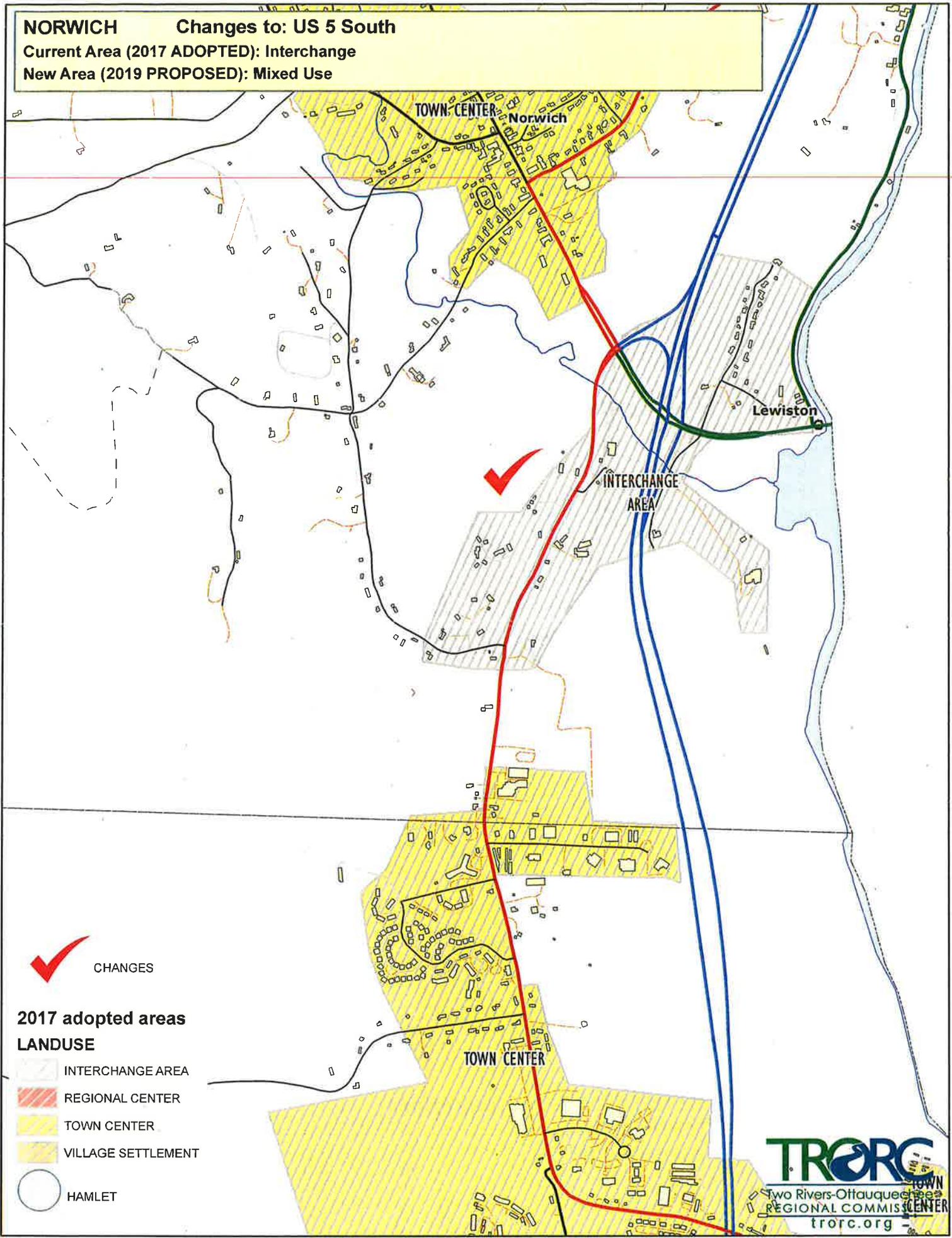
1. No comment
2. Limited comment in support of removing the interchange area in Norwich
3. Suggest more clarity about scale of residential development in rural areas
4. Suggest more flexibility around retail uses in the mixed use district when combined with residential development
5. Suggest more attention to neighborhood amenity (including retail) in mixed use development
6. Request a review of regional future land use area designations for Norwich to better reflect existing land use development patterns, and align with the Town's ongoing planning efforts

I will be available at the Selectboard meeting in June 26 to answer questions from the Board.

NORWICH Changes to: US 5 South

Current Area (2017 ADOPTED): Interchange

New Area (2019 PROPOSED): Mixed Use



CHANGES

2017 adopted areas

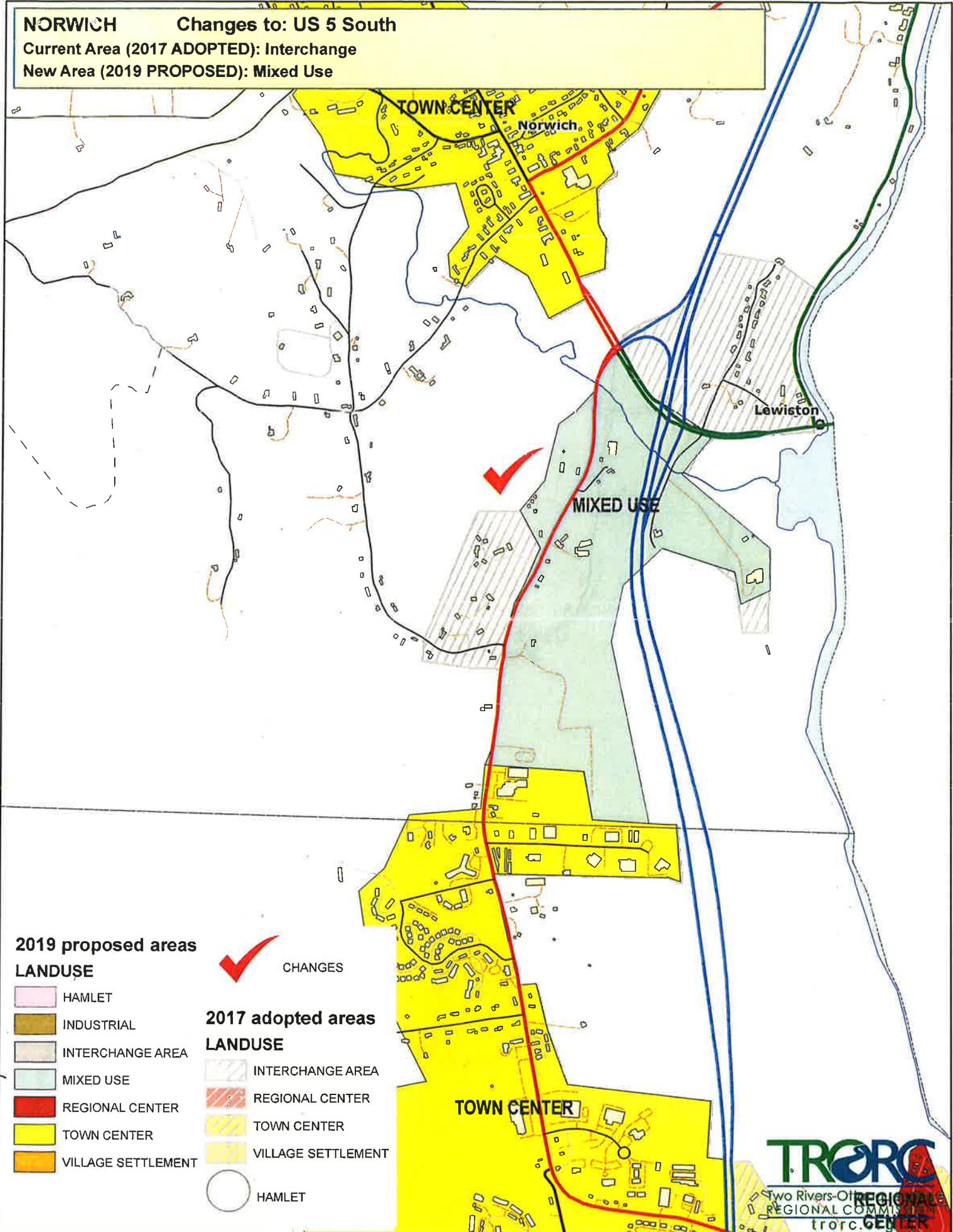
LANDUSE

-  INTERCHANGE AREA
-  REGIONAL CENTER
-  TOWN CENTER
-  VILLAGE SETTLEMENT
-  HAMLET

NORWICH Changes to: US 5 South

Current Area (2017 ADOPTED): Interchange

New Area (2019 PROPOSED): Mixed Use



2019 proposed areas

LANDUSE

-  HAMLET
-  INDUSTRIAL
-  INTERCHANGE AREA
-  MIXED USE
-  REGIONAL CENTER
-  TOWN CENTER
-  VILLAGE SETTLEMENT



2017 adopted areas

LANDUSE

-  INTERCHANGE AREA
-  REGIONAL CENTER
-  TOWN CENTER
-  VILLAGE SETTLEMENT
-  HAMLET

8

4. ~~New buildings should maximize allowable density be clustered to encourage open space in surrounding areas.~~ Where unusual natural features, soil limitations, or special resources (including high value agriculture land) are identified, use of cluster development concepts is encouraged to protect such resources from unnecessary development.
5. ~~Where adjacent subdivisions are planned, they must provide for common open space systems to link active recreation areas such as playgrounds, playfields, and natural areas.~~
6. ~~Design features which contribute to the aesthetic value of residential and non-residential areas, including the provision of open spaces, trees and natural ground cover, and the conservation of stream valleys, historic landmarks and structures must be utilized in planning subdivisions.~~
7. Existing postal facilities, and similar governmental offices, ~~should~~ must be retained in Hamlet Areas and not be relocated into Rural Areas.

INSERT RURAL AND FOREST BASED AREA LANGUAGE PLACEHOLDER

Industrial Areas

Background

Industrial parks and districts are a way to encourage economic growth and high-wage businesses to locate in the Region without adversely affecting neighboring land uses. Industrial uses can produce off-site impacts, such as noise, that can be mitigated if these businesses are located in areas designated specifically for industrial development and job growth. Commonly, Industrial Areas are located where there is direct access to transportation via major roads and/or rail, three-phase power, and other municipal infrastructure. These areas may include other commercial uses, provided that those uses are not more appropriate within Regional Growth Areas. There are Industrial Areas identified in seven communities in the TRO Region.

Industrial Area Policies

1. Industrial development and uses are the primary use within an Industrial Area, provided that the scale and intensity of the development does not have an undue adverse impact on the surrounding area.
2. In addition to industrial development, commercial development (excluding principal retail establishments), services, and offices may be appropriate, provided these are not the dominant uses.
3. Traffic and pedestrian safety must be a strong consideration in the design of development within Industrial Areas, particularly those areas with a large trucking component.

- 1 4. Principal retail establishments shall not be located in Industrial Areas, but secondary
2 retail may be.

5 **Mixed-Use Areas**

6 **Background**

7
8 Given the regional need for increased housing and local needs for commercial establishments
9 that are not best suited to core areas due to their impacts, low value, or large use of land, a
10 Mixed-Use Area can supply needed space for such along state highways without creating sprawl.

11 12 **Mixed-Use Area Policies**

- 14 1. Light industrial development may be appropriate, provided that the scale and intensity of
15 the development does not have an undue adverse impact on the surrounding area.
- 16 2. Multi-family housing at several units per acre is appropriate in this area.
- 17 3. Commercial uses that include land intensive uses, lumberyards, repair services,
18 warehouses, kennels, and indoor recreation are appropriate in this area.
- 19 4. Principal retail shall not be permitted in this area.

21 **Interchange Areas**

22 **Background**

23 Lands that are in close proximity to interstate interchanges are viewed as prime areas for
24 development by some due principally to their ease of public access and favorable site conditions.
25 In this Region, interstate interchanges are located in the towns of Bradford, Fairlee, Hartford,
26 Hartland, Newbury, Norwich, Sharon, Randolph, Royalton, and Thetford. However, not all of
27 these interchanges are designated as Interchange Areas as land use areas in this Plan.

28
29 Despite the benefits of interstate travel and the fact that the interchanges are important transfer
30 points for traffic entering and exiting the Region, there are potential pitfalls to developing these
31 areas. Increased traffic congestion and safety issues resulting from interchange developments
32 can unacceptably decrease the level of service of roadways. ~~to points below acceptable levels.~~
33 One example, the Quechee interchange (I-89, Exit 1), contains acres of developable land located
34 within a mile of the intersection of two interstate highways. This places this interchange at a high
35 degree of vulnerability. Local development decisions made without adequate regard to
36 preserving mobility will degrade the functionality of the public investments. An illustration of
37 this consequence is on Interstate 89 at Exit 20, a strip of commercial development in nearby
38 West Lebanon, NH, where access on and off the interstate for traveler services has been
39 negatively impacted due to traffic and over development. ~~whose functionality has been degraded.~~

1 Other typical problems associated with improper traffic management and development at
2 interchanges include:

- 3
- 4 1. The creation of numerous curb cuts (new driveways) surrounding the interchange to
5 access new development that are permitted incrementally on a case-by-case basis without
6 due regard to an overall plan for the area;
- 7 2. The eventual existence of high traffic generators in the immediate vicinity, which cause
8 degradation of roadway intersections, the need for signalization, lower travel speeds, and
9 extensive queuing of vehicles;
- 10 3. Inadequate planning for pedestrian accesses between developments and loss of significant
11 farm land or access to such land;
- 12 4. Erosion of cultural, social, and economic values of the traditional town center or village
13 settlement due to a dislocation or redistribution of key uses into the area; and
- 14 5. Fragmentation of land parcels in such a manner as to preclude future access or interior
15 roads to properties more removed from the right-of-way; and
- 16 6. Unnecessary loss of scenic qualities resulting from insensitive land development.

17
18 ~~The Regional Commission recognizes that areas in close proximity to its thirteen interchanges on~~
19 ~~Interstates I-89 and I-91 are prime areas for development due principally to their ease of public~~
20 ~~access and favorable site conditions. The Regional Commission acknowledges that these areas~~
21 ~~are important transfer points for traffic entering and exiting the region. The benefits of Interstate~~
22 ~~travel are well documented. However, in many areas in the Northeast, particularly in more~~
23 ~~developed areas, the lack of planning for development at interchanges has prompted various~~
24 ~~forms and types of undesirable development along roads immediate to the interchange.~~
25 ~~Therefore, The level of detail given to planning for interstate interchanges represents a response~~
26 ~~to the market's interest in these areas for development, not the Regional Commission desire to~~
27 ~~see development directed there.~~

28
29 ~~In this Region, interstate interchanges are located in the towns of Bradford, Fairlee, Hartford,~~
30 ~~Hartland, Newbury, Norwich, Sharon, Randolph, Royalton, and Thetford; however, not all of~~
31 ~~these interchanges are designated as Interchange Areas in this Plan.~~

32 ~~Interchanges~~ Lands at interchanges in Bradford, Fairlee, Newbury, Norwich, Sharon, and
33 Hartford (White River Junction) are considered part of an existing Regional Center, Mixed Use
34 Area or Village Settlement and are therefore not identified as separate Interchange Areas ~~land~~
35 ~~use areas~~ in this Plan. Lands at interchanges in Bradford, Newbury and Royalton (in part) are
36 located within Industrial Areas. Lands at interchanges in Thetford and Hartland are in Rural
37 Areas. The interchanges in ~~Bradford, Newbury, Norwich, Quechee, Randolph, and Royalton, and~~
38 ~~Thetford~~ are physically separate from a ~~Regional Center, Town Center or Village Settlement~~
39 Regional Growth Area, being in some cases two or three miles away. Because ~~land use policy~~
40 this Plan and state planning policy affirm Regional Growth Areas as the principal areas for
41 service, retail, civic, and institutional uses, it is in the interest of the Region for these areas to
42 continue to serve these vital functions. Conversely, Interchange Area development, with its

1 different focus, should not be promoted to the detriment of Regional Growth Areas or the public
2 investments made therein.

3
4 TRORC respects the right of municipalities to plan for growth in these areas. At the same time,
5 ~~the Regional Commission~~ TRORC believes that given the considerable public investment in the
6 interstate highway system and Regional Growth Areas, and the significant public exposure to
7 such areas, these interchanges also need to be evaluated from a regional perspective. Land
8 around interchanges and along highways leading to them are powerful magnets for non-
9 residential uses, this often competes with and erodes Regional Growth Areas; the proximity of
10 large parking lots adjacent to high-volume highways is an attractive force to consumers and
11 businesses.

12 **Interchange Area Policies – General**

13 The following policies apply to all designated Interchange Areas:

- 14 1. Land use activities and public or quasi-public investments planned for Interchange Areas
15 that have the effect of eroding the socioeconomic vitality of downtowns are incompatible
16 with this Plan. ~~High priority should be given to public investments benefiting~~
17 ~~infrastructure, housing, and transportation facilities within Designated Downtowns,~~
18 ~~Designated Village Centers, Designated Growth Centers, and other Regional Growth~~
19 ~~Areas.~~
- 20 2. Land uses planned for Interchange Areas ~~should~~ must be of a type, scale, and design that
21 complement rather than compete with uses that exist in ~~Designated Downtowns,~~
22 ~~Designated Village Centers, Designated Growth Centers, and other Regional Growth~~
23 ~~Areas~~ Regional Growth Areas. Unless otherwise noted in the following Interchange
24 Specific Policies, appropriate uses include residential, highway-oriented lodging and
25 service facilities, trucking terminals, light industrial, offices, truck-dependent
26 manufacturing, and park-and-ride commuter lots. No use should impose a burden on the
27 financial capacity of a town or the state to accommodate the growth caused by the
28 project.
- 29 3. Development planned for Interchange Areas development must be constructed to:
 - 30 i. Complement the design principles and standards reflected in this Plan;
 - 31 ii. Promote the most appropriate land uses as determined through a locally sponsored
32 planning process involving affected landowners, municipalities, and ~~the Regional~~
33 ~~Commission~~ TRORC;
 - 34 iii. Minimize visual impacts from roadways through screening and landscaping and
35 maintain a high standard of scenic amenities for visually sensitive areas with due
36 regard to impacts on neighboring land uses and highway users; and
 - 37 iv. ~~Discourage creation or establishment of uses deemed more appropriate to~~
38 ~~Regional Growth Areas; and~~
 - 39 v. Encourage planned unit developments.
- 40 4. Master plans for each Interchange Area should be completed. Support the development of
41 ~~Master Plans for each of the Interchange Areas.~~ Such Plans should be conducted locally

1 as part of each local planning commission’s ongoing planning program in cooperation
2 with landowners, ~~the Regional Commission~~ TRORC, and other affected parties. Work
3 should focus on creating an integrated site plan and design plan that serves as a means of
4 addressing the potential conflicts or problems noted above. Elements that the Plan should
5 include are:

- 6 i. Access management controls;
- 7 ii. Pedestrian amenities;
- 8 iii. Transit access;
- 9 iv. Parking;
- 10 v. Energy efficiency;
- 11 vi. Utilities/public services;
- 12 vii. Outdoor lighting standards;
- 13 viii. Landscaping and screening;
- 14 ix. Signage; and
- 15 x. Open space conservation.

16 5. Master Plans ~~should~~ must serve as the foundation for the identification of the highest and
17 best use of these areas and should provide a framework for future development.
18 Incremental and uncoordinated development inconsistent with Master Plans for each of
19 the Interchange Areas is discouraged.

20 6. Development concepts that ~~should~~ must be utilized for Interchange Areas include:

- 21 i. A circulation system that is conducive to pedestrian, bicycle, and other non-
22 vehicular travel modes;
- 23 ii. A density or lot coverage area that is higher than surrounding rural settlement
24 areas;
- 25 iii. Use of planned unit development concepts, such as compact development that is
26 offset by open space;
- 27 iv. A design that incorporates public spaces and promotes social interactions;
- 28 v. A mixture of uses including non-residential and community facilities, and
29 possibly residential;
- 30 vi. Central focal points or public spaces serving the entire area;
- 31 vii. A pattern and scale of development that complements traditional patterns and uses
32 in Regional Growth Areas; and
- 33 viii. Provision for park-and-ride commuter parking lots, transit access, and travel
34 information services.

35 7. ~~Any new development at or near Interchange Areas should promote a nodal development~~
36 ~~pattern where buildings are clustered, off street parking screened in the rear of the parcel~~
37 ~~and inter connected to adjoining parcels where practical.~~

- 1 8. Municipalities with Interchange Areas are encouraged to promote creation and adoption
2 of an Official Map, per 24 VSA §4421, to provide a legal means of creating an
3 interconnected network of streets, walkways, and other public facilities or amenities on
4 land designated as interchange development areas. Concepts employed in Master Plans
5 and the Official Map should employ traditional streetscape patterns and designs deemed
6 compatible with existing Regional Growth Areas.
- 7 9. Principal retail establishments must be located in Regional Growth Areas to minimize the
8 blighting effects of sprawl and strip development along major highways, to protect the
9 vitality of our villages and downtowns, and to maintain rural character.

10

11 **Interchange Area Policies — Specific**

12 ~~The Regional Commission recognizes that the~~ The characteristics of each of the ~~thirteen~~ three
13 Interchange Areas designated in this Plan are not identical. While all of the Interchange Areas
14 serve as transfer points between the interstate (limited-access roads) and state highways
15 (connectors to villages and outlying countryside), the physical and economic landscapes for
16 these areas is different. Some areas are largely undeveloped open spaces without public
17 infrastructure, especially sewer or water. Other areas are situated at or near prominent vistas or
18 scenic areas and are visually sensitive to certain types of development. Yet other interchanges
19 are experiencing new commercial or industrial development on what is or was farmland. Some
20 interchanges are relatively flat and have greater potential to accommodate appropriate
21 development ~~compared to~~ than others that are steep or have other physical development
22 constraints such as aquifers and wetlands. Lastly, local community planning desires and attitudes
23 suggest that not all land use goals and policies should be universally applied.

24

25 It is the finding of TRORC that in order for this Plan to address each Interchange Area
26 specifically, supplemental ~~goals and~~ policies have been developed for each of these interstate
27 interchanges ~~except for those in Bradford, Fairlee, Hartford (Wilder), Hartford (White River~~
28 ~~Junction), and Sharon because they are part of existing Regional Growth Areas; and Hartland~~
29 ~~and Thetford since they are most appropriate in Rural Areas. Village Settlements with few~~
30 ~~opportunities for significant development. The policies in each Interchange Area section below~~
31 apply specifically to that interchange indicated.

32 **Bradford Interchange (I-91, Exit 16)**

33 ~~Exit 16 on Interstate 91 accesses Route 25. Route 25 runs in an east/west direction commencing~~
34 ~~in Topsham at the junction of Route 302 and extending to Bradford and the Connecticut River.~~
35 ~~Route 25 serves as a major connector road across east central Vermont to New Hampshire. This~~
36 ~~two lane road supports mainly local and regional traffic in Washington and Orange Counties.~~
37 ~~Bradford serves as an economic hub for the area. The primary access points for services and~~
38 ~~shopping are via Routes 5 and 25.~~

39

40 ~~The area adjacent to the interchange consists of a mixture of land uses. Topography, soil~~
41 ~~conditions, and market forces have heavily influenced land uses. Low lying areas adjacent to the~~
42 ~~Waits and Connecticut Rivers are subject to flooding. This has prevented structural development~~

1 from occurring there. Much of the Area The Appleton dairy farm continues to operate here, and
2 the Carson farm was purchased by the Upper Valley Land Trust consists of primary agricultural
3 soils and still is in agricultural use. A portion of land on the northwest quadrant adjacent to
4 Route 25 near the interchange serves as a public water source for Bradford Village and
5 immediate area. Hydro geological studies commissioned by the Bradford Water Commission
6 have identified source protection areas near the interchange. Three management zones have
7 been mapped with recommended land uses suggested to secure protection of this water supply.
8

9 Following the construction of the interchange in the early 1970s, land uses have changed. Some
10 Agricultural and residential uses have been slowly transformed into commercial and industrial
11 uses. This included development of the Pierson Industrial Park, the former Upper Valley Press
12 building (now the Bradford Community Center), an auto parts retail store, a pharmacy, a
13 supermarket, a gas station/restaurant, and rental storage buildings. Land on the Lower Plain, east
14 of Route 5 and south of the Village, was donated to the town by the Carson family for the
15 Bradford Fire Station.

16 Presently public sewer is not available to the Lower Plain. For several years, the Town,
17 landowners, and Bradford Community Development Corporation have considered extending the
18 sewer system to this area of town. Members of the community have argued that extending sewer
19 services to the Lower Plain could provide an opportunity to intensely develop the area and to
20 give Bradford needed space for community services and industry. Local discussions on
21 extending sewer service continue.
22

23 The land located in the immediate area of the interchange should be left in an undeveloped state.
24 Development should be directed to the east, in and around the intersection of Routes 5 and 25,
25 taking opportunities to make use of the available land that is located between and behind the
26 existing development, creating a compact core that allows other lands to remain open.
27

28 **Hartland Interchange (I-91, Exit 9)**

29 Exit 9 on Interstate 91 provides access to U.S. Route 5 and contains four quadrants of open land
30 that is relatively free of natural constraints. There is no public sewer or water available at this
31 interchange. Land conservation has taken place, or is planned, in three of the four quadrants. A
32 Vermont Community Development Program grant was awarded to the Town of Hartland in
33 November of 2000 to develop a master plan for the interchange following the denial of an Act
34 250 permit for a convenience store and gas station in the northeast quadrant. In 2001, the Upper
35 Valley Land Trust purchased 29 acres of land in the southeast quadrant and sold it back to the
36 town for a dollar. There is an official VTrans commuter parking lot located along U.S. Route 5
37 in this quadrant and a wetland that begins east of it, running south toward the town line and
38 meeting a brook halfway there as they drain into the Connecticut River. A significant amount of
39 land at and around the interchange has been permanently conserved, but portions of the
40 interchange remain developable. There is developable land located south of the conserved
41 property, along the northbound lanes, that could be accessed by Route 5; the land is located in
42 the towns of Hartland and Windsor.
43

44 The Upper Valley Land Trust has received funding to purchase the development and excavation
45 rights of a seventy six acre parcel of land in the southwest quadrant that borders on Route 5 and

1 Rice Road. Further south in this quadrant, straddling the Hartland/Windsor town line, 285 acres
2 of highly visible, forested hillside will be conserved with funding received from the National
3 Scenic Byways Program. The grant was submitted to conserve this area along the Connecticut
4 River National Scenic Byway. This parcel is significant because of its scenic attributes and the
5 habitats that are present along the Bashan Brook and aided by the wildlife culvert that provides
6 passage under the interstate.

7
8 The northwest quadrant contains open, developable land along Route 5. The rest of the quadrant
9 contains a few areas of slope of twenty percent or greater, and the Lulls Brook converges with
10 another brook and flows under Interstate 91, toward the Connecticut River.

11
12 The town of Hartland is not a regional economic center. Limitations include a lack of municipal
13 utilities and the town's desire to maintain a rural character. In many ways, Hartland will continue
14 to function as a bedroom community to the Upper Valley. This Interchange Area has not been
15 identified for significant development, although small scale development that is consistent with
16 the area may occur. large capital intensive businesses. Such land use activities would be
17 inconsistent with this Plan and the Hartland Town Plan. There are other nearby areas that could
18 provide space for small and moderately sized businesses, including Hartland Four Corners,
19 Hartland Three Corners, and North Hartland.

20
21 According to the Hartland Town Plan's Future Land Use map, the northeast quadrant has been
22 designated for commercial development while the other three quadrants are classified as rural.
23 As stated in the Town Plan, "Much of the land surrounding the interchange remains undeveloped
24 pasture that serves as an important scenic resource, providing visual contrast to the highways.
25 This variety is one important component of the rural business area character that sets it apart
26 from, and makes it more visually interesting and pleasant than, the commercial strip
27 development that exists at many other interchanges." TRORC concurs.

28
29 The interchange is one mile from the existing Hartland Three Corners commercial district. In
30 accordance with Vermont's so-called "Downtown Initiative", town officials should encourage
31 businesses to locate in existing business or village centers. To accomplish this, strip type
32 development should be discouraged within and outside these areas." Efforts to conserve land in
33 the three quadrants designated as rural by the Hartland Town Plan should be continued.
34 Commercial development in the northeast quadrant should be compact, sited and designed to
35 retain the scenic nature of the landscape. Retail Principal retail development, gas stations, fast-
36 food restaurants, motels, and other full-scale highway oriented services are inconsistent with the
37 goals and policies of the Regional Plan for this Interchange Area due to the close proximity of
38 the Hartland Three Corners village settlement area and its traveler oriented services.
39 Development should not contribute to strip type development that by its nature attracts similar
40 businesses (this includes but is not limited to factory outlets, large grocery stores, shopping
41 malls, convenience stores, large chain retail stores, and fast food establishments).

42 43 Hartland Interchange Policies

- 44
45 1. The Conservation of land in the three quadrants designated as rural by the Hartland Town
46 Plan should be continued.

- 1 2. ~~The types of land development appropriate for this interchange include offices, light~~
2 ~~industrial, residential, and other similar uses that are not intended to draw on regional~~
3 ~~populations.~~
- 4 3. ~~Principal retail establishments must be located in Regional Growth Areas to minimize the~~
5 ~~blighting effects of sprawl and strip development along major highways, to protect the~~
6 ~~vitality of our villages and downtowns, and to maintain rural character.~~

8 **Norwich Interchange (I-91, Exit 13)**

9 ~~Exit 13 off Interstate 91 provides access to Main Street and U.S. Route 5 in Norwich and Route~~
10 ~~10 in Hanover, New Hampshire. There is no municipal sewer available at this interchange,~~
11 ~~although the Hanover, New Hampshire sewer system is located across the river; municipal water~~
12 ~~is available at the interchange. Three zoning districts are present at the interchange: Village~~
13 ~~Residential, Rural Residential, and Commercial/Industrial.~~

14 ~~The Connecticut River (state border), Ledyard Bridge, and the area known as Lewiston are~~
15 ~~located east of the interstate. The northeast quadrant contains residential development, slopes~~
16 ~~twenty percent or greater, an active rail line, and the historically industrial area of Lewiston. The~~
17 ~~southeast quadrant contains a wetland, surface water, an electrical substation, transmission lines,~~
18 ~~slopes of twenty percent or greater, rail, and The Montshire Museum, Vermont's Museum of~~
19 ~~Science.~~

20 ~~The northwest quadrant contains conserved land, slopes of twenty percent or greater, and~~
21 ~~residential and public uses. Four smaller brooks drain into the Bragg Brook and head toward the~~
22 ~~Connecticut River throughout the southwest quadrant. Land to the west of U.S. Route 5 has~~
23 ~~been conserved, but there are still areas of unconstrained land located west of Route 5, stretching~~
24 ~~east to the southbound lanes of the Interstate. A housing development of five units has been~~
25 ~~permitted for construction in this quadrant but possible conservation of the land is presently~~
26 ~~being considered.~~

27 **Quechee (Hartford) Interchange (I-89, Exit 1)**

28 Exit 1 of Interstate 89 accesses U.S. Route 4 and connects travelers and commerce west to
29 Woodstock, Killington, Rutland, and beyond, and east to White River Junction and Interstate 91.
30 Route 4 is one of the few east/west highways spanning the narrower width of the state and
31 therefore carries steady volumes of traffic. This interchange is located ~~a mile and a half~~ 1.5
32 miles from municipal sewer and water service; the residential wastewater system located to the
33 west in Quechee is a shared leachfield system. The on- and off-ramps for the northbound and
34 southbound lanes are located ~~a half mile~~ 0.5 miles apart. There are two different scenarios
35 present at either end, with the northbound interchange leaving few opportunities for development
36 due to the close proximity of 30-percent slopes and the interstate.

37
38 The southbound interchange is a sprawling commercial area with access roads intersecting the
39 on- and off-ramps. ~~There is a proposal to develop a portion of the 135-acre parcel behind the~~
40 ~~commercial enterprises on the west side of Route 4. This land is zoned as Quechee Interstate~~

1 ~~Interchange (QH) and Rural Lands 5 (RL5) in the Town of Hartford's zoning regulation.~~
2 ~~Development around the southbound interchange must be planned based around access points~~
3 ~~that do not degrade the functionality of U.S. Route 4 or the I-89 on- and off-ramps. Stagecoach~~
4 ~~Road intersects the southbound ramps a tenth of a mile from the intersection with U.S. Route 4;~~
5 ~~it provides access to the open and undeveloped land west of the interstate. Intensive~~
6 ~~development that increases traffic volumes must not be permitted on the open lands accessed by~~
7 ~~Stagecoach Road; it would degrade the operation and safety of the Interstate and U.S. Route 4.~~

8
9 White River Junction—the Regional Center, a Vermont Designated Downtown, and a
10 Designated Growth Center—is located 3.5 miles to the east. Development at this interchange
11 should be of a type that does not displace the development and investment that has occurred in
12 the Regional Center or in Quechee Village. In order to mitigate against the impacts of strip
13 development and sprawl, and to ensure the vitality of Hartford's Regional Center, Town Center,
14 Village Center, and Hamlet Area, this interchange is not an appropriate location for a growth
15 center-principal retail establishments.

16 **Quechee Interchange Policies**

- 17
18
19 1. Intensive development that increases traffic volumes must not be permitted on the open
20 lands accessed by Stagecoach Road; it would degrade the operation and safety of the
21 interstate 89 and U.S. Route 4.
- 22 2. Development around the southbound interchange must be planned based around access
23 points that do not degrade the functionality of U.S. Route 4 or the I-89 on- and off-ramps.
- 24 3. The types of land development appropriate for this interchange include offices, light
25 industrial, residential, appropriately scaled traveler-oriented uses, and other similar uses
26 that are not intended to draw on regional populations.
- 27 4. Principal retail establishments must be located in Regional Growth Areas to minimize the
28 blighting effects of sprawl and strip development along major highways, to protect the
29 vitality of our villages and downtowns, and to maintain rural character.

30 31 **Randolph Interchange (I-89, Exit 4)**

32 The Exit 4 interchange on Interstate 89 is located in Randolph, 3 miles from the revitalized
33 historic downtown and commercial district and 1 mile from historic Randolph Center, home of
34 Vermont Technical College (VTC). Exit 4 accesses Route 66, a two-lane connector road that
35 runs in an east/west direction between the Village of Randolph, Randolph Center, East
36 Randolph, and Route 14. This area is predominately open land, including farmland and
37 woodland. The interchange area is particularly well-known for panoramic and distant scenic
38 vistas, particularly the mountain views to the west. There are several structures at the
39 interchange, including a gas station and convenience store, a fast-food restaurant, professional
40 offices, an auto service repair garage, a state highway facility, an industrial/office complex, and
41 several single-family residences.
42

1 Presently there is no existing municipal water supply provided to the area, although there are
2 water supply systems on the western edge of the area (Fish Hill) and eastern edge near VTC. An
3 existing sewer line passes through the area and conveys wastewater from VTC down Route 66 to
4 the municipal treatment facility. Annual average daily traffic (AADT) on Route 66 is estimated
5 to increase with or without new development in the area.

6
7 Since 1998 the Town of Randolph has explored opportunities for development at the Exit 4
8 Interchange. The Randolph Town Plan reflects many of these efforts, dividing the Interchange
9 Area into four quadrants and incorporating design and use standards for each quadrant into its
10 land use regulations. In 1998, the Town of Randolph received a planning grant from the State of
11 Vermont to explore opportunities for development at the Exit 4 Interchange. A final conceptual
12 master plan entitled *Exit 4 Engineering and Development Analysis* was prepared by Dubois and
13 King, Inc. and the Cavendish Partnership in January 1999. Future land use scenarios were
14 developed for each quadrant of the interchange area. The details of this assessment are outlined
15 in the report. Key components include the following:

- 16 1. Provide space for the development of business parks with design guidelines to protect
17 scenic values;
- 18 2. Provide open space for the conservation of wetlands, streams, steep slopes, other natural
19 resources, and visual quality;
- 20 3. Limit or deny new curb cuts to maintain the carrying capacity of Route 66;
- 21 4. Provide space and opportunities for transitional/senior housing;
- 22 5. Provide for an improved park-and-ride commuter lot/Welcome Center; and
- 23 6. Consider land for an agricultural/cultural museum perhaps to be affiliated with other
24 uses.

25 ~~Four years later, the Exit 4 Advisory Committee again studied development scenarios at the~~
26 ~~interchange, this time using 3-D visualization software, and the conclusions reinforced the 1999~~
27 ~~findings concerning the important views at each of the four quadrants of the interchange.~~
28

29 ~~Other key findings were included in the report. More comprehensive design review standards to~~
30 ~~be included under the Randolph Zoning Regulations were recommended. Prior to proceeding~~
31 ~~with any major development, the report recommended that design standards first be in place to~~
32 ~~evaluate development proposals. Selected or preferred development areas were noted during the~~
33 ~~planning process and mapped. Exclusion or avoidance areas were determined to be sensitive due~~
34 ~~to distinct area of environmental limitations or high visual resource values. Further, the~~
35 ~~extensive study conducted by the community over the past 18 years determined that retail~~
36 ~~development at the interchange was concluded as unsuitable for a combination of reasons,~~
37 ~~including traffic impacts on Route 66, visual sensitivity, and conflicts with downtown~~
38 ~~businesses. Moreover, standalone retail development at any scale or size was found to be~~
39 ~~incompatible with the community's values. However, there was one exception. Accessory uses~~
40 ~~of a retail nature for the business park, the agricultural museum, or similar planned uses were~~
41 ~~found acceptable.~~
42

1 ~~In 2000 the Vermont Agency of Transportation (VTrans) commenced a planning study to~~
2 ~~develop long term solutions to a park and ride facility at the interchange area. The need for a~~
3 ~~new or upgraded park and ride lot has been documented. Local meetings have been held in~~
4 ~~Randolph to gauge support. As a result of the meetings, an expansion and redesign of the~~
5 ~~existing site, in the northeast quadrant of the interchange, was preferred. The Vermont Agency~~
6 ~~of Transportation has obtained all permits and anticipates construction in 2007.~~

7
8 ~~This Plan supports and endorses the efforts of the community to undertake further refinement of~~
9 ~~the planning concepts referenced in the Exit 4 planning reports. These include plans for a new~~
10 ~~park and ride facility, the possible creation of an adjunct agricultural museum, and~~
11 ~~office/industrial parks. Additionally, this Plan discourages large scale retail development of the~~
12 ~~interchange including shopping centers, malls, auto dealerships, and big box stores.~~
13 ~~Small scale retail uses subordinate to primary uses and non traditional to downtown Randolph or~~
14 ~~other designated areas may be acceptable uses subject to in depth review and evaluation. The~~
15 ~~reuse of the former DuBois & King offices by the Vermont Resources Center and Incubator~~
16 ~~illustrates the goals of this Plan.~~

17
18 ~~Any project planned for the interchange should employ high design and construction standards~~
19 ~~and not unduly impair the scenic resources of the area. New development should be sited in areas~~
20 ~~that are not highly scenic, visible, or environmentally sensitive. The Randolph Planning~~
21 ~~Commission is currently preparing amendments to the town's zoning regulations, including~~
22 ~~design review standards and a density overlay, to protect scenic and natural resources. Future~~
23 ~~development at the interchange that requires improvements to Route 66, including traffic signals~~
24 ~~and turning lanes, needs to be carefully evaluated. These should only be authorized where it is~~
25 ~~determined such a public investment will not unreasonably endanger or interfere with the~~
26 ~~function, efficiency, safety, or use of this route. New development should coordinate with~~
27 ~~existing development on shared access or retrofit access point locations to improve safety.~~

28 29 **Randolph Interchange Policies**

- 30
31 1. ~~The development of large-scale retail development of the at the Randolph interchange—~~
32 ~~including shopping centers, malls, auto dealerships, and big-box stores—is inconsistent~~
33 ~~with this Plan.~~
- 34 2. Small-scale retail uses secondary or subordinate to primary uses and non-traditional to
35 downtown Randolph or its village areas may be acceptable uses subject to in-depth
36 review and evaluation by the community.
- 37 3. Any project planned for the interchange should must employ high design and
38 construction standards that will ensure that development does and not unduly impair the
39 scenic resources of the area.
- 40 4. New development should be sited in areas that are not highly scenic, visible, or
41 environmentally sensitive.
- 42 5. Future development at the interchange that requires improvements to Route 66, including
43 traffic signals, ~~and~~ turning lanes, or roundabouts, needs to must be carefully evaluated.
44 These should only be authorized where it is determined such a ~~public~~ privately funded

1 investment will not unreasonably endanger or interfere with the function, efficiency,
2 safety, or use of this route.

3 6. New development ~~should~~ must coordinate with existing development on shared access or
4 retrofit access point locations to improve safety.

5 7. The types of land development appropriate for this interchange include offices, light
6 industrial, residential, appropriately scaled traveler-oriented uses, and other similar uses
7 that are not intended to draw on regional populations.

8 8. Principal retail establishments must be located in Regional Growth Areas to minimize the
9 blighting effects of sprawl and strip development along major highways, to protect the
10 vitality of our villages and downtowns, and to maintain rural character.

12 **Royalton Interchange (I-89, Exit 3)**

13 Exit 3 on Interstate 89 in Royalton accesses Route 107, which runs in an east/west direction,
14 connecting to Bethel and Stockbridge and Routes 100 and 14. Route 107 is classified as a minor
15 arterial road. It is a heavily traveled road and forms part of a major transportation corridor
16 between I-89 and Rutland and points west. Forecasts reveal that traffic volume will continue to
17 grow over the next 20 years.

18
19 Following the completion of I-89 35 years ago, several parcels of land near the interchange area
20 have been developed. Primarily these changes in land use have been from rural residential and
21 agricultural uses to industrial or commercial uses, but still much of the area remains
22 undeveloped, consisting of farm and forestland. Several areas contribute to highly scenic vistas,
23 particularly from I-89 and Route 107. Due to its prominent location, ~~pressures for it is likely that~~
24 new development at Exit 3 will continue. Solid transportation planning, coupled with sound land
25 use planning principles, can minimize land use and traffic conflicts that have plagued many other
26 Interchange Areas.

27
28 In 1999, the Town of Royalton conducted an extensive planning project in which the was
29 awarded a grant from the State of Vermont to develop a community vision and policy for the
30 future growth of this area. The Royalton Planning Commission's goal was not to prohibit
31 growth in the area, but to be more specific about how change should be directed. Informational
32 meetings were held to update the public on the study and to get ideas on what the Regional
33 Commission should focus on for future planning in this area. Following this, the Royalton
34 Planning Commission found the following values to be important to the area:

- 35 1. Provide space for future business growth, but only when it doesn't detract from
36 Royalton's two villages;
- 37 2. Promote new development when plans are carefully laid out for safe access onto Routes
38 14 and 107;
- 39 3. Protect sensitive resource and scenic areas and encourage good design for new projects;
40 and
- 41 4. Preserve the carrying capacity of Route 107 as a minor arterial road.

1 Given these values and an analysis of development suitability, nine future land use designations
2 were recommended and depicted on a map. These included areas for industry, service and office
3 type uses, residences, agriculture, and limited development. Goals and recommendations were
4 listed to help guide the community on the highest and best uses for each sub-area. TRORC
5 accepts the findings and conclusions contained in the *Exit 3 Planning and Development Study*
6 (September 2000), which has since been incorporated into the Royalton Town Plan, as the
7 planning policies developed by the Town of Royalton for this area and development proposals
8 should be compatible with this report. Additionally, the potential for sprawling strip development
9 along Route 107 to the west of the interchange concerns TRORC. Development to the west of
10 the interchange should be designed to minimize the impacts of strip development or sprawl,
11 create small nodes, focused around existing development, that are surrounded by open space or
12 natural areas.

13 14 **Royalton Interchange Policies** 15

- 16 1. The types of land development appropriate for this interchange include offices, light
17 industrial, residential, appropriately scaled traveler-oriented uses, and other similar uses
18 that are not intended to draw on regional populations.
- 19 2. Principal retail establishments must be located in Regional Growth Areas to minimize the
20 blighting effects of sprawl and strip development along major highways, to protect the
21 vitality of our villages and downtowns, and to maintain rural character.

22 23 **Thetford Interchange (I-91, Exit 14)**

24 Exit 14 on Interstate 91 accesses Route 113, which runs in an east/west direction connecting East
25 Thetford (Route 5) to Thetford Hill, Thetford Center, and Post Mills, and beyond to West
26 Fairlee, Vershire, and Chelsea. This Interchange Area is prominently open, with a few residential
27 structures and no commercial or industrial uses. The nearest concentration of buildings is the
28 hamlet Hamlet Area of Thetford Hill. This historic settlement has remained largely unchanged
29 since the opening of I-91 in the late 1960s. Thetford Hill has been identified as a Village
30 Residential Area in the Thetford Town Plan. The purpose of the area is to encourage the
31 development of residential centers and to serve as a nucleus for future growth of the town. The
32 Village Residential Area extends easterly from the village center to an undeveloped area near the
33 interchange. A portion of Thetford Hill has been selected to be included in a Preservation
34 Overlay District under the Town Zoning Regulations. All other land within the Interchange Area
35 is classified as Rural Residential according to the Thetford Town Plan. The purpose of this Rural
36 Residential area is “to maintain a low density rural character primarily ... of farms, residences
37 and woodlands.” This designation includes certain non residential uses, provided that the uses
38 are relatively small and fit the nature of the terrain and character of the setting.

39
40 The Town of Thetford is not a major regional economic center. Limitations include a lack of
41 municipal services, limited land suitable for industrial and commercial locations, and the town’s
42 rural character. Even with increased industrial development, Thetford will continue to function
43 primarily as a bedroom community to the Upper Valley. Historically, the town’s residents have

1 ~~been against major development at the intersection of Interstate 91 and Vermont Route 113. This~~
2 ~~Interchange Area has not been identified for large capital intensive businesses. Such land use~~
3 ~~activities would be inconsistent with this Plan and the Thetford Town Plan. There are other areas~~
4 ~~that could provide space for small and moderately sized businesses, including East Thetford,~~
5 ~~Post Mills, and Thetford Center. Historically, the town's residents have been against major~~
6 ~~development at the intersection of Interstate 91 and Vermont Route 113. Retail development,~~
7 ~~gas stations, fast food restaurants, motels, and other full scale highway oriented services are~~
8 ~~inconsistent with the goals and policies of the Regional Plan for this Area. Small office parks~~
9 ~~and research facilities are permissible land use activities, subject to meeting appropriate design~~
10 ~~guidelines set forth in this Plan.~~

11
12 ~~This Plan strongly recommends that the Town of Thetford develop a Master Plan for this~~
13 ~~interchange; land use decisions need to make efficient use of public infrastructure investments,~~
14 ~~including roads, sewer, and water systems. This planning effort should be locally directed and~~
15 ~~comprehensive enough to ensure that the goals and policies set forth in this section are~~
16 ~~thoroughly addressed. By doing this This would make evaluating the benefits and costs~~
17 ~~associated with major land developments would be more readily assured and predictable.~~

18
19 **Thetford Interchange Policies**

- 20
21 1. ~~Large scale commercial businesses at the Thetford Interchange are inconsistent with this~~
22 ~~Plan.~~
23 2. ~~Small office parks and research facilities are permissible land use activities, subject to~~
24 ~~meeting appropriate design guidelines set forth in this Plan.~~
25 3. ~~Principal retail establishments must be located in Regional Growth Areas to minimize the~~
26 ~~blighting effects of sprawl and strip development along major highways, to protect the~~
27 ~~vitality of our villages and downtowns, and to maintain rural character.~~

28
29
30 **Wells River (Newbury) Interchange (I-91, Exit 17)**

31 ~~Exit 17 (Boltonville) on Interstate 91 accesses Route 302 which runs in a east/west direction~~
32 ~~commencing in Barre and extending to Wells River and beyond into the White Mountain Region~~
33 ~~of New Hampshire and the Lakes Region of Maine. Route 302 is a state designated truck route;~~
34 ~~it is capable of handling larger trucks without state issued truck permits. The Exit 17 interchange~~
35 ~~is a well recognized stop for truckers and tourists at the P & H Truck Stop which is open twenty-~~
36 ~~four hours a day. Current traffic volumes on Route 302 are generally low and free of traffic~~
37 ~~congestion. Projections undertaken by the Regional Commission in 1999 show that traffic levels~~
38 ~~are projected to increase over the next twenty years. However, traffic congestion and the service~~
39 ~~capacity of the road and intersections should still be within acceptable ranges. What is unknown~~
40 ~~at this point is the potential impact that large scale commercial development in Woodsville, NH~~
41 ~~will have on traffic patterns.~~

1 The area around the interchange is predominantly undeveloped consisting of a mixture of open
2 and forestland. There are a few commercial uses along Route 302 mainly extending easterly
3 from the interchange toward Wells River, including a restaurant and other service uses.
4 Northeasterly of the interchange is a fully developed industrial park. Recent changes to the
5 Newbury Town Plan identify a portion of the interchange area as appropriate for mixed use
6 development, excluding principal retail. Uses at this site are at a relatively low density and are
7 not highly visible from either Route 302 or I 91. Blue Mountain High Union School is located
8 near the interchange with a direct access onto Route 302.

9
10 Along the three-mile length of the Route 302 corridor extending from Wells River Village, land
11 suitable for intense development is very limited due to topography, proximity to water resources,
12 presence of protected natural areas, access limitations, and poor soils. Opportunities for more
13 concentrated development within the Route 302 corridor becomes more prevalent in the area of
14 Wallace Hill Road and Leighton Hill Road where the land is relatively level with open
15 topography and better soils.

16
17 There are several scenic vistas available to travelers on I 91 and Route 302 at or near the
18 interchange area. Scenic resources include fields, farmsteads, forestland, historic buildings, and
19 streams. It is the policy of this Plan that future land development at the interchange be carefully
20 planned and designed to protect and enhance these valuable resources. Development that
21 detracts from the valued landscape or creates unsafe road conditions conflicts with this Plan's
22 land use goals.

23
24 All uses at the interchange are dependent on onsite sewer and water supply. There are no plans
25 to provide public water or sewer services to the interchange area by the Village or Town; the
26 costs reported in a 1990 financial study were deemed infeasible by the Village Trustees. Future
27 development of the area will be limited by the capacity of the land to provide onsite wastewater
28 disposal and water systems. Uses that require large onsite disposal capacity will have difficulties
29 obtaining wastewater disposal permits, this development is unlikely to be concentrated or dense.

30
31 Future land development should be designed and planned to fit the context, the site and the
32 surrounding area. Projects that are incompatible with the surrounding area are discouraged and
33 need to be redesigned so that they are not significant visual intrusions to travelers along I 91,
34 Route 302, and town roads in the vicinity. To ensure that individual development proposals fit
35 with the planning policies of this section, use of an expanded local site plan review process,
36 including specific design criteria, is encouraged. State grant funds may be available to help the
37 Town evaluate these options.

38 39 **Newbury Interchange Policies**

- 40
- 41 1. The types of land development appropriate for the Newbury interchange include offices,
42 light industrial, residential, appropriately scaled traveler oriented uses, and other similar
43 uses that are not intended to draw on regional populations.
 - 44 2. Principal retail establishments must be located in Regional Growth Areas to minimize the
45 blighting effects of sprawl and strip development along major highways, to protect the
46 vitality of our villages and downtowns, and maintain rural character.

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White River Junction (Hartford) Interchanges (I-91, Exits 10 and 11)

Exit 10 is the intersection of two interstate highways, I 89 and I 91. Since Exit 10 is located immediately to the south of Exit 11, these two exits will be discussed in conjunction with one another. These interchanges are both served by public sewer and water, are located in the heart of the Regional Center, and possess the most intensive development around interstate exits in the region. There are proposals to develop two parcels of open land in the immediate interchange area. The land in southeast quadrant of Exit 10 is dominated by slopes that are twenty and thirty percent or steeper but there is open land along the northbound lane of I 91 that is bordered on the south by the Kilburn Brook. This land could be accessed from the Connecticut River Road but slope and residential development separate the land from the access. Much of the land south of the Kilburn Brook is constrained by its designation as a Deer Wintering Area by the Vermont Agency of Natural Resources.

The undeveloped land in the southwest quadrant of Exit 10 is largely wooded with some twenty percent slopes present. North Hartland Road and Milisi Road could provide access to these wooded areas that are located along the southbound lane of I 91. The open land along the southbound lane of I 89 in this quadrant is constrained by surface water and wetlands located along the North Hartland Road but east of those natural constraints there is a proposal to build an aquatic fitness center. This parcel is zoned Industrial/Commercial (IC). South of Milisi Road there are four brooks that merge into one and drain under the interstate and into the Connecticut River.

The northern two quadrants of Exit 10 are also the southern two quadrants of Exit 11. Both quadrants are nearly fully developed with the exception of some wooded land in the eastern quadrant along I 89 and some open land that is also along I 89 on the east side of North Hartland Road.

The northern two quadrants of Exit 11 are either fully developed with the Veteran's Administration Hospital, residential development and commercial enterprises, or are constrained by slopes of twenty percent or more.

[CNT AREA LANGUAGE IS MOVING TO CONSERVATION AREA SECTION]



Willard Bridge, Pomfret | © John Know

Land Use Endnotes

1. See Vermont Natural Resources Board for “existing settlement” test.
2. Vermont Department of Fish and Wildlife
3. Vermont Conservation Design: Maintaining and Enhancing and Ecologically Functional Landscape, Eric Sorenson, Robert Zaino, Jens Hilke - Vermont Fish and Wildlife Department and Elizabeth Thompson -Vermont Land Trust

Goals, Policies and Recommendations: Flood Resilience

Recommendations

1. TRORC will work with towns to strengthen their Flood Hazard Bylaws in order to mitigate risks to public safety, critical infrastructure, historic structures, and municipal investments from inundation and erosion.
2. TRORC will work with VTrans on advocating for and improving the flood capabilities of state- or town-owned transportation infrastructure.
3. TRORC should continue working with the Emergency Coordinators, response agencies, and Selectboards from each town to develop mitigation plans and emergency preparedness and recovery procedures from flooding.
4. Existing homes and businesses at serious risk of flood damage should be identified and prioritized by towns in concert with the VT ANR River Management Section and TRORC for mitigation actions such as elevation/relocation or purchase and demolition.
5. To fully address flood risks, towns should add areas not designated in either FEMA's maps or in VT ANR's maps but that are flooded during a weather event to local flood regulations.
6. Watershed-level planning should be done by towns with assistance from TRORC to evaluate natural and constructed flood storage options upstream of existing areas of concentrated development that are at risk of flooding.
7. TRORC will work with VT ANR, towns, and landowners to lessen flood risk by restoring natural channel functions through berm or dam removal or intentional lowering of streambanks.
8. TRORC will work with towns to understand the impact stormwater runoff has on the region and on specific towns, and then work to address impacts from impervious surfaces through increased retention and infiltration.
9. The state should institute a permanent buyout program to continue to lessen flood risk.
10. TRORC will work with VT ANR to adjust the boundaries of river corridors in developed areas per the Vermont Flood Hazard Area and River Corridor Protection Procedure.

Goals, Policies and Recommendations: Flood Resilience

Goals

1. The citizens, property, and economy of the TRO Region and the quality of the region's rivers as natural and recreational resources are protected by using sound planning practices to address flood risks.
2. The Region is able to recover from flooding quickly and in a manner that improves flood resilience.
3. The creation of impervious surfaces and development in wetlands or upland forests is lessened, and where it does occur, is done in a manner that does not worsen flooding.

Policies

1. All new fill and construction of buildings in FEMA-mapped Special Flood Hazard Areas increases flood risk and is discouraged, and at a minimum must comply with the Association of State Floodplain Managers' No Adverse Impact policy.
2. All new buildings, other than accessory structures, in FEMA-mapped flood areas must have the lowest floor elevated or floodproofed at least one foot above base flood elevation.
3. Natural areas, non-structural outdoor recreational, and agricultural uses are the preferred land uses within river corridor areas due to the dangerous erosive nature of these areas. Commercial, industrial, and residential uses within river corridors are strongly discouraged outside of village and town centers.
4. New buildings within FEMA-mapped floodways shall be prohibited.
5. In order to lessen the conflict between roads and streams, towns and the state should consider moving or abandoning roads when there are more cost-effective solutions or other routes.
6. The state and municipalities should only rebuild/install culverts and bridges that are designed at least to VTrans' Hydraulics Manual and ANR's Stream Alteration Standards, and are encouraged to adopt road and bridge standards to the 50 or 100-year storm level for identified critical transportation routes.
7. Critical facilities such as emergency services, wastewater treatment plants, power substations, and municipal buildings shall not be built in Special Flood Hazard Areas unless floodproofed or elevated to at least 2 feet above the base flood elevation, designed to withstand erosion risk, and must have dry access above the base flood.
8. To reduce flood flows and be more protective of existing development, the current one-acre threshold in Vermont's Stormwater Management Rule should be reduced to one-half acre.
9. Rock rip-rap and retaining walls should only be used to the extent necessary and when bioengineering techniques may not be adequate to prevent significant loss of land or property.
10. Upland forests and watersheds should be maintained predominately in forest use to ensure high-quality valley streams and to ensure that flood flows are absorbed.
11. Outside of areas of existing compact development, new development must preserve vegetated riparian buffer zones that are consistent with state riparian buffer guidelines.
12. All wetlands that provide flood storage functions shall remain undeveloped or have compensatory storage constructed so as to achieve no net loss of such wetland function.
13. In the long term, restoration and enhancement of additional wetlands should be pursued in order to improve the region's flood resilience.
14. Structural development or intensive land uses shall not occur in Class I and Class II wetlands unless there is an overriding public interest.
15. The purchase of flood easements is encouraged to both reduce flood risk to structures and to support owners who leave lands open.
16. Emergency planning for flood response and recovery is encouraged.

Goals, policies, and recommendations continued on next page

centers and downtowns for their outright benefits to their community. However, it is also important to understand the stormwater runoff issues that exist and the various ways to mitigate their effects.

The Site-Specific Nature of Flooding

The risk of flooding in Vermont varies site by site, to the point that even adjacent parcels may be impacted differently in a flooding event.

Generally speaking, floodways are extremely dangerous places and the Special Flood Hazard Area and river corridors are high risk, but each site presents specific issues and a unique set of circumstances. For example, on a site only in the Special Flood Hazard Area, the risk may be solely from inundation, so the specific elevation is a major factor in flood damage. On a site in the river corridor, the risk may be due to lateral erosion, so elevation is less important than whether you are sitting on bedrock. On other sites, the risk may be from both inundation flooding and erosion. The site-specific nature of flooding complicates assessing and planning

for flood risks. It is important to understand the specific risks that are present at each site before attempting to mitigate flood damage on that site.

The late Gilbert White, considered the father of floodplain management in the United States, wrote, “Floods are ‘acts of God,’ but flood losses are largely acts of man.” By this he meant that flooding is a hazard not simply because it rains hard, but that we have put things in the way that will suffer from that rain. Historically, Vermont town and village centers were established around water power, which created the densely developed village and town centers we value. Today, the desire to maintain and continue this settlement pattern still holds true—even if the downtown or village center is vulnerable to flood risks. As such, it is important to recognize that there are trade-offs between flood risk and having compact development. Keeping these areas of compact settlement as safe from flooding as possible, given their location, may require elevation and floodproofing efforts, but will largely depend upon natural flood storage and surface runoff retention in upstream areas.

Low Impact Development (LID) refers to the process of designing and implementing practices at the site-level to control stormwater. LID attempts to replicate the pre-development conditions at a site.



Low Impact Development | Source: deeproot

the ability to adopt mechanisms that provide stricter protections than are required by the state. For more on wetlands, please see the Natural Resources chapter.

Riparian Buffers and Lands Adjacent to Streams

Naturally vegetated riparian zones (vegetated buffer strips next to surface waters) are essential for healthy and resilient river corridors. Vegetated riparian buffers provide a number of “ecosystem services” including attenuating floodwaters; providing river bank support and stabilization; reducing flood and ice damage to adjacent lands and structures; and slowing surface water runoff.

Moving outside of the riparian buffer, lands adjacent to streams also provide benefits, especially during flooding events. Once water overtops the river or stream channel, these areas help slow the velocity of the water by allowing the water to expand laterally over the land area instead of moving down the river or stream channel. Because of their tendency to flood and the consequent deposition of nutrients on the land, these areas tend to be very productive agricultural lands. They also serve to collect ice or debris during floods, helping river or stream channels to stay clear. The importance of these lands was demonstrated during the flooding caused by Tropical Storm Irene, as the White River was able to dissipate along fields between towns, helping to attenuate some of the floodwater.

Upland Forests

Upland forests are distinguished by having a nearly continuous canopy cover of 60 percent or more. They also contain many small unnamed streams that make up the headwaters of a watershed. These headwater streams are the smallest yet most abundant streams draining the state of Vermont and the TRO Region. Therefore, the activities occurring in the headwaters can impact an entire watershed.

Healthy and well-managed upland forests

reduce flooding by intercepting rainfall so that the force of rain is less erosive, increasing the infiltration and storage of rainwater into rich soils, and soaking up massive amounts of water during the growing season. The TRO Region is home to many different kinds of forested areas. For instance, the region contains some of the vast unbroken forested ridgelines of the Green Mountain National Forest, as well as several large blocks of conserved forested areas, like the Chateaugay No Town Conservation Project, which stretches across the towns of Barnard, Bridgewater, Stockbridge, and Killington. These and other forested lands not only provide ecological, scenic, and economic benefits but also help mitigate flood damage.

Stormwater and Impervious Surfaces

Impervious surfaces prevent the infiltration of water into the soil. Man-made impervious surfaces include parking lots, rooftops, roads (even gravel roads), and severely compacted soils, all of which exacerbate flooding events by increasing the amount and velocity of stormwater runoff, especially in heavy rain events. The percentage of impervious surfaces can be reduced by limiting the number of rooftops and amount of pavement, by using permeable surfacing materials, by employing disconnection practices, and by implementing Low Impact Development (LID) principles. Low Impact Development refers to the process of designing and implementing practices at the site level to minimize the creation of stormwater and to replicate conditions present before the development of an area by managing stormwater runoff the way a healthy and intact environment would—by slowing it, spreading it, and/or sinking the runoff into the ground.

While widespread impervious surfaces are detrimental to water quality, and even as little as 10 percent impervious cover in a watershed can destabilize rivers, impervious surfaces in village centers and downtowns are the desired result of dense development and are important in the fabric of the Vermont landscape. It is critical to maintain the dense development of village

Home/Property Buyouts

Following the flood damage caused by the 2011 spring flooding and Tropical Storm Irene, a number of property owners in Vermont applied for property buyouts, which were funded by FEMA's Hazard Mitigation Grant Program (HMGP) and HUD's Community Development Block Grants for Disaster Recovery (CDBG-DR) administered through TRORC. Roughly 70 properties in the TRO Region, and 150 in the state, were involved in the buyout process. The towns in our region with buyout properties include Bethel, Braintree, Bridgewater, Granville, Hartford, Pittsfield, Plymouth, Rochester, Royalton, Sharon, and Stockbridge. Most of these towns are located on the White River and its tributaries. Buyouts are an effective way to reduce a community's vulnerability to flooding and therefore improve the community's overall resilience to flooding. Homes are no longer potential objects that will wash downriver and clog a bridge, and buyout sites (once cleared) provide floodwaters more room to release energy. As a result, a number of communities in our Region have been made safer.



A home in Rochester that was bought out in the buyout program.

Lands That Help Prevent Flooding

Wetlands

Wetlands are a vital component for maintaining the ecological integrity of land and water,

and they provide an array of functions and values that support environmental health and provide benefits to humans, including flood and stormwater control. Draining, filling, and development have resulted in the loss of more than 35 percent of Vermont's original wetland acreage, primarily due to agricultural and large-scale development projects, and this loss has increased flood risk.

The Vermont Wetlands Rules "identify and protect significant wetlands and the values and functions which they serve in such a manner that the goal of no net loss of such wetlands and their functions is achieved." Although only wetlands designated as "significant" are protected under the Wetlands Rules, the Rules state, "Wetlands not designated as significant under these rules should be assumed to have public value, and therefore may merit protection under other statutory or regulatory authority."

In the Region, just over 1 percent of the land area has been identified by the State of Vermont as "significant" wetlands, eligible for state protection under the Vermont Wetlands Rules. However, there are a large number of smaller wetlands that may qualify for protection. Examples of larger wetlands that help to attenuate floodwaters and reduce flooding damage in the TRO Region include the Class 2 wetlands through the Killington Flats area and along Swamp Road in Newbury. However, there are a number of smaller wetlands in all of the towns that also provide flood mitigation, water quality benefits, and wildlife habitat.

The Regional Commission recognizes the critical value of wetlands in relation to the health of the water, wildlife, and plant resources in the region and to the ecosystem as a whole. The Regional Commission supports and encourages communities to identify and inventory wetlands within the region and to adopt mechanisms for their increased protection. This information can increase the effectiveness of the state and federal regulatory process. Towns and communities have

never move, they are poor at showing that these other areas along small streams or alongside channels are at risk from flash flooding and lateral erosion. This leads to these areas often not being recognized as flood-prone or to the risk being identified simply as high water. Property owners in such areas outside of SFHAs are not required to have flood insurance.

To remedy this lack of accurate risk information and to create a tool that would allow towns to regulate development in these additional areas with flood erosion risk (but that are not shown on FIRMs), VT ANR has developed the second kind of flood risk map we have: a “river corridor” map. Initial river corridor maps have been produced for the entire state, and the agency is refining these as additional data is available. Maps of river corridors depict where the lateral movement of the river and the associated erosion is more of a threat than inundation by floodwaters. Elevation or floodproofing alone is often not protective of structures in these areas as erosion can undermine them.

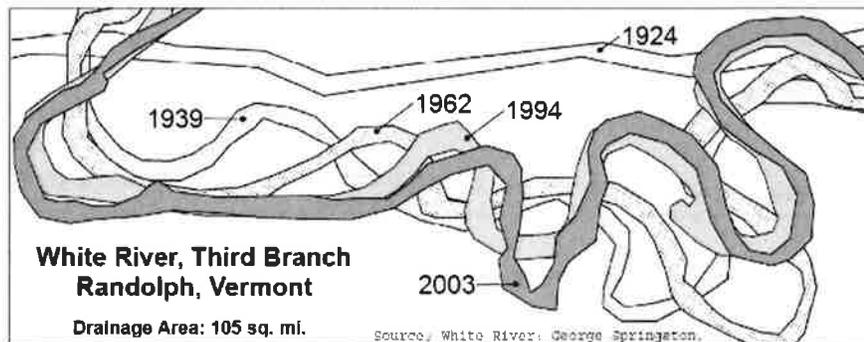
It should be noted that some lands within developed areas or next to existing structures, though mapped as river corridors and potentially subject to erosion risk, may be removed from this area during permitting, as the channel’s edge has already been reinforced so that erosion does not occur or will be repaired. In recognition of this, the river corridor maps already stop at state highways and railroads, as it is assumed that these will be protected from erosion or replaced post-disaster by the government. TRORC is working with VT ANR on having the maps in

developed areas adjusted to reflect this reality.

Flood Hazard Regulations

In order to enable property owners to be eligible for federal flood insurance through the National Flood Insurance Program (NFIP), municipalities must adopt and enforce flood hazard area regulations either through their regular zoning bylaws or through a separate bylaw. A community’s flood hazard regulations must apply to at least the Special Flood Hazard Areas (SFHA) identified by FEMA and contain certain minimum standards. The regulations deal with the permitting of new structures in the floodplain and place restrictions on other types of activities within the floodplain. They also specify land, area, and structural requirements to be adhered to within the SFHA. Paradoxically, using only the minimum required FEMA regulations can increase flood risk, as these allow the placement of fill in areas that could have stored flood waters, permit development to flood heights that are outdated and too low, and also fail to take erosion into account at all.

Municipalities can seek to reduce the threat of flood damage within their jurisdiction by not allowing new structures in the floodplain and through enacting stricter standards than the minimum required by the NFIP, such as elevating structures one to two feet above the base flood level and regulating development in river corridor areas as well. Lax enforcement of flood regulations can place people at risk of injury or death, place infrastructure and property at risk of damage or destruction, and can even create liability on the part of the community.



White River's path through the years. | Source: George Springston

Figure 3-5: Example of a FIRM map in Pittsfield



2011 and 20.4 percent in October of 2011.

Flood Hazard and Fluvial Erosion Hazard Areas in the TRO Region

Flood Hazard Areas

There are two sets of official maps that can govern development in the floodplain in Vermont. Though they have limitations, these maps are the best current means of showing areas with higher flood risk. The first of these is the Federal Emergency Management Agency’s (FEMA) Flood Insurance Rate Maps (FIRMs). Every town in our region has these areas of flood risk mapped by FEMA. The FIRMs show the floodplain (the Special Flood Hazard Area or SFHA) that FEMA has calculated would be covered by water in a 1% chance annual inundation event, also referred to as the “100-year flood” or base flood. It is important to understand that the 1 percent chance flood was calculated with limited historical rainfall data on a relatively rough topographic scale. Many parts of the region have had several “100-year” floods in the last 20 years and there is now evidence that extreme rainfall increased starting in the mid-1990s.

(labeled “Zone A”). In these areas, the base flood elevation has not even been determined and the map is drawn at a rough scale. As a result, a map like this does not provide the elevation to which a structure must be raised, leading to more expense by landowners who must find out that information. Such maps also do not show where the “floodway” is. The floodway is an extremely risky part of the floodplain where the current is strong. Since special restrictions apply to floodways, not having these mapped is cumbersome for owners and towns as these areas must first be determined on a case by case basis. Lastly, no special flood hazard areas or floodways are mapped at all for smaller streams, leaving out these risky areas and creating a false sense of safety.

A significant portion of flood damage in Vermont occurs outside of the FEMA mapped areas along these smaller upland streams, as well as along road drainage systems that fail to convey the amount of water they are receiving. Since FEMA maps in the region are concerned only with inundation and also assume that river channels

Most of the FIRMs used by the towns in the Region are outdated. Most towns have maps based drawn up in the 1970s. Orange County’s maps are largely still in paper form and are not able to be used with modern mapping programs. Windsor County’s maps have been converted to digital format, but the underlying data, except along the Connecticut River, is also 30 to 40 years old. The outdated information on these FIRMs provides challenges for administering a town’s flood hazard regulations. Some towns or areas of towns have extremely basic FIRMs with approximate A Zones

already documented in the Northeastern U.S., especially after 1996. Precipitation models currently used in designing and building road infrastructure, informing policy decisions, and in regulating the location where structures and facilities are built rely on historical data that is no longer accurate for current conditions and will only become less accurate as climate change continues.

Flood Damages

Floodwaters spilling over riverbanks have given us broad and fertile floodplains. Floods have carved our valleys and made our hills and mountains. Were it not for human infrastructure and settlement in the path of it, flooding would be a natural occurrence but not a hazard. However, we have built most of our towns and villages right next to the rivers that powered our mills, carried logs, provided water, and took away our waste. We built our roads along streams, as that was the easiest route, and often used gravel mined from the adjacent stream. When it seemed inconvenient to plow around meandering streams or to bridge rivers, we just moved the waterways aside. Erroneously thinking that rivers behaved like pipes, we straightened them thinking they would flood less, but that actually only increased their erosive force. Due to our actions, not nature's, flooding is the worst current natural threat to residents and infrastructure in the TRO Region.

Flooding in the Region causes immediate impacts such as eroded river banks, road closures, flooded structures, and crop damage. However, once the stress of the initial flooding impacts has subsided, the more long-term impacts begin to show, especially after major flooding events. One long-term impact is the effect of flooding on the region's economy. Economically speaking, Tropical Storm Irene struck at a very inopportune time at the end of August 2011, when the year's



Route 4 Before and After Tropical Storm Irene
| Source: USDA Farm Service ©Google

crops were ready for harvest or would have been ready in a few weeks. Because many of the region's farms and agricultural lands are located in the floodplain, crop damage was widespread. Approximately \$2 million in vegetable crops alone were destroyed or left to decompose statewide. The economic estimate for flood damage to farms statewide was estimated at \$20 million (this estimate includes buildings and land, hay, corn, pasture, soybeans, vegetables, and fruit).

Vermont is a destination for travelers, especially in the fall foliage season. Due to the damaged road infrastructure after Irene travel was difficult. Finding an east-west route was especially difficult, as many of the major roads in the region had been damaged at one section or another, including US 4, VT 100, VT 107, and VT125. With the fall season approaching, travel to areas not directly off the major highways was slow or impossible. Woodstock was among the most hard-hit areas in the state for room sales, reporting a drop of 68.4 percent in September

Causes of Flooding

Flooding in our region is caused by a small number of distinctive types of weather, and can be worsened by the conditions on the land (such as saturated or frozen soils) at the time the flooding occurs. By far the most common type of weather event to cause flooding in our region is a severe thunderstorm. These storms are usually afternoon storms in the warmer months, but they can also be associated with hurricanes and tropical storms, which also occur during the summer and into the fall. By the time most hurricanes reach Vermont, they have been downgraded to tropical storms, but that is not to say they are less dangerous. The speed of the hurricane or tropical storm and pockets of varying severity within the storm system have an impact on the rainfall totals observed from town to town. For example, Tropical Storm Irene dropped over six inches in much of the White River Valley (and nine inches in Rochester, according to local reports), causing extensive flooding damage. However, the towns in the Region along the Connecticut River received only 3” to 5” and experienced minimal flood damage. Storm impacts can be greatly magnified by previous rains. Tropical Storm Floyd in 1999 was very similar to Irene, but it fell on dry ground and is hardly remembered.

“Resilience” means that an entity—a person, neighborhood, town, state, region or society— when faced with a particular situation or event, has the ability to effectively return to its previous state or adapt to change(s) resulting from the situation or event without undue strain.

Ice jams due to the combination of melting snow and rain leave our region vulnerable to the impacts of flooding in the winter and early spring. Ice jams typically occur during the spring when river ice begins to break up and move downstream, but they may also occur during a

thaw period in the winter months. These sheets of ice then “jam” as they become hung up on a narrow or shallow portion of the stream or river creating a dam, and additional ice and water rapidly back up behind them. Once the “dam” breaks free, flash flooding may occur downstream. Ice jams in our region typically cause minimal damage, but they can damage road infrastructure and flood homes and businesses. The mainstem and First and Third Branches of the White River, the Waits River, the Connecticut River, and several smaller brooks have all experienced ice jams.

Flooding is worsened by land uses that create hard surfaces, which lead to faster runoff, and by past stream modifications such as straightened or dredged channels, which can create channel instability.

Implications of Climate Change and Flooding

According to a white paper produced by the Vermont Agency of Natural Resources (VT ANR)’s Climate Change Team, climate change will likely bring about conditions that exacerbate flooding in Vermont. The summer season is expected to lengthen overall, and the total precipitation is expected to increase in all seasons except the fall. The frequency of heavy precipitation events is likely to increase in all seasons, with the heaviest precipitation events occurring during the summer months. Perhaps more importantly, precipitation will likely occur in shorter, more intense bursts and, consequently, will produce precipitation that runs off the land more than it filters into it. An increase in extreme precipitation is

Climate change will likely bring about conditions that exacerbate flooding in Vermont.

~VT Agency of Natural Resources

C. Flood Resilience

Types of Flooding

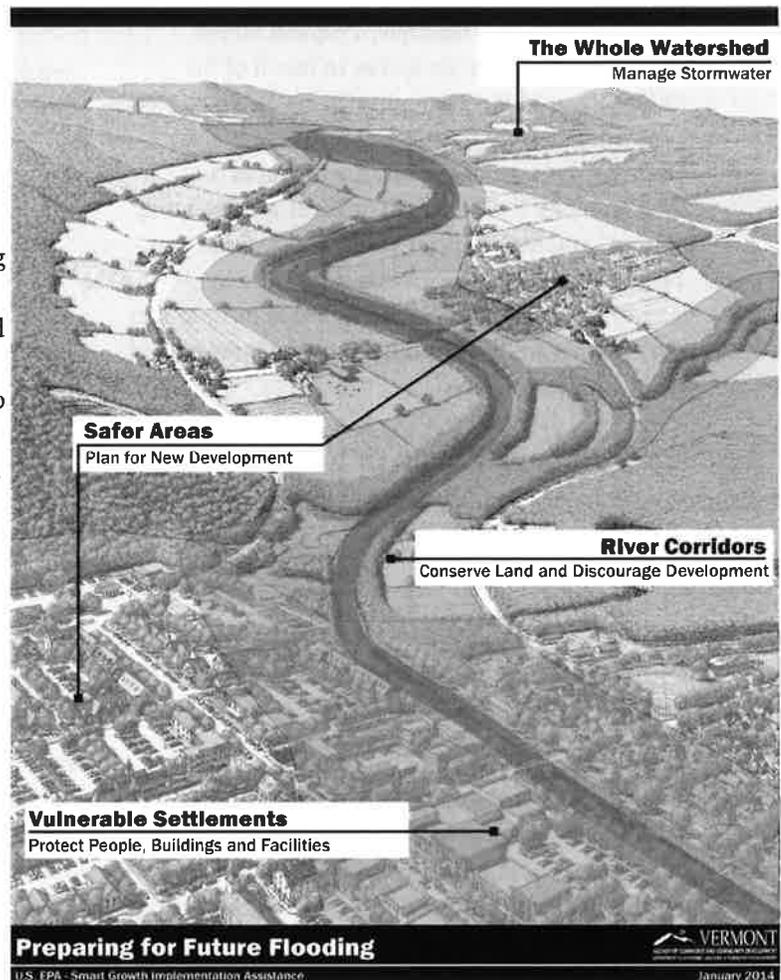
Generally speaking, there are two types of flooding that impact communities in the state of Vermont—flooding caused by inundation and flash flooding. Inundation flooding usually occurs slowly, but flood waters can cover a large area. It may take days or weeks for inundation floodwaters to subside from low areas, which may severely damage property. Inundation flooding takes place on flat and poorly drained land, typically along obvious floodplains. Ice or debris jams can also create inundation flooding as floodwaters back up behind such jams.

Flash flooding occurs when heavy precipitation falls on the land so quickly that the soil is unable to absorb it into the ground, leading to surface runoff. Runoff can be increased by saturated soil, extremely dry soil, frozen ground, and impervious surfaces. The quick-moving runoff collects in the lowest channel in an area, turning upland streams, small tributaries, and even dry ditches into roaring brooks. Flash flooding typically does not cover a large area, but the water moves at a very high velocity and the flooding manifests quickly, making flash floods particularly dangerous. Due to the velocity of the water, a flash flood can move boulders, trees, cars, or even houses.

Heavy storms can also cause fluvial channel erosion, in which the bank erodes and the channel migrates sideways and/or cuts deeper. Fast-moving water in a stream channel may undermine roads and structures and permanently change the channel itself, predisposing other roads and structures to future flooding damage. Flash floods can also mobilize large amounts of gravel and woody debris, depositing these in less steep areas as well as plugging culverts and leading to even greater damage. In Vermont and the Region, most flood-related damage

is caused by flash flooding and fluvial erosion (erosion of stream banks). Flooding is the worst current natural threat to residents and infrastructure in the TRO Region and the state.⁴

Significant flooding events have occurred in the TRO Region throughout recorded weather history. Due to the topography of the region, it is likely that large-scale or widespread localized flooding has been occurring for hundreds or thousands of years. Please see Appendix D for a table outlining the flooding events that have occurred in the TRO Region over the past 100 years, beginning with the worst flooding event to hit the TRORC and Vermont, the “Great Flood of 1927.”



Source: EPA

Goals, Policies and Recommendations: Forest-Based Resource Areas

Policies (continued)

13. New developments must take reasonable steps to avoid disruption or loss of major identified wildlife corridor crossings. Transportation enhancement projects should be pursued to mitigate vehicle conflicts with wildlife, including signage and education and awareness programs along road corridors that host significant numbers of wildlife crossings. In addition, initiatives should provide for improvements to the transportation infrastructure to reduce vehicle collisions and wildlife fatalities.
14. Upgrading or paving gravel roads; upgrading electric distribution lines or extension of utilities is not appropriate in this area, except as needed to serve outside areas, unless the public is clearly benefited thereby and where it is determined not to compromise the land use goals and policies for this Area.

Recommendations

1. As habitat data is updated, the TRORC will re-evaluate this land use area to ensure that its purposes are being met.
2. The TRORC will work to ensure that the functions of these areas are economically valued so that both the towns containing them and their owners have incentives to leave them in a largely undeveloped state.
3. The TRORC will work with state and federal agencies and conservation organizations to conserve these lands in ways that also support the local economy and bring value to landowners.
4. The TRORC will work with member towns on town plans and bylaws that will address smaller development not subject to Act 250 so that it is done in ways that preserve the functions of these areas while allowing compatible development.

Goals, Policies and Recommendations: Forest-Based Resource Areas

Goals

1. Healthy forests remain an important part of the Region's landscape and continue to provide their unique functions, including recreation, forest products, and wildlife habitat.
2. Upland forests serve to retain and cleanse water and have high quality waters.
3. Forest blocks are connected so that species can move between them.

Policies

1. Land above 2,500 feet elevation shall be maintained predominantly in a natural wilderness state, except in cases of wind power and/or telecommunications projects endorsed by this Plan.
2. Acquisition of lands, or conservation easements on lands, by the Federal Government, the State of Vermont and non-profits is encouraged between willing parties. Management plans prepared for conserved or acquired areas must recognize the concept of preservation as well as forest utilization.
3. Outdoor recreation and forestry uses are encouraged provided these uses do not unduly impact other significant resources of the site.
4. Timber production is encouraged in this land use area provided it is done in accordance with best management practices and managed and harvested in ways that keep soil erosion and sedimentation of streams to a minimum.
5. Motorized recreation must be limited to designated existing trail/road networks and new connections between trails, and be compatible with any critical wildlife habitat and water quality protections. Retention of snowmobile trails, many of which go over private land and are part of the statewide VAST trail network, is a priority. Conservation plans developed for landowners in this land use area should reflect, where practicable, the desire to retain this network of trails and not close or cut off important trail routes.
6. New structures capable of being occupied year-round are not appropriate in interior (greater than 300 feet from the forest edge) parts of these areas, but noncommercial seasonal camps serving hunters, snowmobilers, and other outdoor recreational users are appropriate.
7. Any use deemed appropriate to elevations over 2,500 feet should be sensitive to slow vegetative recovery and severe soil limitations and must avoid erosion.
8. Subdivisions and other development subject to Act 250 on lots over 30 acres shall minimize impacts on forestry potential and habitat values of undeveloped areas by concentrating development at the forest edge near other development and roads; use small lot sizes and shapes so that at least 80 percent of the land remains in a large undeveloped tract; minimize clearing of forest, and avoid the creation of additional roads or power lines that would further future development into interior areas.
9. Large subdivisions of more than ten structures are inconsistent with this Plan.
10. Outdoor recreation is encouraged. Development of snowmobile, hiking, and cross-country ski trails and similar recreational facilities are appropriate uses subject to meeting acceptable management practices and applicable state law.
11. Formal designation of Class II groundwater areas and Class A1 and B1 surface waters by the State of Vermont is encouraged within the land use area.
12. No development in its built-out state shall create more than one acre of impervious surface.

Goals, policies, and recommendations continued on next page

of Fairlee and West Fairlee. With the help of the Trust for Public Land, an additional 580 acres was added in northern Fairlee that had been owned by the Town of Bradford. The 1,055 acre area is now owned by the Town of West Fairlee and protected from development through a conservation easement. It abuts the separate West Fairlee Town Forest and the large 1,500 acre Fairlee Town Forest. The lands in public ownership comprise just a small section of the greater 28,000-acre Brushwood Forest area that boasts an extensive trail network, vast undeveloped forestlands, wetlands, and wildlife habitat.

Coolidge State Forest (CSF)/Arthur Davis Wildlife Management Area

CSF encompasses 21,500 acres of land in Plymouth and Woodstock, and additional lands in Reading, Killington, Mendon and Shrewsbury. The State Forest includes Coolidge State Park where campsites, hiking trails, and beautiful scenic views are abundant. CSF is the state's third largest State Forest and is managed by the Vermont State Parks' Department of Forests, Parks, and Recreation (FPR). It abuts the 7,788 acre Arthur Davis WMA found in Plymouth and Reading, which is managed by the Vermont Department of Fish and Wildlife.

Green Mountain National Forest (GMNF)

With over 400,000 acres, the GMNF is located within several Two Rivers towns, including: Woodstock, Rochester, Hancock, Pittsfield, Stockbridge, Granville, Bridgewater, Pomfret, Hartford, and Norwich. The lands contain portions of the Long Trail, Appalachian Trail, and the Robert Frost National Recreation Trail. These areas preserve the headwaters of the White River and provide significant outdoor recreation and forestry opportunities, as well as form part of the largest north-south wildlife corridor in the state.

Orange County Headwaters (OCH)

The OCH Project was started by landowners in the Towns of Washington and Corinth who had an interest in conservation. Through the Vermont

Land Trust and the Upper Valley Land Trust, 31 OCH landowners have conserved 4,500 acres. Much of this land is forested.

Pine Mountain Wildlife Management Area (WMA)

Pine Mountain is one of the larger WMAs in the Region. It spans the towns of Topsham and Newbury as well as Groton and Ryegate (outside of the Region). It is 2,274 acres in size, 95% of which is forested. Managed by the Vermont Fish and Wildlife Department, the Pine Mountain WMA is home to white-tailed deer, black bear, moose, and many other mammals, birds, fish, and amphibians. The area is open for hiking, fishing, trapping, and hunting.

Other Lands

The Region has other smaller state and town owned lands, as well as privately-owned lands that are protected through conservation easements held by land trusts, such as the Vermont Land Trust or Upper Valley Land Trust.

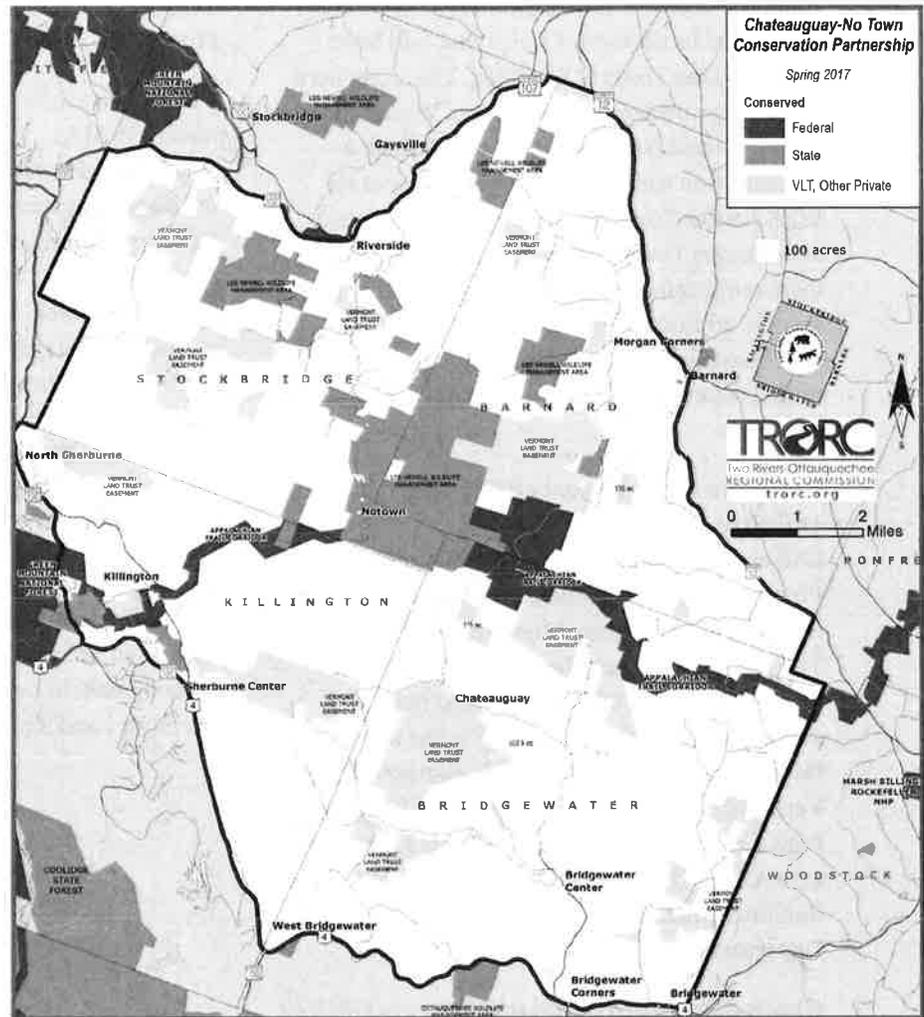
project on specific plans to voluntarily conserve their land.

Like much of the Forest-based Resource Areas, in the CNT multiple recreational activities are present, especially seasonal hunting camps, snowmobiling, and hiking. The Appalachian Trail passes through the central section of the CNT. The CNT also contains the 7,988 acre Les Newell Wildlife Management Area and provides valuable habitats for wildlife, including black bear, moose, bobcat, and deer. The entire CNT has been identified by the Vermont Department of Fish and Wildlife as bear production habitat. The CNT serves as a critical link between the bear production areas south and north of US Route 4. The long-term stability of black bear depends on the retention of this area in a predominately undeveloped state.

Taylor Valley

The Taylor Valley area straddles parts of the towns of Vershire, Chelsea, Tunbridge, and Strafford. This area has large stretches of undeveloped land, wildlife habitat, unique flora and fauna, productive timber land, productive agricultural land, and extensive areas for hunting and other outdoor recreational opportunities. The privately organized Taylor Valley Conservation Project has identified a core area of 19,000 acres centered around the Taylor Valley for special conservation attention. Approximately 4,000 acres in the core area have been conserved

Figure 3-5: Chateaugay No Town (CNT) Conservation Area Map



through conservation easements, and landowners have committed to the conservation of an additional 1,700 acres in the core area. The greater Taylor Valley Area also includes extensive forestlands stretching from the Strafford-Tunbridge Road in a southerly direction to the Joseph Smith birthplace including over 1,000 acres protected by conservation easements held by the Upper Valley Land Trust and the Vermont Land Trust.

Brushwood Community Forest/West Fairlee Town Forest/Fairlee Town Forest

In 2009, Brushwood Community Forest was established on approximately 475 acres of relatively undeveloped forestland in the Towns

Figure 3-5: Highest Priority and Priority Connectivity Blocks in the TRO Region

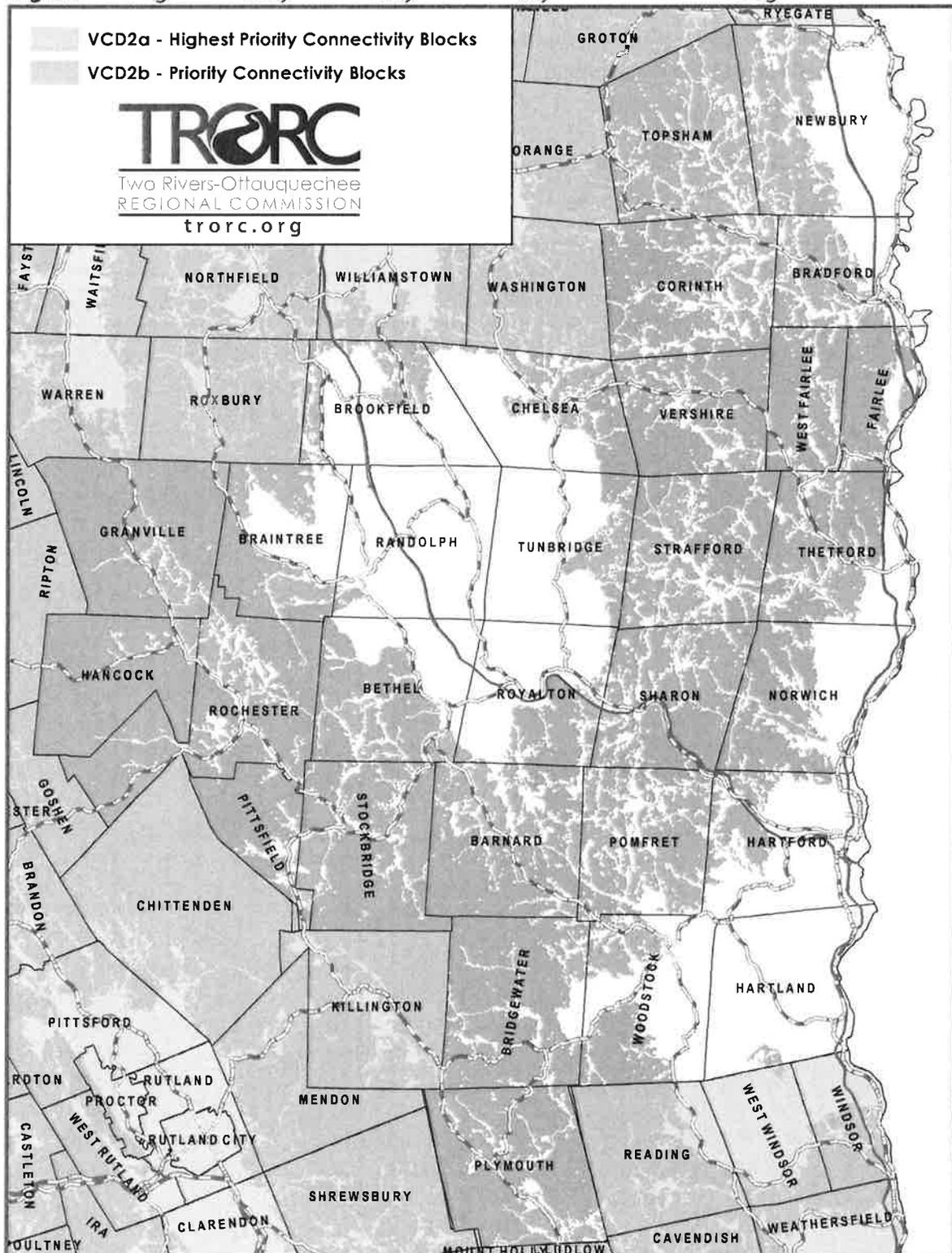
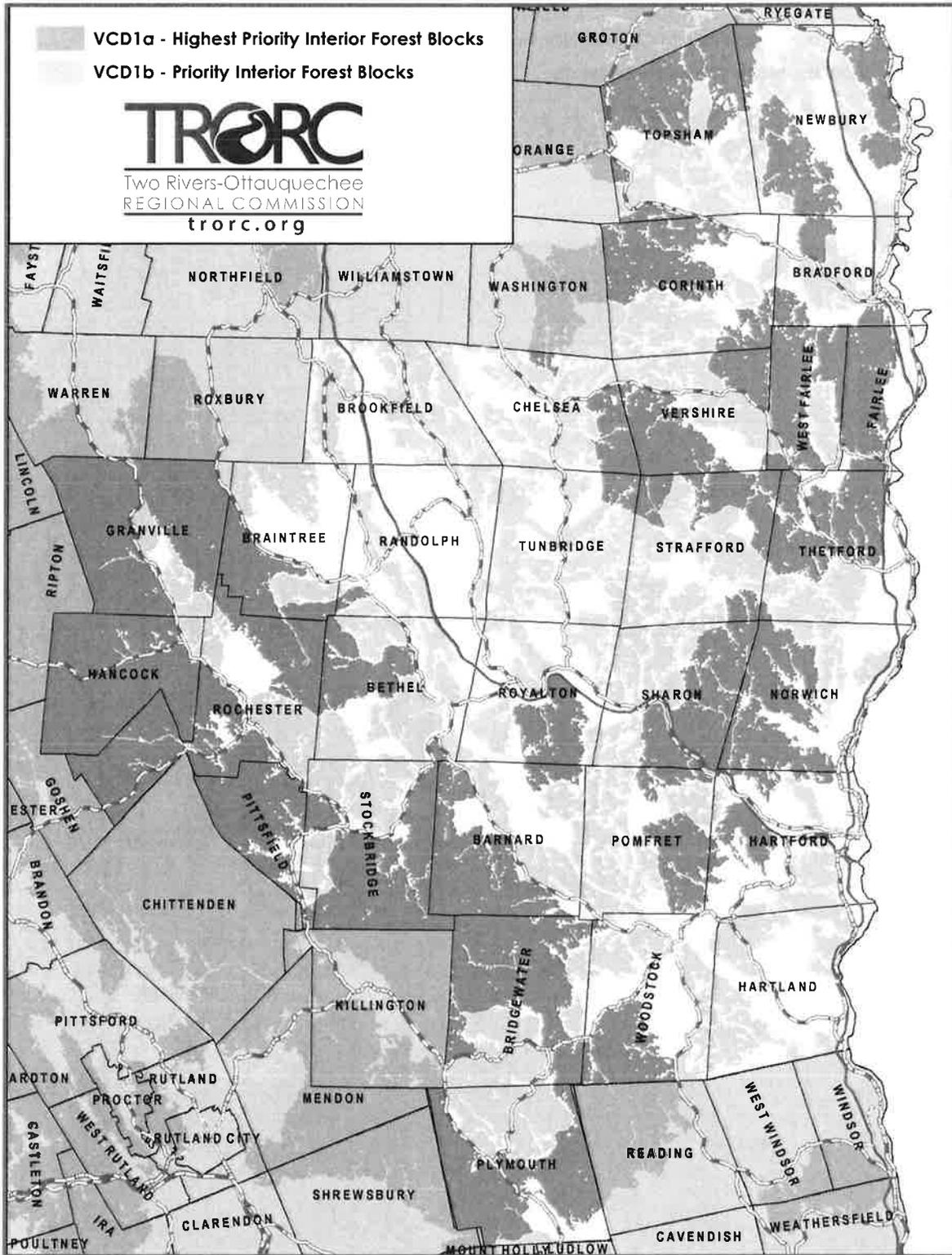
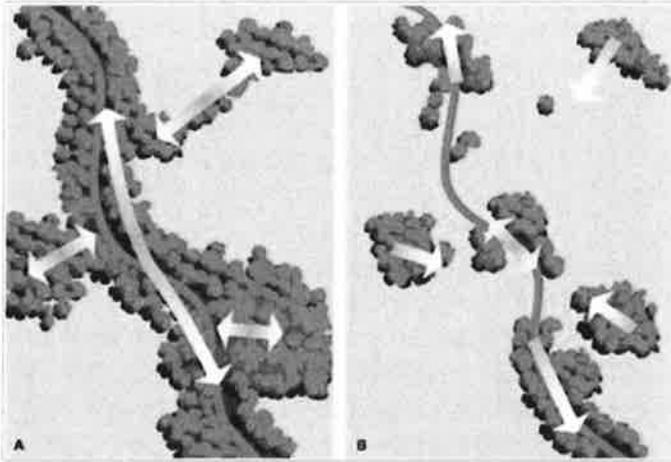


Figure 3-5: Highest Priority and Priority Interior Forest Blocks in the TRO Region





Landscapes with (A) high and (B) low degrees of connectivity. Corridors are particularly important for wide-ranging species whose habitat needs are not accommodated by a single patch of suitable habitat.

nearby, and adjacent conserved or public lands. This resulted in the final Forest-based Resource Areas shown on the future land use map.

Allowing the lands in these Areas to remain largely undeveloped will maintain their ability to provide timber production, outdoor recreation, flood storage and aquifer recharge, scenic beauty, and wildlife habitat, and contribute to our economic well-being and quality of life. Allowing some careful development in them will create income for landowners and address other regional goals, such as outdoor recreation and housing. It is not the intent of this Plan to create true wilderness areas, and the policies in the Plan reflect that. However, much of the Region's land that once provided large-scale wildlife habitat can no longer do so due to existing development, and therefore the remaining lands in some towns are more likely the *minimum* needed to fulfill these functions rather than the optimal amount.

Figure 3-5 shows VCD Forest Blocks, where the dark green areas are highest priority blocks and the light green are priority blocks. Figure 3-6

shows VCD Connectivity Blocks, where the dark blue are highest priority blocks and light blue are priority blocks.

Chateauguay No Town (CNT)

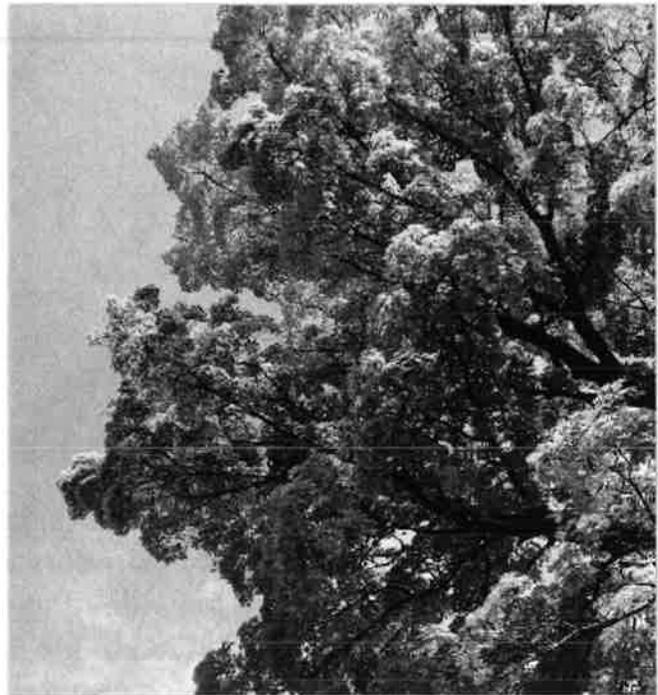
A particularly large and coherent part of the region that exemplifies the Forest-based Resource Areas is what is known as the Chateauguay No Town (CNT) area. This is a remote upland wilderness area consisting of approximately 55,000 acres covering parts of the Towns of Barnard, Bridgewater, Stockbridge, and Killington. With limited exception, land parcels are large, ranging up to several thousand acres in size. Human settlement is sparse, year-round public access is practically non-existent for most of the area, and public services (such as electric or telephone) are very limited. Roads are relatively narrow and steep and are not designed to sustain heavy vehicles or high volumes of traffic. The few inhabitants living here mostly provide their own power and lighting and maintain and plow their own roads. Much of the CNT is owned by timber companies or families interested in using the land for wood production and land is enrolled in Vermont's Land Use Value Appraisal Program.

In late 1997, the Chateauguay No Town Conservation Project was launched by the four towns the CNT is located in, "to foster, through locally sponsored conservation activities, the long-term commitment to stewardship of exceptional forest, wildlife, and recreational lands." Since then, a locally appointed committee, in cooperation with the Vermont Land Trust, The Conservation Fund, TRORC, Appalachian Trail Conference, and the Vermont Agency of Natural Resources, has been evaluating ways to voluntarily conserve this area, to protect critical habitats, to promote sustainable forestry, and to ensure recreational opportunities. To assist the CNT partners in the implementation of the project, both a local and a regional conservation fund have been established to provide financial resources to landowners interested in conservation of their property. Several landowners have agreed to work with the

Both fragmentation and parcelization, will impact the important functions we now enjoy from the large forest blocks in these Areas, but unless lands are bought outright for conservation or have easements on them, some development is likely. How this development, from simple subdivision into lots to subsequent construction of roads and buildings, takes place is a matter of public concern, as it can negatively affecting forestry and the many species that depend on such areas, as well as generate off-site impacts, such as increased flood flows. Further development in remote areas would also create increased costs for towns to maintain or upgrade minor Class 3 or 4 roads and would work against regional energy goals as such development is much more reliant on single occupant vehicle trips.

Such landscapes need to be addressed at the regional level. An individual landowner might be able to conserve a wooded wetland for salamanders or a small forest for deer habitat, but this would not be enough to meet all the needs of that species within the area. A large timber owner might conserve a sizable forest, but that does not support an industry. Even efforts at the town scale (though very important) do not contain enough land for many species' needs, which can be several hundred acres per individual among the larger animals.

The best available data on where the priority interior forest blocks, as well as priority wildlife corridors exist (see maps below) has been produced by the Vermont Conservation Design (VCD) joint project of the Vermont Department of Fish and Wildlife, Vermont Department of Forests, Parks and Recreation and the Vermont Land Trust. Wildlife corridors are critical to connect the large blocks so that populations do not become inbred, species can move as climate shifts, and isolated incidents such as blowdowns or timber harvesting do not threaten overall species' health. Several wildlife species need habitat areas exceeding one square mile for population health.² As noted in the report *Vermont Conservation Design: Maintaining*



Vermont Foliage | © Judith A. Brown

and Enhancing an Ecologically Functional Landscape, if these landscapes are conserved on a large scale, then “most of the species they contain . . . will also be conserved.”³ There are no doubt additional locally significant lands that are not large enough to show up in this data that are still important.

While the Region looks well forested from the air, there are places in the Region where forest blocks and wildlife corridors are tenuous, particularly in the area that stretches from Barnard to Sharon. However, as the maps below show, for nearly half of the Region's towns the entire town outside of small developed areas along roads is either a forest block or a wildlife corridor block.

It is not a regional goal, and certainly not a town goal, to have nearly entire towns developed at a very low density. Consequently, the VCD map of these areas was used as a *starting point* when developing the Forest-based Resource Areas regional future land use area, and then it was modified based on town future land use maps, infrastructure, the amount of land that performs interior forest or habitat connector functions

Table 3-1: Wildlife Present in Forest Patches

Undeveloped	500 - 2,500 acre blocks	100 - 500 acre blocks	20 - 100 acre blocks	1 - 20 acre blocks
Raccoon	Raccoon	Raccoon	Raccoon	Raccoon
Small rodent	Small rodent	Small rodent	Small rodent	Small rodent
Squirrel	Squirrel	Squirrel	Squirrel	Squirrel
Red fox	Red fox	Red fox	Red fox	Red fox
Songbirds	Songbirds	Songbirds	Songbirds	Songbirds
Skunk	Skunk	Skunk	Skunk	Skunk
Amphibians	Amphibians	Most Amphibians	Most Amphibians	Most Amphibians
Reptiles	Reptiles	Reptiles	Most Reptiles	Most Reptiles
Hare	Hare	Hare	Hare	
Porcupine	Porcupine	Porcupine	Porcupine	
Beaver	Beaver	Beaver	Beaver	
Weasel	Weasel	Weasel	Weasel	
Mink	Mink	Mink		
Turkey	Turkey	Turkey		
Horned owl	Horned owl	Horned owl		
Barred owl	Barred owl	Barred owl		
Sharp-skinned hawk	Sharp-skinned hawk	Sharp-skinned hawk		
Cooper's hawk	Cooper's hawk	Cooper's hawk		
Broad-winged hawk	Broad-winged hawk	Broad-winged hawk		
Osprey	Osprey	Osprey		
Harrier	Harrier	Harrier		
Deer	Deer	Deer		
Wood frog	Wood frog	Wood frog		
Ring-neck snake	Ring-neck snake	Ring-neck snake		
Bald eagle	Bald eagle			
Goshawk	Goshawk			
Moose	Moose			
Red-tailed hawk	Red-tailed hawk			
Coyote				
Bobcat				
Black bear				
Fisher				

Source: Above and Beyond. Campoll, J., Humstone, E., & MacLean, A. 2002.

Goals, Policies and Recommendations: Rural Areas

Policies (continued)

9. Non-residential uses, including small service businesses, small professional offices, and inns are acceptable land uses for Rural Areas provided that such uses are located near existing transportation infrastructure; planned at a residential scale and form; are not primary or dominant uses in an area; would not unduly conflict with existing or planned residential, forestry, or agricultural uses; and do not unduly affect rural character.
10. TRORC supports the right of a resident to use a minor portion of a dwelling unit for a home occupation, which is customary in Rural Areas, provided it does not create a nuisance or have an undue adverse effect on the values noted in this Plan as being important to sustaining the character of Rural Areas.
11. Major retail enterprises or service centers that draw principally on regional market shares (including factory outlets, large grocery stores, fast food establishments, and shopping malls) shall not be permitted in Rural Areas.
12. Smaller non-formula retail stores aimed at local markets may be appropriate when located where commercial development already exists, but shall not create a new node of commerce.
13. Development shall be designed to take reasonable steps to minimize accesses onto public roads, and projects that would create traffic demands that require the paving of rural gravel roads are not appropriate in Rural Areas.

Recommendations

1. The TRORC will work with towns and developers to site housing in Rural Areas to meet housing needs. (See also the Housing Chapter)
2. The TRORC will work to ensure that agriculture in these areas remains an important part our economy. (see also the Working Lands Chapter)
3. The TRORC will work with towns, state and federal agencies and conservation organizations to conserve important forest and agricultural lands.
4. The TRORC will work with member towns on town plans and bylaws to address development in the Rural Areas so that it is meets state planning goals and the desires of towns.

Forest-Based Resource Areas

The lands within the Forest-based Resource Areas—primarily large blocks of unfragmented forest that are needed to sustain a forestry industry and areas that contain critical wildlife habitat and allow safe wildlife movement—provide the Region with important services that cannot be replaced on other lands. Land with these characteristics is shrinking in both the State and the Region. The health of many natural communities and wildlife depend on these large, uninterrupted areas of forestland, commonly referred to as “forest blocks”, and these must be connected to each other through wildlife corridors.

The main threats to such areas and their

functions are fragmentation and parcelization. Forest fragmentation is the division or conversion of forest blocks through the clearing of land, building structures, and other activities associated with development (excluding recreational trails). Even the seemingly simple act of installing roads affects wildlife movement and increases invasive plants and pests. Development that causes forest fragmentation creates barriers which limit species movement over the landscape, interrupts ecological processes, and impacts genetic diversity. Parcelization, which is part of fragmentation, is the subdividing of forest parcels to smaller lots but does not necessarily involve further development. Parcelization makes continuing to manage forests for forestry or conservation more difficult or even impossible.

the best use of the limited amount of land in Regional Growth Areas that have sewer and/or public water supply. Locating these immediately adjacent (within a quarter mile) to Regional

Growth Areas along major roads can serve to protect the desired aspects of both rural and more urbanized areas.

Goals, Policies and Recommendations: Rural Areas

Goals

1. Agriculture continues to form an important visual, economic, and cultural part of the landscape.
2. Rural lands provide a place for people's homes and small businesses.
3. Development is at a scale and type that conforms to historical patterns and does not detract from Regional Growth Areas.

Policies

1. Development shall be at a scale that is less dense than adjacent Regional Growth Areas.
2. Except along paved roads, development density greater than one principal structure per two acres is not appropriate to maintain rural character, but lot sizes are encouraged to be smaller than this in subdivisions so as to preserve a larger portion of the remaining lot as undeveloped and still meet overall density goals.
3. New individual multi-unit residential buildings containing five units or less are appropriate along Class 3 or better roads, but larger ones are not, excepting inns, outdoor recreation, and other lodging.
4. Adaptive reuses, such as small light industrial operations or multiple housing units, are encouraged in older existing large structures as towns desire, but care must be taken to not lead to development too intensive for the rural character.
5. Development of resource-based commercial uses is appropriate in these areas, with safeguards to protect neighbors from undue adverse impacts from noise, dust, and other nuisances (see also Section G in Chapter VI for more on extraction policies).
6. In Rural Areas that abut state highways and that are no greater than a quarter mile to Regional Growth Areas, land-intensive uses may be appropriate, provided that they do not have an adverse impact on the character of the adjacent Regional Growth Area, and mitigate the impacts of sprawl and strip development.
7. Projects subject to Act 250 must be planned and sited to satisfy the following:
 - a. Utilize compact development design and locate new development or lots near or adjacent to existing road infrastructure and away from productive fields or forests to conserve the maximum feasible amount of usable farm, pasture land, or managed woodland;
 - b. Locate non-agricultural buildings next to or within the forest edge (if any), instead of in open fields, to enable new construction to be screened by natural landscape features;
 - c. Minimize buildings, utilities, or structures blocking or interrupting scenic vistas as viewed from a public highway;
 - d. Take reasonable steps to protect historic features, wetlands, stream buffers, forest blocks, wildlife crossing areas, necessary wildlife habitat, and habitat connectors; and
 - e. Give consideration to burying power and phone lines, if cost effective, when new roads are being constructed.
8. Use of planned unit developments or conservation subdivision design schemes is strongly encouraged as a means of providing rural development that concentrates development on part of a parcel in order to preserve larger lots that are more useful for farming, forestry, or wildlife habitat. Towns should consider incentives such as density bonuses.

Goals, policies, and recommendations continued on next page

lands often may be the cheapest land to put an affordable home on. But, there are tradeoffs and the overall effect of unplanned growth in certain locations and at a certain scale in Rural Areas is in the process of transforming the landscape from something recognizable as “rural” in Vermont into something that is not. Rural Areas are not simply suburbs waiting to be created; they are a valued land use to Vermonters in their own right, and can remain that way for a long time if we are careful in how we develop them.

Not all land within Rural Areas is similar, nor should it be treated the same when planning for development. Some land is steep, wet, prime agricultural soil, of special habitat value, adjacent to waterways, or subject to flooding, and should largely remain undeveloped for these reasons. However, these aspects are very site specific and are dealt with on a policy basis elsewhere in this Plan. This section of the Plan addresses those uses desired for Rural Areas in general and that complement the more developed parts of the region.

One of the main land uses in the Rural Areas is agriculture, either in cropland or in pasture. These open lands are part of the aesthetic appeal of the region, underlie an agrarian culture, and form the basis for a significant part of our economy. Forestry is another important use, though most larger forests are in the Forest-based Resource Area. Agriculture and forestry and the land they depend on are addressed both in this chapter and in the Working Landscape chapter.

Regional land use policy elsewhere in this Plan focuses most business uses within or close to Regional Growth Areas. Rural Areas, however, can accommodate certain non-residential uses in ways that serve to maintain the vitality of more developed areas and that do not unduly compromise one of the principal objectives for these Areas—to retain rural character. For example, the Rural Areas are where many of the region’s homes are, so naturally many home occupations are found there as well. Home

occupations are allowed by right in local regulations in Vermont anywhere homes are allowed and are a way for people to earn a living with minimal land use impact. They must use less of the building than the home uses and can have a variety of small commercial operations.

Home enterprises are typically larger operations that are still on a residential lot, but are allowed in many town zoning bylaws with some limitations on the number of people that can work there and on impacts such as traffic. Contractor’s yards are a common home enterprise. With appropriate review, Rural Areas can provide these land uses a good location.

Rural Areas have many older structures such as large farmhouses or barns that lend themselves to adaptive reuses that can both preserve these visual assets and provide employment. Inns, small industrial operations, and multi-family dwellings are examples of uses that can keep these structures from becoming obsolete while not creating a major visual change to the rural landscape.

While commercial enterprises in the Rural Areas are smaller and scattered, there are two somewhat intensive commercial uses that make sense to locate in Rural Areas. These are either based on resources found there or are land-intensive commercial uses. Resource-based commercial uses provide economic benefits and jobs that support the rural landscape, and they are uses that would largely detract from developed Regional Growth Areas if located within them. Such uses have a traditional rural role in Rural Areas.

Commercial land-intensive uses that are not dependent on natural resources do not make

Land-intensive commercial uses are commercial operations that rely on large amounts of indoor or outdoor storage as the dominant use of space, and include sales lots and warehouses.

Rural Areas

The vast majority of land in the region lies outside any of the Regional Growth Areas identified in this Plan but is still not remote forest. These Rural Areas make up 51% of the region and are where many of us live, where most of our local food is grown, and form the principal visual backdrop along most roads. While we each may have a picture in our minds of what “rural” is, for this plan “rural”, and hence Rural Areas, means lands that consist of a low-density mixed pattern of land uses, primarily homes, interspersed with scattered small-scale businesses, resource-dependent or land-intensive commercial operations, outdoor recreation, and natural

Resource-based commercial uses are such things as sawmills, quarries, and sandpits, outdoor recreation, nurseries, and agricultural product processing. These are dependent on resources at the site or coming from Rural Areas or Forest-based Resource Areas and may include retail of products produced on site.

resource uses. The land is predominantly covered by forest, active agricultural land, or fallow agricultural lands transitioning back to forest. Rural lands are largely remote and undeveloped, or developed enough to constitute an existing settlement¹. Development within these Rural Areas has been largely constrained by on site limitations, including soil composition, slope, and elevation; ease of access to highways; lot size minimums, and distance to community services.

Historically, a significant amount of the Region’s growth over the past forty years has taken place in the Rural Areas, primarily in the form of scattered residential development that has crept up hillsides, out into fields, and deeper into forests. As residents locate their homes farther from Regional Growth Areas, commercial businesses

that serve those populations seek to locate closer to them, moving out of or away from traditional business centers. This pattern of growth in the Region is our version of sprawl and places land development pressures on Rural Areas, particularly in those communities that are nearest to major highways and serve larger populations.

This development has brought new life back into many towns, but these land use changes have also gnawed away at rural landscapes despite local planning efforts and public investment strategies that give priority to new projects within defined growth areas. This incremental change is largely due to no regulation through Act 250 of small-scale subdivisions, low regulation of residential development in those towns with zoning, and a general desire for “development” at the local level as this is seen as helping a town in terms of tax revenue or increased vitality. This in no way means that all residential development in Rural Areas is bad, but such development comes with costs, too. New homes increase a town’s tax base but the residents may require better road maintenance. Building on what were once farm fields offers farmers needed income but impairs the future of local farming. Higher property values increase a town’s grand list but may also drive up taxes on current residents.

It is in the interest of the region and in conformance with state our towns’ plans that scattered development not continue unabated so that the present land use features within Rural Areas can be maintained and remain dominant. State planning goals, to which the Regional Plan must be consistent, direct plans to “maintain the historic settlement pattern of compact village and urban centers separated by rural countryside . . . (and that) intensive residential development shall be encouraged primarily in areas related to community centers and that strip development along highways should be discouraged.” Rural Areas in the Region can provide substantial amounts of new opportunities to both reuse existing structures and to locate new homes, home occupations, and small businesses. These

Royalton Interchange (I-89, Exit 3)

Exit 3 on Interstate 89 in Royalton accesses Route 107, which runs in an east/west direction, connecting to Bethel and Stockbridge and Routes 100 and 14. Route 107 is classified as a minor arterial road. It is a heavily traveled road and forms part of a major transportation corridor between I-89 and Rutland and points west. Forecasts reveal that traffic volume will continue to grow over the next 20 years.

Following the completion of I-89 35 years ago, several parcels of land near the interchange area have been developed. Primarily these changes in land use have been from rural residential and agricultural uses to industrial or commercial uses, but still much of the area remains undeveloped, consisting of farm and forestland. Several areas contribute to highly scenic vistas, particularly from I-89 and Route 107. Due to its prominent location, it is likely that new development at Exit 3 will continue. Solid transportation planning, coupled with sound land use planning principles, can minimize land use and traffic conflicts that have plagued many other Interchange Areas.

In 1999, the Town of Royalton conducted an extensive planning project in which the Royalton

Planning Commission found the following values to be important to the area:

1. Provide space for future business growth, but only when it doesn't detract from Royalton's two villages;
2. Promote new development when plans are carefully laid out for safe access onto Routes 14 and 107;
3. Protect sensitive resource and scenic areas and encourage good design for new projects; and
4. Preserve the carrying capacity of Route 107 as a minor arterial road.

Given these values and an analysis of development suitability, nine future land use designations were recommended and depicted on a map. These included areas for industry, service and office type uses, residences, agriculture, and limited development. Goals and recommendations were listed to help guide the community on the highest and best uses for each sub-area. TRORC accepts the findings and conclusions contained in the *Exit 3 Planning and Development Study* (September 2000), which has since been incorporated into the Royalton Town Plan, as the planning policies developed by the Town of Royalton for this area.

Policies: Royalton Interchange

Policies

1. The types of land development appropriate for this interchange include offices, light industrial, residential, appropriately scaled traveler-oriented uses, and other similar uses that are not intended to draw on regional populations.
2. Principal retail establishments must be located in Regional Growth Areas to minimize the blighting effects of sprawl and strip development along major highways, to protect the vitality of our villages and downtowns, and to maintain rural character.

opportunities for development at the Exit 4 Interchange. The Randolph Town Plan reflects many of these efforts, dividing the Interchange Area into four quadrants and incorporating design and use standards for each quadrant into its land use regulations. Key components include the following:

1. Provide space for the development of business parks with design guidelines to protect scenic values;
2. Provide open space for the conservation of wetlands, streams, steep slopes, other natural resources, and visual quality;
3. Limit or deny new curb cuts to maintain the carrying capacity of Route 66;
4. Provide space and opportunities for

transitional/senior housing;

5. Provide for an improved park-and-ride commuter lot/Welcome Center; and
6. Consider land for an agricultural/cultural museum perhaps to be affiliated with other uses.

Further, the extensive study conducted by the community over the past 18 years determined that retail development at the interchange was unsuitable for a combination of reasons, including traffic impacts on Route 66, visual sensitivity, and conflicts with downtown businesses. Moreover, standalone retail development at any scale or size was found to be incompatible with the community's values. However, there was one exception. Accessory uses of a retail nature were found acceptable.

Policies: Randolph Interchange

Policies

1. The development of large-scale retail at the Randolph interchange—including shopping centers, malls, auto dealerships, and big-box stores—is inconsistent with this Plan.
2. Small-scale retail uses secondary or subordinate to primary uses and non-traditional to downtown Randolph or its village areas may be acceptable uses subject to in-depth review and evaluation by the community.
3. Any project planned for the interchange must employ design and construction standards that will ensure that development does not unduly impair the scenic resources of the area.
4. New development should be sited in areas that are not highly scenic, visible, or environmentally sensitive.
5. Future development at the interchange that requires improvements to Route 66, including traffic signals, turning lanes, or roundabouts, must be carefully evaluated. These should only be authorized where it is determined such a privately funded investment will not unreasonably endanger or interfere with the function, efficiency, safety, or use of this route.
6. New development must coordinate with existing development on shared access or retrofit access point locations to improve safety.
7. The types of land development appropriate for this interchange include offices, light industrial, residential, appropriately scaled traveler-oriented uses, and other similar uses that are not intended to draw on regional populations.
8. Principal retail establishments must be located in Regional Growth Areas to minimize the blighting effects of sprawl and strip development along major highways, to protect the vitality of our villages and downtowns, and to maintain rural character.

the state and therefore carries steady volumes of traffic. This interchange is located 1.5 miles from municipal sewer and water service; the residential wastewater system located to the west in Quechee is a shared leachfield system. The on- and off-ramps for the northbound and southbound lanes are located 0.5 miles apart. There are two different scenarios present at either end, with the northbound interchange leaving few opportunities for development due to the close proximity of 30-percent slopes and the interstate.

The southbound interchange is a sprawling commercial area with access roads intersecting the on- and off-ramps.

White River Junction—the Regional Center, a Vermont Designated Downtown, and a Designated Growth Center is located 3.5 miles to the east. Development at this interchange should be of a type that does not displace the development and investment that has occurred in the Regional Center or in Quechee Village. In order to mitigate against the impacts of strip development and sprawl, and to ensure the vitality of Hartford’s Regional Center, Town Center, Village Center, and Hamlet Area, this interchange is not an appropriate location for principal retail establishments.

Policies: Quechee Interchange

Policies

1. Intensive development that increases traffic volumes must not be permitted on the open lands accessed by Stagecoach Road; it would degrade the operation and safety of the interstate 89 and U.S. Route 4.
2. Development around the southbound interchange must be planned based around access points that do not degrade the functionality of U.S. Route 4 or the I-89 on- and off-ramps.
3. The types of land development appropriate for this interchange include offices, light industrial, residential, appropriately scaled traveler-oriented uses, and other similar uses that are not intended to draw on regional populations.
4. Principal retail establishments must be located in Regional Growth Areas to minimize the blighting effects of sprawl and strip development along major highways, to protect the vitality of our villages and downtowns, and to maintain rural character.

Randolph Interchange (I-89, Exit 4)

The Exit 4 interchange on Interstate 89 is located in Randolph, 3 miles from the revitalized historic downtown and commercial district and 1 mile from historic Randolph Center, home of Vermont Technical College (VTC). Exit 4 accesses Route 66, a two-lane connector road that runs in an east/west direction between the Village of Randolph, Randolph Center, East Randolph, and Route 14. This area is predominately open land, including farmland and woodland. The interchange area is particularly well-known for panoramic and distant scenic vistas, particularly the mountain views to the west. There are several structures at the interchange, including a gas station and convenience store, a fast-

food restaurant, professional offices, an auto service repair garage, a state highway facility, an industrial/office complex, and several single-family residences.

Presently there is no existing municipal water supply provided to the area, although there are water supply systems on the western edge of the area (Fish Hill) and eastern edge near VTC. An existing sewer line passes through the area and conveys wastewater from VTC down Route 66 to the municipal treatment facility. Annual average daily traffic (AADT) on Route 66 is estimated to increase with or without new development in the area.

Since 1998 the Town of Randolph has explored

Policies: Interchange Areas - General

Policies

5. Development concepts that must be utilized for Interchange Areas include:
 - a. A circulation system that is conducive to pedestrian, bicycle, and other non-vehicular travel modes;
 - b. A density or lot coverage area that is higher than surrounding rural settlement areas;
 - c. Use of planned unit development concepts, such as compact development that is offset by open space;
 - d. A design that incorporates public spaces and promotes social interactions;
 - e. A mixture of uses including non-residential and community facilities, and possibly residential;
 - f. Central focal points or public spaces serving the entire area;
 - g. A pattern and scale of development that complements traditional patterns and uses in Regional Growth Areas; and
 - h. Provision for park-and-ride commuter parking lots, transit access, and travel information services.
6. Municipalities with Interchange Areas are encouraged to promote creation and adoption of an Official Map, per 24 VSA §4421, to provide a legal means of creating an interconnected network of streets, walkways, and other public facilities or amenities on land designated as interchange development areas. Concepts employed in Master Plans and the Official Map should employ traditional streetscape patterns and designs deemed compatible with existing Regional Growth Areas.
7. Principal retail establishments must be located in Regional Growth Areas to minimize the blighting effects of sprawl and strip development along major highways, to protect the vitality of our villages and downtowns, and to maintain rural character.

Interchange Area Policies - Specific

The characteristics of each of the three Interchange Areas designated in this Plan are not identical. While all of the Interchange Areas serve as transfer points between the interstate (limited-access roads) and state highways (connectors to villages and outlying countryside), the physical and economic landscapes for these areas is different. Some areas are largely undeveloped open spaces without public infrastructure, especially sewer or water. Other areas are situated at or near prominent vistas or scenic areas and are visually sensitive to certain types of development. Yet other interchanges are experiencing new commercial or industrial development on what is or was farmland. Some interchanges are relatively flat and have greater potential to accommodate appropriate development than others that are steep or have other physical development constraints such as

aquifers and wetlands. Lastly, local community planning desires and attitudes suggest that not all land use goals and policies should be universally applied.

It is the finding of TRORC that in order for this Plan to address each Interchange Area specifically, supplemental policies have been developed for each of these interstate interchanges. The policies in each Interchange Area section apply specifically to that interchange indicated.

Quechee (Hartford) Interchange (I-89, Exit 1)

Exit 1 of Interstate 89 accesses U.S. Route 4 and connects travelers and commerce west to Woodstock, Killington, Rutland, and beyond, and east to White River Junction and Interstate 91. Route 4 is one of the few east/west highways spanning the narrower width of

and along highways leading to them are powerful magnets for non-residential uses, this often competes with and erodes Regional Growth Areas; the proximity of large parking lots adjacent to high-volume highways is an attractive force to consumers and businesses.

Policies: Interchange Areas - General

Policies

The following policies apply to all designated Interchange Areas:

1. 1. Land use activities and public or quasi-public investments planned for Interchange Areas that have the effect of eroding the socioeconomic vitality of downtowns are incompatible with this Plan. Land uses planned for Interchange Areas must be of a type, scale, and design that complement rather than compete with uses that exist in Regional Growth Areas. Unless otherwise noted in the following Interchange Specific Policies, appropriate uses include residential, highway-oriented lodging and service facilities, trucking terminals, light industrial, offices, truck-dependent manufacturing, and park-and-ride commuter lots. No use should impose a burden on the financial capacity of a town or the state to accommodate the growth caused by the project.
2. Development planned for Interchange Areas must be constructed to:
 - a. Complement the design principles and standards reflected in this Plan;
 - b. Promote the most appropriate land uses as determined through a locally sponsored planning process involving affected landowners, municipalities, and TRORC;
 - c. Minimize visual impacts from roadways through screening and landscaping and maintain a high standard of scenic amenities for visually sensitive areas with due regard to impacts on neighboring land uses and highway users; and
 - d. Encourage planned unit developments.
3. Master plans for each Interchange Area should be completed. Such Plans should be conducted locally as part of each local planning commission's ongoing planning program in cooperation with landowners, TRORC, and other affected parties. Work should focus on creating an integrated site plan and design plan that serves as a means of addressing the potential conflicts or problems noted above. Elements that the Plan should include are:
 - a. Access management controls;
 - b. Pedestrian amenities;
 - c. Transit access;
 - d. Parking;
 - e. Energy efficiency;
 - f. Utilities/public services;
 - g. Outdoor lighting standards;
 - h. Landscaping and screening;
 - i. Signage; and
 - j. Open space conservation.
4. Master Plans must serve as the foundation for the identification of the highest and best use of these areas and should provide a framework for future development. Incremental and uncoordinated development inconsistent with Master Plans for each of the Interchange Areas is discouraged.

Goals, policies, and recommendations continued on next page

Interchange Areas

Lands that are in close proximity to interstate interchanges are viewed as prime areas for development by some due principally to their ease of public access and favorable site conditions. In this Region, interstate interchanges are located in the towns of Bradford, Fairlee, Hartford, Hartland, Newbury, Norwich, Sharon, Randolph, Royalton, and Thetford. However, not all of these interchanges are designated as Interchange Areas as land use areas in this Plan.

Despite the benefits of interstate travel and the fact that the interchanges are important transfer points for traffic entering and exiting the Region, there are potential pitfalls to developing these areas. Increased traffic congestion and safety issues resulting from interchange developments can unacceptably decrease the level of service of roadways. One example, the Quechee interchange (I-89, Exit 1), contains acres of developable land located within a mile of the intersection of two interstate highways. This places this interchange at a high degree of vulnerability. Local development decisions made without adequate regard to preserving mobility will degrade the functionality of the public investments. An illustration of this consequence is on Interstate 89 at Exit 20, a strip of commercial development in nearby West Lebanon, NH, where access on and off the interstate for traveler services has been negatively impacted due to traffic and over development. Other typical problems associated with improper traffic management and development at interchanges include:

1. The creation of numerous curb cuts (new driveways) surrounding the interchange to access new development that are permitted incrementally on a case-by-case basis without due regard to an overall plan for the area;
2. The eventual existence of high traffic generators in the immediate vicinity, which cause degradation of roadway intersections, the need for signalization, lower travel

- speeds, and extensive queuing of vehicles;
3. Inadequate planning for pedestrian accesses between developments and loss of significant farm land or access to such land;
4. Erosion of cultural, social, and economic values of the traditional town center or village settlement due to a dislocation or redistribution of key uses into the area; and
5. Fragmentation of land parcels in such a manner as to preclude future access or interior roads to properties more removed from the right-of-way; and
6. Unnecessary loss of scenic qualities resulting from insensitive land development.

Lands at interchanges in Bradford, Fairlee, Newbury, Norwich, Sharon, and Hartford (White River Junction) are considered part of an existing Regional Center, Mixed Use Area or Village Settlement and are therefore not identified as separate Interchange Areas in this Plan. Lands at interchanges in Bradford, Newbury and Royalton (in part) are located within Industrial Areas. Lands at interchanges in Thetford and Hartland are in Rural Areas. The interchanges in Quechee, Randolph, and Royalton, are physically separate from a Regional Growth Area, being in some cases two or three miles away. Because this Plan and state planning policy affirm Regional Growth Areas as the principal areas for service, retail, civic, and institutional uses, it is in the interest of the Region for these areas to continue to serve these vital functions. Conversely, Interchange Area development, with its different focus, should not be promoted to the detriment of Regional Growth Areas or the public investments made therein.

TRORC respects the right of municipalities to plan for growth in these areas. At the same time, TRORC believes that given the considerable public investment in the interstate highway system and Regional Growth Areas, and the significant public exposure to such areas, these interchanges also need to be evaluated from a regional perspective. Land around interchanges

Industrial Areas

Industrial parks and districts are a way to encourage economic growth and high-wage businesses to locate in the Region without adversely affecting neighboring land uses. Industrial uses can produce off-site impacts, such as noise, that can be mitigated if these businesses are located in areas designated specifically for industrial development and job growth. Commonly, Industrial Areas are located where there is direct access to transportation via major roads and/or rail, three-phase power, and other municipal infrastructure. These areas

may include other commercial uses, provided that those uses are not more appropriate within Regional Growth Areas. There are Industrial Areas identified in seven communities in the TRO Region.

Policies: Industrial Areas

Policies

1. Industrial development and uses are the primary use within an Industrial Area, provided that the scale and intensity of the development does not have an undue adverse impact on the surrounding area.
2. In addition to industrial development, commercial development (excluding principal retail establishments), services, and offices may be appropriate, provided these are not the dominant uses.
3. Traffic and pedestrian safety must be a strong consideration in the design of development within Industrial Areas, particularly those areas with a large trucking component.
4. Principal retail establishments shall not be located in Industrial Areas, but secondary retail may be.

Mixed-Use Areas

Given the regional need for increased housing and local needs for commercial establishments that are not best suited to core areas due to their impacts, low value, or large use of land, a Mixed-Use Area can supply needed space for such along state highways without creating sprawl.

Policies: Mixed-Use Areas

Policies

1. Light industrial development may be appropriate, provided that the scale and intensity of the development does not have an undue adverse impact on the surrounding area.
2. Multi-family housing at several units per acre or greater is appropriate in this area.
3. Commercial uses that include land intensive uses, lumberyards, repair services, warehouses, kennels, and indoor recreation are appropriate in this area.
4. Principal retail shall not be permitted in this area.

Hamlet Areas

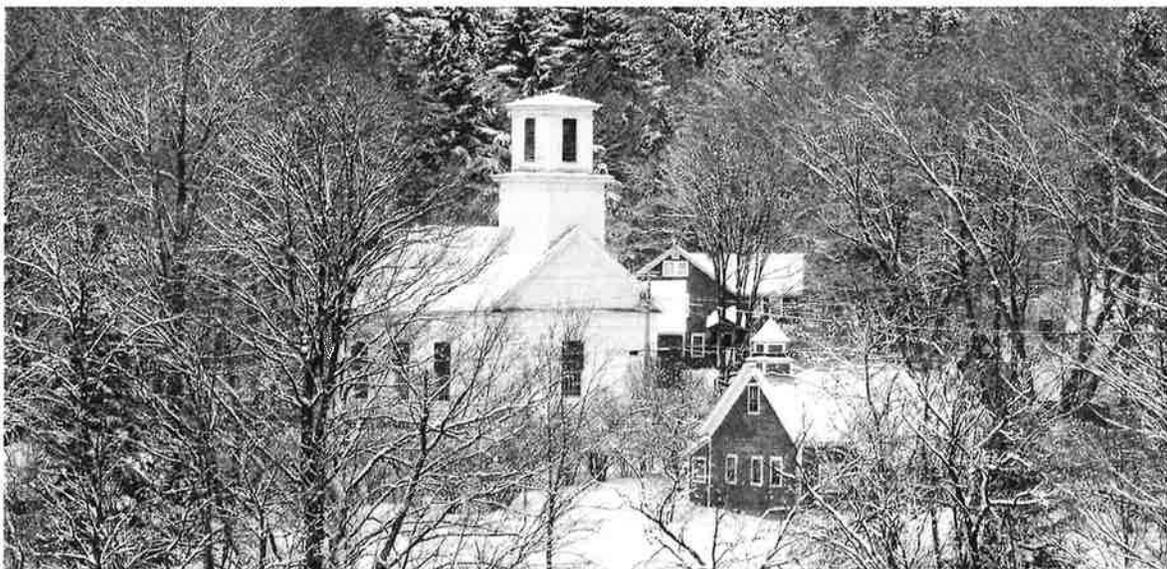
Hamlet Areas were significantly more prevalent throughout the communities in the TRO Region in the past. Presently those Hamlets that remain consist of groupings of buildings that are generally residential in nature. Hamlets are significantly smaller in scale than Village Settlements. They historically have served as the location for single-family homes, with a few stores and businesses supported primarily by local residents. Hamlets are not regional markets or trade centers. These areas generally do not contain a community water supply or sewer

system. Minor community facilities and services sometimes are located in these areas. Hamlet Areas in the Region are Bridgewater Center, Bridgewater Corners, Corinth, East Barnard, East Bethel, East Braintree, East Brookfield, East Granville, Gaysville (Stockbridge), Locust Creek, North Pomfret, North Thetford, North Tunbridge, Stockbridge Central School, Thetford Hill, Vershire, Vershire Center, Waits River (Topsham), West Braintree, West Bridgewater, West Brookfield, West Hartford, West Newbury, West Topsham, and areas immediately adjoining such areas.

Policies: Hamlet Areas

Policies

1. The density of development in Hamlet Areas must reflect the existing settlement patterns, physical land capability, and availability of utilities for expansion. Hamlet Areas should support primarily single- and two-family homes and residential-scale small business enterprises (including principal retail establishments) that fit the context of the immediate area and are meant primarily to serve local markets
2. Major traffic thoroughfares through Hamlet Areas must be planned with traffic calming elements.
3. New buildings should maximize allowable density. Where unusual natural features, soil limitations, or special resources (including high value agriculture land) are identified, use of cluster development concepts is encouraged to protect such resources from unnecessary development.
4. Existing postal facilities, and similar governmental offices, must be retained in Hamlet Areas and not be relocated into Rural Areas.



West Brookfield | © John Knox

Village Settlements

Village Settlements are the most Regional Growth Areas in the TRO Region. Village Settlements normally consist of a mix of uses at medium to high densities. Density in Village Settlements varies based on the availability of municipal water and sewer. Those Village Settlements that do not have water or sewer are prime candidates for future infrastructure investments. Unlike Regional Centers and Town Centers, Village Settlements are not regional markets or trade centers and typically serve a local clientele. The Region's Village Settlements are core areas in Barnard, Braintree, Bridgewater, East Corinth, East Randolph, East Thetford, East Topsham, Granville, Hancock, Hartford Village, Hartland Four Corners, Hartland Three Corners, Newbury, North Hartland, Pittsfield, Plymouth Union (Plymouth), Pond Village (Brookfield), Post Mills

(Thetford), Quechee, Randolph Center, Royalton Village, Sharon, South Pomfret, South Strafford, South Woodstock, Stockbridge, Strafford, Taftsville, Thetford Center, Thetford Hill, Tunbridge, Tyson (Plymouth), Vershire, West Fairlee, and West Woodstock.



Bridgewater Center | © John Knox

Policies: Village Settlements

Policies

1. Village Settlements should support a mixture of single-family, two-family, and multi-family structures at the highest densities possible given existing sewer and water capacity. Village Settlements that have neither public water nor sewer should plan for that the maximum densities that can be supported by the soils present, in order to avoid ground and surface water contamination while also keeping the area denser than surrounding rural areas.
2. Conversion of larger older homes (particularly those with historic merit) to newer, more economical uses, such as offices and multi-unit housing, is consistent with this Plan. See the Historic Resources section for more information.
3. Principal retail establishments, services, tourist businesses, lodging, public facilities, and business and industrial enterprises of a scale and design that fit the context of the area are appropriate for this area.
4. Local capital planning programs and public investment strategies must support renovation of existing buildings and in-filling within Village Settlement Areas.
5. New development must not place undue burdens on municipal or regional facilities, utilities, and services, including transportation systems.
6. New development shall be planned to be reasonably compatible with existing development, preserve buildings of historic, architectural, or engineering significance, and not unduly impact the character of the area.
7. Long-range planning for the provision of public services in these areas to accommodate future growth is encouraged.
8. Planned and existing services should be coordinated so that the future expansion of services can be more accurately evaluated.
9. Highway investments within Village Settlements must include pedestrian circulation, traffic calming, and streetscaping.

Town Centers

Town Centers are less urban than Regional Centers but also contain a concentrated mix of uses at a high level of density. They are those areas where central public utilities for water and sewer are available and where there exists a central location for commercial activities, schools, and cultural and civic activities for the town and the surrounding communities. In our Region, Designated Downtowns, Designated Villages, and a Designated Growth Centers are included in this land use area. Town Centers are found in Bethel, Bradford, Chelsea, Fairlee, Norwich, Randolph, Rochester, South Royalton, Wells River, Wilder, and Woodstock.



Woodstock Village from Mt. Tom | © John Knox

Policies: Town Centers

Policies

1. Town Centers shall support a mixture of single-family, two-family, and multi-family structures at the highest densities possible given existing sewer and water capacity and community character.
2. Commercial uses (including principal retail establishments), services, offices, wholesale business, industry, transport facilities, and community facilities and services are appropriate to locate in these areas.
3. Intense growth is appropriate in Town Centers when a complete complement of public services such as water, sewer, and highways is available. To accommodate additional development, continued maintenance or expansion of such facilities must occur.
4. Local capital planning programs and public investment strategies must encourage renovation of existing buildings and in-filling within Town Centers.
5. Principal retail establishments must be located in Regional Growth Areas to minimize the blighting effects of sprawl and strip development along major highways, to protect the vitality of our villages and downtowns, and to maintain rural character.
6. Conversion of larger older homes (particularly those with historic merit) to newer, more economical uses, such as offices and multi-unit housing, is consistent with this Plan. See the Historic Resources section for more information.
7. New development shall be planned to be reasonably compatible with existing development, preserve buildings of historic, architectural, or engineering significance, and not unduly impact the character of the area.
8. Postal facilities and similar governmental offices should be located where other public services are available or planned.
9. Highway investments within Town Centers must give significant consideration to multi-modal transportation, and include pedestrian circulation, traffic calming, and streetscaping.

Regional Center

Regional Centers are a region's largest urban areas, where development is highly concentrated with a diverse mix of uses. They are areas where public sewer and water utilities exist, transportation infrastructure is capable of handling significant volumes of commuting and commercial traffic, a public transportation system provides options, and there are intermodal opportunities present. In order to achieve the level of density appropriate for a Regional Center, buildings are often multi-story, with mixed uses – particularly in the core of the area. People use Regional Centers for their variety of employment and business opportunities, governmental and judicial functions, hospitals, schools, and cultural and civic activities. White River Junction is the only Regional Center in our Region. Our only State Designated Growth Centers and a Designated Downtowns are included in this land use area.



White River Junction | © Kevin Geiger

Policies: Regional Centers

Policies

1. Regional Centers should support a mixture of single family, two-family, and multi-family housing and should have the highest densities in the Region.
2. Commercial land uses, services, offices, wholesale business, industry, transport facilities, and community facilities and programs that serve regional needs and markets shall be located in Regional Centers.
3. Intense growth in Regional Centers is appropriate when a complete complement of public services such as water, sewer, and highways are available. To accommodate additional development, continued maintenance or expansion of such facilities must occur.
4. Local capital planning programs and public investment strategies must encourage renovation of existing buildings and in-filling within Regional Centers.
5. Retail establishments that provide goods and services to a regional clientele must be located within or immediately adjacent to Regional Centers and Town Centers to ensure that the vitality of these economic centers is maintained.
6. Conversion of larger older homes (particularly those with historic merit) to newer, more economical uses, such as offices and multi-unit housing, is consistent with this Plan. See the Historic Resources section for more information.
7. In areas containing structures and buildings of architectural or engineering significance, new development must be planned to be compatible with existing development and to not unduly impact the general and special character of the area.
8. Major developments like large governmental, medical, and commercial buildings must be located in Regional Centers where utilities, facilities, populations and are concentrated.
9. Highway investments within Regional Centers must include multi-modal transportation, pedestrian circulation, traffic calming, and streetscaping.

Growth Centers, Downtown Designation, and Village Center Designation

In 1997, Vermont enacted the Historic Downtown Development Act (24 VSA Chapter 76A) to recognize local efforts to revitalize traditional villages and downtowns.

Under the law, towns may apply to the Vermont Downtown Development Board for designation as a downtown, village center, or growth center. If designated, commercial property owners in downtowns and villages are eligible for state tax credits for rehabilitation of historic structures, facade improvements, and building code improvements; if the tax credits cannot be used by the property owner, they may be sold to banks. In addition to tax credits, designated villages and downtowns receive priority consideration in Vermont's Municipal Planning Grant Program and the federal funding programs guided by Vermont's HUD (U.S. Department of Housing and Urban Development) Consolidated Plan, including the Vermont Community Development Program.

It is the intent of TRORC to preserve and encourage development of the Region's downtowns and village centers; to encourage investment in housing, historic preservation, and transportation (including parking facilities); and to reflect traditional settlement patterns. Furthermore, TRORC believes that dynamic planning programs focused on downtowns and villages will strengthen the vitality of our community centers and protect the rural landscape.



Theford | © John Knox

Goals and Policies: Overall Land Use

Policies

The land use policies apply throughout the Region. Subsequent sections on individual types of land use areas have policies specific to each of them.

1. Any public investment in public and private housing for the elderly, disabled, and low- or moderate-income families shall be directed into Regional Centers, Town Centers, and Village Settlements, or areas within one mile of these along state highways and transit routes, and away from unsettled rural areas where no services exist.
2. Principal retail establishments must be located only in Regional Growth Areas to minimize the blighting effects of sprawl and strip development along major highways, to protect the vitality of our villages and downtowns, and to maintain rural character.
3. Development of federal or state governmental offices distant from and outside Regional Growth Areas contributes to increased traffic, scattered development, and costly public services. Such a pattern of development is incompatible with the goals and policies of this Plan.

B. Future Land Use Areas

For the purposes of this Plan, six types and four subtypes of Future Land Use Areas have been established and identified. These Areas have characteristics that identify them within the Region. They are designed to accommodate future growth based on the capacity of infrastructure and suitable land without threatening critical resources or creating sprawl. These Areas are:

- Regional Growth Areas
 - Regional Center
 - Town Centers
 - Village Settlements
 - Hamlet Areas
- Industrial Areas
- Mixed-Use Areas
- Interchange Areas
- Rural Areas
- Forest-Based Resource Areas

The Region's Land Use Areas are depicted on Map 4, the Future Land Use Areas map that is included in this Plan. The Regional Center, Town Centers, Village Settlements, Forest-Based Resource Areas, Mixed-Use Areas, Industrial Areas, and Interchange Areas are identified by boundaries. Hamlet Areas are identified by center points; when making land use decisions using the policies in this Plan, Hamlet Areas must include the locally recognized extent of the hamlet as it

is designated in the appropriate town plan. Rural Areas are the remaining lands in the Region.

Regional Growth Areas

Growth throughout the Region must be balanced with a respect for the traditional patterns of development that make our Region distinct (these patterns are supported by Vermont's planning goals) and the need to adapt to an ever-changing world. To sustain both rural and more developed core areas, major growth or investments must be channeled into existing settlement centers or development immediately adjacent to such centers.

Regional Growth Areas represent areas of concentrated mixed use at varying scales and with differing mixes of uses. These areas are either served by public facilities (such as sewer, water, and public transit) or are potential locations for future infrastructure investments that will encourage growth and vitality. Depending on their scale and location, these areas generally include a diverse mix of services, businesses, and housing opportunities for our citizens.

Acknowledging that Regional Growth Areas range from urban to rural, the Regional Plan differentiates these areas into the four subtypes mentioned above and detailed below.

live and how it does, or does not, support our economy, reduce energy use, encourage a sense of community, and protect our natural resources.

As our Region's population ages, the appeal of owning a house in the country can change. For many, the cost and effort it takes to maintain a larger home or to travel to locations that offer goods and services can be a burden. We must recognize that, as we move forward, planning will need to provide a greater diversity of housing in areas that are affordable, walkable, and vibrant.

Our community cores and roads were built along, and sometimes in, streams, wetlands, and rivers because these are flat areas. This was practical in some ways, but ignored the fact that these are also areas prone to flooding, sometimes with disastrous results. As we have continued to build and create more impervious surfaces, and the climate has shifted to one with more extreme rains, the specter of flooding now must be taken into account as we look at our compact centers and where they can safely grow.

Our forests are an important component of our Region. They represent a significant store of natural resources, are a driver for economic activity, and provide us with a backdrop that is distinctly rural. However, the landscape shift of open lands reverting to forest over the last century has ended, and we are now starting to lose forest again as a state, with 1,500 acres a year being converted to development or open land. We continue to fragment the forest we do have with subdivisions, reducing the natural functions of large, contiguous sections of forested land that are vital to many plant and animal species. In planning for the future, we need to consider the places where we have already impacted forest integrity beyond repair and the places where good forests remain.

These background issues have been considered as part of the development of this Plan. We continue to strive to move planning forward, to adapt to changes in the Region, and to support our communities while remaining consistent with Vermont's land use goals.

Goals and Policies: Overall Land Use

Goals

The land use goals within this section represent the foundation of the planning and development for the Region. These goals are intended to be applied throughout the Region.

1. Development patterns and their related transportation systems promote public health and reduce energy use and greenhouse gas emissions.
2. Energy-efficient and affordable housing choices are expanded.
3. Land use planning and regulation maintains our quality of life, environment, and economy.
4. Intensive development occurs only where adequate public services and facilities are currently available or planned.
5. The health of residents is improved by investing in clean water, soil, and air, and safe and walkable neighborhoods.
6. The patterns of development in the TRO Region remain consistent and compatible with the goals of V.S.A. Title 24, Chapter 117, §4302.

Goals and policies continued on next page

LAND USE



West Fairlee | © John Knox

A. Background Issues

For almost two decades the TRO Region has been in a post-growth period following a time of rapid economic growth and profound changes to its landscape, spanning 30 years (1970-2000).

During that time planning focused on mitigating the impacts of growth. While managing the impacts of uncontrolled growth remains an important part of the TRORC Regional Plan, the key issues that must be considered when planning for the Region's future have changed.

As the Region looks to the future, it will need to adapt land use policy to the changing business environment by supporting existing businesses, encouraging entrepreneurial development, investing in our existing downtowns, improving infrastructure (particularly within our villages and downtowns), and strengthening those things that make Vermont unique (such as the arts, and our forest-related, agricultural, and other value-added products). The impact of broadband, and online sales is changing the way we access

entertainment, commute to work, buy goods, and even receive services. The generation of renewable energy and the coming electrification of our transportation and heat systems will engender new services and facilities.

With all of this change, we still hope to meet one of the fundamental guiding goals of state land use law, which is to further the traditional pattern of development so as to maintain the historic settlement pattern of compact village and urban centers separated by rural countryside. While this model is greatly responsible for sustaining Vermont's rural character, it has its challenges as this pattern was built when Vermont's countryside had an agrarian lifestyle where residents did not travel much and did not have cars and commute. For many, the luxury of having a home in a rural setting is why we choose to live here. But our choice to live in more rural areas means that we must use cars and trucks to get to work, access goods and services, and be part of our communities. When we plan for our future, we will need to consider where we

From: Mary Layton <marydlayton@gmail.com>
Sent: Friday, June 21, 2019 10:05 AM
To: Miranda Bergmeier
Subject: Affordable Housing Fund

Follow Up Flag: Follow up
Flag Status: Flagged

Memorandum

To: Norwich Selectboard
From: Mary Layton
CC: Herb Durfee, Miranda Bergmeier
Subject: Discussion/Action Affordable Housing Fund

This is an update for the rest of the SB, and a suggested action of raising more money for the fund. I am quite interested in the latter via a connection with local charitable foundations. Roger may have other ideas for actions as well.

Roger and I attended the last AHSC meeting. I came away with a more informed idea of the possible scope and time frame for the development of 50 units. It is my perception that it will be actually very difficult to pull this off and the time frame will be five to ten years. The message from TRORC that multi-use development is not preferred near the interstate exchange is also an impediment to developing affordable units.

In terms of public perception perhaps the idea that big box stores and strip development are not wanted by the Town or the TRORC but also not really feasible would alleviate some anxiety about the topic. I think a modest, appropriate, and attractive mix of retail and affordable housing units might be able to be built and might be supported by the voters. Listening sessions will help to see if this is the case.

Kathleen Shepherd mentioned a "circle of virtue" which referred to citizens who will support the development of affordable housing over time, and be vocal in their support. Positive citizen support in the form of correspondence, public comment at SB meetings, a petition, comments at listening sessions, and list serve posts would all be helpful.

Our 2018/2019 BHAG of 100 affordable housing units within 3 years is unrealistic as far as scope and feasibility, but the need for affordable housing is at least on our list of important goals.

Mary

10a

Miranda Bergmeier

From: Mary Layton <marydayton@gmail.com>
Sent: Thursday, June 20, 2019 2:52 PM
To: Miranda Bergmeier
Cc: Herb Durfee; Miranda Bergmeier
Attachments: TON Master Financial Policy Purpose and Goals.docx; Town of Norwich Master Financial Policy.docx

Memorandum:

To: Norwich Select Board
From: Mary Layton
CC: Herb Durfee, Miranda Bergmeier
Date: June 20, 2019

Hello All,

I would like to include a review of the Master Policies framework as an agenda item for the June 26th meeting. I have written a draft Introduction of Purpose and Goals, and have put together a draft Table of Contents that includes existing Select Board policies, Town Manager Protocols, and “gaps” that have been fit into the VLCT framework. This is the overview of the framework and gives an idea of the policy creation and revision that is the next step. The introduction notes that the policies and protocols included in the framework are subject to revision, and that additional policies or elimination of defunct policies is intended.

As a first step that would happen at the June 26th meeting I would move to adopt the draft Table of Contents, and the Introduction. This puts the framework in place. I do not expect this to be time consuming.

The second step is to update the policies and to double check to make sure everything is under the correct heading. I will continue to work on this with John and Herb and will try to get draft policy updates done by the July 10th SB meeting so they can be voted on.

Note that Section 8, “Trustees of Public Funds Investment Policy” is the responsibility of the Trustees, and also is recommended for a Master Policy by VLCT. I have contacted the Trustees by letter to see if they would like to develop a policy.

If it is agreed to add this item to the agenda I would like the enclosed documents to be included in the packet.

Mary Layton

Town of Norwich Master Financial Policy Purpose and Goals

Introduction

Statement of Purpose

The Town of Norwich has in recent years evolved from being managed by a citizen run Select Board to the present day Town Manager form of government. The role of the Select Board has shifted away from day to day management to that of oversight and policy development. A variety of Select Board financial policies and Town Manager protocols have been developed over the years, and are compiled within this framework to provide a central and comprehensive reference point.

Written, adopted financial policies have many benefits, such as assisting the Select Board and Town Manager in the financial management of the Town, saving time and energy when discussing financial matters, engendering public confidence, and providing continuity over time as elected officials and staff members change. While these policies and protocols will be amended periodically, they will provide the basic foundation and framework for many of the issues and decisions facing the Town. They will promote sound financial management and assist in the Town's stability, efficiency, and effectiveness.

This framework was developed and adopted from the VLCT document, "Dormont Financial Policies and Procedures-Final Version, 2013". Selected financial policies were reviewed by the Norwich Select Board, Town Manager, and Finance Committee in 2019 before the compilation of the document. Others are incorporated pending review. It is expected that policies will be reviewed, edited, added, and deleted as necessary on an ongoing basis within the Master Policy Framework.

Financial Goals

- Ensure the financial integrity of the Town
- Manage financial assets in a sound and prudent manner
- Improve financial information for decision makers including policy makers and management
- Maintain and further develop programs to ensure the long term ability to pay costs necessary to provide the level and quality of service required by the citizens
- Maintain a spirit of openness and transparency while being fully accountable to the public for the Town's fiscal activities

Town of Norwich Master Financial Policy

Table of Contents

Introduction

Purpose and Goals.....

Section 1

Accounting, Auditing, and Financial Report Policy.....

#31 Town of Norwich, Vermont Capital Assets Accounting Policy, 4/13/2005

No policies specific to the annual audit or Financial Reports exist. The "Dormont Policy is succinct and may be easily adopted.

Section 2

Purchasing Policy.....

Town of Norwich Purchasing Policy, under development, 2019

Town Manager Purchasing Protocol, Neil Fulton, 4/22/2013

Section 3

Cash Receipts, Petty Cash, and Returned Check Policy.....

Town Manager Protocol 4/22/2013, Department Revenues, page 1, #3

Town Manager Protocol 4/22/2013, Voided Checks, page 2, #4

Town Manager Protocol 4/22/2013, Accounts Payable and Interim Checks, page 2, #5

Town Manager Protocol 4/22/2013, Employee Reimbursement, page 2, #7

Town Manager Protocol 4/22/2013, page 3, #8 Petty Cash

Recommendation by Herb Durfee to eliminate petty cash meeting 6/13/2019

Section 4

Credit Card Policy.....

Town Manager Protocol 4/22/2013, page 2.

Section 5

Debt Management Policy.....
#36 Town of Norwich Debt Management Policy, 9/26/ 2007

Section 6

Capital Program and Budget Policy.....
#39 Town of Norwich Financial Policy #1: Budget Management 3/19/2010, under review
2019

#43 Annex to Select Board Financial Policies Defining Fund Types, 5/12/2010

#35 Norwich Conservation Trust Fund Policy and Procedures, developed and "passed by
the Conservation Commission, reviewed by the Select Board on May 20, 1996.

#69 Select Board Appropriation Request and Disbursement Policy, Adopted 2014, no
signature page is available, effective 7/1/205.

#12 Disposal of Town Property Memorandum, 5/23/1995

Town Manager Protocol 4/22/2013, Grant Applications and Administration, page 1,
#1.

Town Manager Protocol 4/22/2013, Encumbrances and Year End Purchasing, page 1
#2.

Town Manager Protocol 4/22/2013, Forms, page 3, #9

Section 7

Investment Policy.....
#61 Investment Policy, 6/21/2017

Section 8

Trustees of Public Funds Investment Policy.....
No policy at this time. New policy is responsibility of Trustees.

Section 9

Fraud Prevention Policy.....
#70 Fraud Prevention Policy, 11/1/2018

Section 10

Fund Balance Reserve Policy.....

#48 Select Board Financial Policy #2 Undesignated Fund Balance, 10/XX/2017

Section 11

Balanced Budget Policy.....

No policy at this time

Section 12

Gift Policy.....

Select Board Financial Policy #3:Grants, Gifts, and Special Funds, 3/10/2010



DRM : Downs
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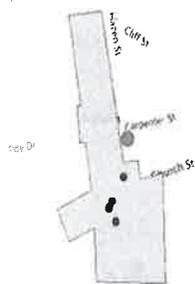
May 24, 2019

William J. Dodge
Tel: (802) 864-8395
wdodge@drm.com

To: Parties Entitled to Notice Pursuant to 30 V.S.A. § 248a(e) and Procedures Order
Re: AT&T Mobility: Attachment on Replacement Utility Pole
Lary Lane, Norwich, Vermont 05055
AT&T Site Name: Small Cell RCTB_00077: Lary Lane, Norwich, Vermont
60-DAY ADVANCE NOTICE

Dear Recipients:

Our firm represents New Cingular Wireless PCS, LLC d/b/a AT&T Mobility (“AT&T”) in connection with a proposal to replace an existing utility pole owned by Green Mountain Power (“GMP”) located in the right-of-way along Lary Lane, Norwich, Vermont with a new taller utility pole (the “Pole”), and install wireless communications equipment on the Pole (the “Facility” or “Project”). The Pole has the following coordinates: latitude 43.714818°N and longitude 72.307754°W. AT&T refers to this project as RCTB_00077 (the “Property” or “Site”). The Pole sits in a parking lot area off of Carpenter Street / Lary Lane, and is located just outside the limits of the Norwich Village Historic District where most of the municipal civic buildings and several shops are located (see map from the Agency of Commerce and Community Development atlas, below).



Pursuant to 30 V.S.A. § 248a, this letter is intended to provide 60 days advance notice to Town of Norwich Selectboard and Planning Commission (the “Town”), Two Rivers-Ottauquechee Regional Commission (the “RPC”), and adjoining landowners, that AT&T intends to submit a petition to the Vermont Public Utility Commission (“PUC”) for approval to install the Facility at the Site. Attached as Exhibit A to this notice is a statement that itemizes the rights and opportunities available to the Town and RPC pursuant to §§ 248a(c)(2), (e)(2), (m), (n) and (o).

This letter is also being filed electronically with the PUC via its ePUC system, which will provide notice to the Vermont Agency of Natural Resources, the Vermont Department of Public Service, the Vermont Division for Historic Preservation, and the Vermont Agency of Transportation (collectively, the “Advance Notice Parties”).

This notice provides a description of the Facility and its anticipated impacts. This notice also describes the process for PUC review of petitions submitted under § 248a, in addition to the rights of the regional and local planning commissions, municipal legislative bodies, and adjoining landowners to comment on the Project.

AT&T’s petition will be filed pursuant to the PUC’s “Sixth Amended Order implementing standards and procedures for issuance of a certificate of public good for communications facilities pursuant to 30 V.S.A. § 248a,” dated September 21, 2018 (the “Procedures Order”). The Procedures Order, as well as more information concerning review of communications projects under 30 V.S.A. § 248a, is available at the PUC’s office in Montpelier and on its website: <http://puc.vermont.gov/>.



II. Project Description

AT&T is licensed by the Federal Communications Commission to provide multiple technologies in Vermont, including long-term evolution wireless broadband internet service. AT&T is improving and enhancing its voice and data network in the state through deployment of technology generally known as “small cells,” used to address capacity issues at specific locations. Small cell technology is especially helpful to remedy connectivity issues experienced in more heavily populated areas or during certain high network traffic periods. The Project will improve capacity in and around Williston Road in South Burlington and the surrounding areas.

The proposed Facility will consist of the following components:

- A. Replacement of the existing 32’ 4” aboveground level (“AGL”) utility pole with a new 38’ 6” utility pole;
- B. Install one (1) canister antenna, measuring approximately 2’ 1” in height and 10”, to be mounted on top of the Pole, with the top of the antenna extending to 41’ 7” AGL;
- C. Install a Pole-mounted equipment cabinet measuring approximately 3’ 3” in height and 1’ 11” in width at a height of 12’ 9”AGL; and
- D. Appropriate grounding and utility connections under the Pole to be used in connection with operation of the wireless facility.

Each feature of the Facility is described and depicted in more detail on the Site Plan attached as Exhibit B. The Project will result in less than 10,000 square feet of permanent earth disturbance. The width of the Pole will not be more than 20’ wider than the existing pole, and will have an overall height less than 200’ AGL. Consequently, the Project qualifies as a “Project of Limited Size and Scope,” as defined in 30 V.S.A. § 248a(b)(3).

In selecting to undertake the Project, AT&T has analyzed whether there are existing structures in the area to be served that could be used for the antennas and equipment without replacing the existing pole. For various reasons, none of the existing telecommunications facilities in the area will allow AT&T to meet its objectives of meeting its customers’ needs and planning for future growth of the network in the area to be served by the Project.

III. Process for Review of Communications Facilities under 30 V.S.A. § 248a

Pursuant to 30 V.S.A. § 248a, the PUC may grant a certificate of public good for construction or installation of one or more telecommunications facilities that are to be interconnected with other telecommunications facilities proposed or already in existence if, after review of the project, the PUC finds that the facilities will promote the general good of the state consistent with the policies relating to providing improved telecommunications technology to all Vermonters articulated by 30 V.S.A. § 202c(b).

Among the criteria considered by the PUC in evaluating each facility under 30 V.S.A. § 248a is whether the project is consistent with the recommendations of selectboards, municipal planning commissions and regional planning commissions. In turn, those recommendations can be based on town / regional plans, as well as telecommunications provisions in local zoning bylaws or a stand-alone ordinance. 30 V.S.A. § 248a(c)(2). Based on a review of the relevant municipal and regional planning documents, AT&T believes that the Project is consistent with the applicable substantive criteria.

a. Norwich Town Plan

The Norwich Town Plan, adopted July 11, 2018, recognizes that cell phone service and high-speed internet access have become a necessity in our lives, just as electricity and the telephone were in the early part of the last century. Town Plan at 8-7. “The availability of cell service (which often also delivers Internet access) and broadband internet access are services providing important benefits to residents including safety and security, education, economic, health monitoring, entertainment, etc.” Town Plan at 8-7. The Town Plan indicates that the Town should continue to support these services while minimizing the adverse visual impact of the towers, antennas and wires to the extent possible.

To that end, the Project furthers these goals and objectives insofar as the Facility will enhance the availability and quality of AT&T wireless coverage in the Town. The use of a slightly taller utility pole within the existing parking lot as the support structure achieves the Plan’s goal of locating additional telecommunications infrastructure while minimizing the aesthetic and environmental impact of such new development.

b. Two Rivers-Ottawquechee Regional Plan

The Project is consistent with the Two Rivers-Ottawquechee Regional Plan, adopted July 26, 2017 and effective August 31, 2017 (the “Regional Plan”). The Regional Plan recognizes that wired and wireless telecommunications have become increasingly important to the economic needs of the residents and businesses in the region. Regional Plan at 287. The Regional Planning Commission states that “[it] is highly supportive of efforts to expand broadband access provided that the infrastructure required does not have an undue adverse impact on the rural character of our communications.” Regional Plan at 290. The Regional Plan sets forth policies that focus on reducing the impacts of siting in the region, and identifies co-location on existing structures as an important tool. Regional Plan at 290-293.

The Project advances these goals by improving public access to reliable, high quality broadband internet service without adversely impacting the County’s scenic and environmental qualities. The small cell Facility will improve connectivity for AT&T users, and “densify” AT&T’s network service by bringing it “closer” to its users, in particular along the civic building area of the town (i.e., Main and Church streets). The Project also allows AT&T to prepare for implementation of newer technologies—including 5G capabilities, “smart cities” and new developments in the Internet of Things (“IoT”).

VI. Opportunity to Comment; Contact for More Information

As a recipient of this notice, you will be notified when the petition is filed with the PUC, which will be at least 60 days from the above date. Once AT&T’s petition has been accepted for filing by with the PUC, any interested person may submit comments and seek to intervene in the proceeding within 30 days of the receipt of the notification that the petition has been filed.

AT&T Small Cell
Lary Lane, Norwich, Vermont (RCTB_00077)
May 24, 2019
Page 4

Should you have any questions relating to the Project, please direct all inquiries and/or comments to Nicole Caplan-Mason at (978) 284-3906, or email to ncaplan@empiretelecomm.com. I can be reached at the telephone number and/or email provided in the letterhead above.

We look forward to your review and recommendation, and thank you in advance for your attention to this important project.

Sincerely,



William J. Dodge, Esq.

Enclosures

cc: Service List
Green Mountain Power Corporation, Attn: Kate McClallen (via electronic mail)
Nicole Caplan-Mason and Rossana Ferrante, Empire Telecom (via electronic mail)

MUNICIPAL AND REGIONAL REPRESENTATIVES / OFFICIALS

<p><i>Via US Mail and email</i> Norwich Selectboard Attn: John Pepper, Chair 300 Main Street, PO Box 376 Norwich VT 05055 Selectboard@norwich.vt.us</p>	<p><i>Via US Mail and email</i> Norwich Planning Commission Attn: Jaci Allen, Chair 300 Main Street, PO Box 376 Norwich VT 05055 manager-assistant@norwich.vt.us</p>
<p><i>Via US Mail and email</i> Two Rivers-Ottawaquechee Regional Commission Attn: Kevin W. Geiger, AICP, CFM, Senior Planner 128 King Farm Road Woodstock, VT 05091 kgeiger@trorc.org</p>	

ADJOINING LANDOWNERS (VIA U.S. MAIL ONLY)

<p>Parcel ID: 20-223-000 8 Carpenter LLC PO Box 621 Norwich, VT 05055-0621</p>	<p>Parcel ID: 20-214-010 Condo Katzenjammer Properties LLC c/o Bruce D. MacLeod PO Box 1623 Norwich, VT 05055-1623</p>
<p>Parcel ID: 20-214-020 Condo Janet Flanders 317 Hopson Road Norwich, VT 05055</p>	<p>Parcel ID: 20-214-030 Condo 1820 House-Norwich LLC Po Box 1439 Norwich, VT 05055-1439</p>
<p>Parcel ID: 20-214-040 Condo William H. Bender 1190 Turnpike Road Norwich, VT 05055</p>	<p>Parcel ID: 20-228-000 Ledyard National Bank 38 South Main Street Hanover, NH 03755</p>
<p>Parcel ID: 20-229-000 Sudlow LLC PO Box 111 Hanover, NH 03755</p>	<p>Parcel ID: 20-224-000 Happy Hill Partners LLC 5 Happy Hill Road Norwich, VT 05055</p>

STATE OFFICIALS

Vermont Public Utility Commission <i>(via ePUC)</i>	Vermont Agency of Transportation <i>(via ePUC)</i>
Vermont Agency of Natural Resources <i>(via ePUC)</i>	Vermont Division for Historic Preservation <i>(via ePUC)</i>
Vermont Department of Public Service <i>(via ePUC)</i>	

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30 V.S.A. § 248a(e) Statement of Rights and Opportunities

Pursuant to §§ 248a(c)(2), (e)(2), (m), (n), (o), and (p), for the municipality and planning region in which the proposed facility is located, municipal legislative bodies and municipal / regional planning commissions have the rights and opportunities listed below.

- Substantial deference will be given to duly adopted municipal or regional plans, and any recommendations concerning those plans.
- With respect to municipal legislative bodies and municipal planning commissions, nothing shall prevent you from basing your recommendation(s) on an ordinance adopted pursuant to 24 V.S.A., § 2291(19) or a bylaw adopted under 24 V.S.A., chapter 117.
- A letter from your entity / organization will create a rebuttable presumption with the Vermont Public Utility Commission (“PUC”) respecting compliance with the applicable plans.
- Municipal officials may request that the Petitioner attend a public meeting within the 60-day notice period, before the Petitioner submits its Petition.
- Municipal officials may also request that an official from the Vermont Department of Public Service (“Department”) attend a public hearing during the 60-day notice period. The Department, if in attendance, shall consider any comments made and information obtained at the meeting in making its recommendation to the PUC on the petition, and on whether to retain additional personnel to evaluate the project.
- Municipal legislative bodies and municipal planning commissions may, at the commencement of the 60-day notice process, request that the Department, at Petitioner’s expense, retain experts and other personnel to provide information essential to full consideration of the petition.
- Municipal legislative bodies and municipal planning commissions have the right to appear and participate on any petition seeking a certificate of public good.
- The PUC is required to consider your comments or recommendation(s) when deciding to issue or deny a certificate of public good for the project and shall include a detailed written response to each of recommendation.
- You may learn more about the § 248a process from the *Guide to the 248a process for Siting and Construction of Telecommunications Facilities*, published pursuant to 248a(p), at the Vermont Department of Public Service, 112 State Street, Montpelier, VT, or by visiting <http://publicservice.vermont.gov/telecom/>. You may request a copy by mail by calling the Department at (802) 828-2811.
- Additional documents available on the PUC’s website to assist you with this process include the following:
 - *A Citizen’s Guide to the Public Utility Commission*, available at: <https://puc.vermont.gov/document/citizen-guide-public-utility-commission>.
 - *Public Participation and Intervention in Proceedings Before the Public Utility Commission*, available at: <https://puc.vermont.gov/document/public-participation-and-intervention-proceedings-public-utility-commission>.
 - <http://puc.vermont.gov/document/section-248a-procedures>

GENERAL CONSTRUCTION

1. FOR THE PURPOSE OF CONSTRUCTION DRAWINGS, THE FOLLOWING DEFINITIONS SHALL APPLY
 - CONTRACTOR/CM - AT&T WIRELESS
 - GENERAL CONTRACTOR - TBD
 - OWNER - AT&T WIRELESS
2. ALL SITE WORK SHALL BE COMPLETED AS INDICATED ON THE DRAWINGS AND AT&T SPECIFICATIONS PROJECT
3. CONTRACTOR SHALL VISIT THE SITE AND SHALL FAMILIARIZE THEMSELVES WITH ALL CONDITIONS AFFECTING THE PROPOSED WORK AND SHALL MAKE PROVISIONS CONTRACTOR SHALL BE RESPONSIBLE FOR FAMILIARIZING THEMSELVES WITH ALL CONTRACT DOCUMENTS, FIELD CONDITIONS, DIMENSIONS, AND CONFIRMING THAT THE WORK MAY BE ACCOMPLISHED AS SHOWN PRIOR TO PROCEEDING WITH CONSTRUCTION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO THE COMMENCEMENT OF WORK.
4. ALL MATERIALS FURNISHED AND INSTALLED SHALL BE IN STRICT ACCORDANCE WITH ALL APPLICABLE CODES, REGULATIONS, AND ORDINANCES. CONTRACTOR SHALL ISSUE ALL APPROPRIATE NOTICES AND COMPLY WITH ALL LAWS, ORDINANCES, RULES, REGULATIONS, AND LAWFUL ORDERS OF ANY PUBLIC AUTHORITY REGARDING THE PERFORMANCE OF WORK.
5. ALL WORK CARRIED OUT SHALL COMPLY WITH ALL APPLICABLE MUNICIPAL AND UTILITY COMPANY SPECIFICATIONS AND LOCAL JURISDICTIONAL CODES, ORDINANCES, AND APPLICABLE REGULATIONS.
6. UNLESS NOTED OTHERWISE, THE WORK SHALL INCLUDE FURNISHING MATERIALS, EQUIPMENT, APPURTENANCES, AND LABOR NECESSARY TO COMPLETE ALL INSTALLATIONS AS INDICATED ON THE DRAWINGS.
7. PLANS ARE NOT TO BE SCALED. THESE PLANS ARE INTENDED TO BE A DIAGRAMMATIC OUTLINE ONLY UNLESS OTHERWISE NOTED. DIMENSIONS SHOWN ARE TO FINISH SURFACES UNLESS OTHERWISE NOTED. SPACINGS BETWEEN EQUIPMENT IS THE MINIMUM REQUIRED CLEARANCE. THEREFORE, IT IS CRITICAL TO FIELD VERIFY DIMENSIONS. SHOULD THERE BE ANY QUESTIONS REGARDING THE CONTRACT DOCUMENTS, THE CONTRACTOR SHALL REQUEST A CLARIFICATION FROM THE ENGINEER PRIOR TO PROCEEDING WITH THE WORK. DETAILS ARE INTENDED TO SHOW DESIGN INTENT. MODIFICATIONS MAY BE REQUIRED TO SUIT JOB DIMENSIONS OR CONDITIONS AND SUCH MODIFICATIONS SHALL BE INCLUDED AS PART OF WORK AND PREPARED BY THE ENGINEER PRIOR TO PROCEEDING WITH WORK.
8. THE CONTRACTOR SHALL INSTALL ALL EQUIPMENT AND MATERIALS IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS UNLESS SPECIFICALLY STATED OTHERWISE.
9. IF THE SPECIFIED EQUIPMENT CANNOT BE INSTALLED AS SHOWN ON THESE DRAWINGS, THE CONTRACTOR SHALL PROPOSE AN ALTERNATIVE INSTALLATION SPACE FOR APPROVAL BY THE ENGINEER PRIOR TO PROCEEDING.
10. GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE SAFETY OF WORK AREA, ADJACENT AREAS AND BUILDING OCCUPANTS THAT ARE LIKELY TO BE AFFECTED BY THE WORK UNDER THIS CONTRACT. WORK SHALL CONFORM TO ALL OSHA REQUIREMENTS AND THE LOCAL JURISDICTION.
11. CONTRACTOR SHALL COORDINATE WORK AND SCHEDULE WORK ACTIVITIES WITH OTHER DISCIPLINES.
12. ERECTION SHALL BE DONE IN A WORKMANLIKE MANNER BY COMPETENT EXPERIENCED WORKMEN IN ACCORDANCE WITH APPLICABLE CODES AND THE BEST ACCEPTED PRACTICE. ALL MEMBERS SHALL BE LAID PLUMB AND TRUE AS INDICATED ON THE DRAWINGS.
13. WORK PREVIOUSLY COMPLETED IS REPRESENTED BY LIGHT SHADED LINES AND NOTES. THE SCOPE OF WORK FOR THIS PROJECT IS REPRESENTED BY DARK SHADED LINES AND NOTES. CONTRACTOR SHALL NOTIFY THE CONTRACTOR OF ANY EXISTING CONDITIONS THAT DEVIATE FROM THE DRAWINGS PRIOR TO BEGINNING CONSTRUCTION.
14. CONTRACTOR SHALL PROVIDE WRITTEN NOTICE TO THE CONSTRUCTION MANAGER 48 HOURS PRIOR TO COMMENCEMENT OF WORK.
15. THE GENERAL CONTRACTOR SHALL PROTECT EXISTING IMPROVEMENTS, PAVEMENTS, CURBS, LANDSCAPING AND STRUCTURES. ANY DAMAGED PARTS SHALL BE REPAIRED AT CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE OWNER.
16. THE GENERAL CONTRACTOR SHALL CONTACT UTILITY LOCATING SERVICES PRIOR TO THE START OF CONSTRUCTION.
17. CONTRACTOR SHALL COORDINATE AND MAINTAIN ACCESS FOR ALL TRADES AND CONTRACTORS TO THE SITE AND/OR BUILDING.
18. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR SECURITY OF THE SITE FOR THE DURATION OF CONSTRUCTION UNTIL JOB COMPLETION.
19. THE GENERAL CONTRACTOR SHALL MAINTAIN IN GOOD CONDITION ONE COMPLETE SET OF PLANS WITH ALL REVISIONS, ADDENDA, AND CHANGE ORDERS ON THE PREMISES AT ALL TIMES.
20. THE GENERAL CONTRACTOR SHALL PROVIDE PORTABLE FIRE EXTINGUISHERS WITH A RATING OF NOT LESS THAN 2-A OR 2-A 10-B-C AND SHALL BE WITHIN 25 FEET OF TRAVEL DISTANCE TO ALL PORTIONS OF WHERE THE WORK IS BEING COMPLETED DURING CONSTRUCTION.
21. ALL EXISTING ACTIVE SEWER, WATER, GAS, ELECTRIC, AND OTHER UTILITIES SHALL BE PROTECTED AT ALL TIMES, AND WHERE REQUIRED FOR THE PROPER EXECUTION OF THE WORK, SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. EXTREME CAUTION SHOULD BE USED BY THE GENERAL CONTRACTOR WHEN EXCAVATING OR DRILLING PIERS AROUND OR NEAR UTILITIES. GENERAL CONTRACTOR SHALL PROVIDE SAFETY TRAINING FOR THE WORKING CREW. THIS SHALL INCLUDE BUT NOT BE LIMITED TO A) FALL PROTECTION, B) CONFINED SPACE, C) ELECTRICAL SAFETY, AND D) TRENCHING & EXCAVATION.
22. ALL EXISTING INACTIVE SEWER, WATER, GAS, ELECTRIC, AND OTHER UTILITIES, WHICH INTERFERE WITH THE EXECUTION OF THE WORK, SHALL BE REMOVED, CAPPED, PLUGGED OR OTHERWISE DISCONNECTED AT POINTS WHICH WILL NOT INTERFERE WITH THE EXECUTION OF THE WORK, AS DIRECTED BY THE RESPONSIBLE ENGINEER, AND SUBJECT TO THE APPROVAL OF THE OWNER AND/OR LOCAL UTILITIES.
23. THE AREAS OF THE OWNERS PROPERTY DISTURBED BY THE WORK AND NOT COVERED BY THE POLE, EQUIPMENT OR DRIVEWAY, SHALL BE GRADED TO A UNIFORM SLOPE, AND STABILIZED TO PREVENT EROSION.

25. GENERAL CONTRACTOR SHALL MINIMIZE DISTURBANCE TO THE EXISTING SITE DURING CONSTRUCTION. EROSION CONTROL MEASURES, IF REQUIRED DURING CONSTRUCTION, SHALL BE IN CONFORMANCE WITH THE FEDERAL AND LOCAL JURISDICTION FOR EROSION AND SEDIMENT CONTROL.
26. NO FILL OR EMBANKMENT MATERIAL SHALL BE PLACED ON FROZEN GROUND, FROZEN MATERIALS, SNOW OR ICE. SHALL NOT BE PLACED IN ANY FILL OR EMBANKMENT.
27. THE SUBGRADE SHALL BE BROUGHT TO A SMOOTH UNIFORM GRADE AND COMPACTED TO 95 PERCENT STANDARD PROCTOR DENSITY UNDER PAVEMENT AND STRUCTURES AND 80 PERCENT STANDARD PROCTOR DENSITY IN OPEN SPACE. ALL TRENCHES IN PUBLIC RIGHT OF WAY SHALL BE BACKFILLED WITH FLOWABLE FILL OR OTHER MATERIAL, PRE-APPROVED BY THE LOCAL JURISDICTION.
28. ALL NECESSARY RUBBISH, STUMPS, DEBRIS, STICKS, STONES, AND OTHER REFUSE SHALL BE REMOVED FROM THE SITE AND DISPOSED OF IN A LAWFUL MANNER.
29. ALL BROCHURES, OPERATING AND MAINTENANCE MANUALS, CATALOGS, SHOP DRAWINGS, AND OTHER DOCUMENTS SHALL BE TURNED OVER TO THE CONTRACTOR AT COMPLETION OF CONSTRUCTION AND PRIOR TO PAYMENT.
30. GENERAL CONTRACTOR SHALL SUBMIT A COMPLETE SET OF AS-BUILT REDLINES TO THE GENERAL CONTRACTOR UPON COMPLETION OF PROJECT AND PRIOR TO FINAL PAYMENT.
31. GENERAL CONTRACTOR SHALL LEAVE PREMISES IN A CLEAN CONDITION.
32. THE PROPOSED FACILITY WILL BE UNMANNED AND DOES NOT REQUIRE POTABLE WATER OR SEWER SERVICE, AND IS NOT FOR HUMAN HABITAT (NO HANDICAP ACCESS REQUIRED). OCCUPANCY IS LIMITED TO PERIODIC MAINTENANCE AND INSPECTION, APPROXIMATELY 2 TIMES PER MONTH, BY AT&T TECHNICIANS.
34. NO OUTDOOR STORAGE OR SOLID WASTE CONTAINERS ARE PROPOSED.
35. ALL MATERIAL SHALL BE FURNISHED AND WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE LATEST REVISION AT&T MOBILITY GROUNDING STANDARD "TECHNICAL SPECIFICATION FOR CONSTRUCTION OF GSM/GPRS WIRELESS SITES" AND "TECHNICAL SPECIFICATION FOR FACILITY GROUNDING" IN CASE OF A CONFLICT BETWEEN THE CONSTRUCTION SPECIFICATION AND THE DRAWINGS. THE DRAWINGS SHALL GOVERN.
36. GENERAL CONTRACTORS SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS AND INSPECTIONS REQUIRED FOR CONSTRUCTION. IF GENERAL CONTRACTOR CANNOT OBTAIN A PERMIT, THEY MUST NOTIFY THE CONTRACTOR IMMEDIATELY.
37. GENERAL CONTRACTOR SHALL REMOVE ALL TRASH AND DEBRIS FROM THE SITE ON A DAILY BASIS.
38. INFORMATION SHOWN ON THESE DRAWINGS WAS OBTAINED FROM SITE VISITS AND/OR DRAWINGS PROVIDED BY THE SITE OWNER. CONTRACTORS SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES PRIOR TO ORDERING MATERIAL OR PROCEEDING WITH CONSTRUCTION.
39. NO WHITE STROBE LIGHTS ARE PERMITTED. LIGHTING IF REQUIRED, WILL MEET FAA STANDARDS AND REQUIREMENTS.

ANTENNA MOUNTING

40. DESIGN AND CONSTRUCTION OF ANTENNA SUPPORTS SHALL CONFORM TO CURRENT ANSI/TIA-222 OR APPLICABLE LOCAL CODES.
41. ALL STEEL MATERIALS SHALL BE GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH ASTM A123 "ZINC (HOT-DIP GALVANIZED) COATINGS ON IRON AND STEEL PRODUCTS", UNLESS NOTED OTHERWISE.
42. ALL BOLTS, ANCHORS AND MISCELLANEOUS HARDWARE SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM A153 "ZINC COATINGS (HOT-DIP) ON IRON AND STEEL HARDWARE", UNLESS NOTED OTHERWISE.
43. DAMAGED GALVANIZED SURFACES SHALL BE REPAIRED BY COLD GALVANIZING IN ACCORDANCE WITH ASTM A780.
44. ALL ANTENNA MOUNTS SHALL BE INSTALLED WITH LOCK NUTS, DOUBLE NUTS AND SHALL BE TORQUED TO MANUFACTURER'S RECOMMENDATIONS.
45. GENERAL CONTRACTOR SHALL INSTALL ANTENNA PER MANUFACTURER'S RECOMMENDATION FOR INSTALLATION AND GROUNDING.
46. ALL UNUSED PORTS ON ANY ANTENNAS SHALL BE TERMINATED WITH A 50 OHM LOAD TO ENSURE ANTENNAS PERFORM AS DESIGNED.
47. GENERAL CONTRACTOR SHALL RECORD THE SERIAL #, SECTOR, AND POSITION OF EACH ACTUATOR INSTALLED AT THE ANTENNAS AND PROVIDE THE INFORMATION TO AT&T.

COLOR / TEXT INDEX



NOTE

GENERAL NOTES APPLY IN APPLICABLE SITUATIONS ONLY

TORQUE REQUIREMENTS

48. ALL RF CONNECTIONS, GROUNDING HARDWARE AND ANTENNA HARDWARE SHALL HAVE A TORQUE MARK INSTALLED IN A CONTINUOUS STRAIGHT LINE FROM BOTH SIDES OF THE CONNECTION.
 - A. RF CONNECTION BOTH SIDES OF THE CONNECTOR
 - B. GROUNDING AND ANTENNA HARDWARE ON THE NUT SIDE STARTING FROM THE THREADS TO THE SOLID SURFACE. EXAMPLE OF SOLID SURFACE: GROUND BAR, ANTENNA BRACKET METAL.

CABLE MOUNTING

49. TYPES AND SIZES OF THE ANTENNA CABLE ARE BASED ON ESTIMATED LENGTHS. PRIOR TO ORDERING CABLE, CONTRACTOR SHALL VERIFY ACTUAL LENGTHS BASED ON CONSTRUCTION LAYOUT AND NOTIFY THE PROJECT MANAGER IF ACTUAL LENGTHS EXCEED ESTIMATED LENGTHS.
50. ALL COAXIAL CABLE SHALL BE SECURED TO THE DESIGNED SUPPORT STRUCTURE, IN AN APPROVED MANNER, AT DISTANCES NOT TO EXCEED 4'-0" OC.
51. GENERAL CONTRACTOR SHALL FOLLOW ALL MANUFACTURER'S RECOMMENDATIONS REGARDING BOTH THE INSTALLATION AND GROUNDING OF ALL COAXIAL CABLES, CONNECTORS, ANTENNAS, AND ALL OTHER EQUIPMENT.
52. GENERAL CONTRACTOR SHALL GROUND ALL EQUIPMENT INCLUDING ANTENNAS, RET MOTORS, TMS, COAX CABLES, AND RET CONTROL CABLES AS A COMPLETE SYSTEM. GROUNDING SHALL BE EXECUTED BY QUALIFIED WIREMEN IN COMPLIANCE WITH MANUFACTURER'S SPECIFICATION AND RECOMMENDATION.

GENERAL CABLE AND EQUIPMENT NOTES

53. GENERAL CONTRACTOR SHALL BE RESPONSIBLE TO VERIFY ANTENNA, AND COAX CONFIGURATION, MAKE AND MODELS PRIOR TO INSTALLATION.
54. ALL CONNECTIONS FOR HANGERS, SUPPORTS, BRACING, ETC. SHALL BE INSTALLED PER POLE MANUFACTURER'S RECOMMENDATIONS.
55. GENERAL CONTRACTOR SHALL REFERENCE THE POLE STRUCTURAL ANALYSIS/DESIGN DRAWINGS FOR DIRECTIONS ON CABLE DISTRIBUTION/ROUTING.
56. ALL OUTDOOR RF CONNECTORS/CONNECTIONS SHALL BE WEATHERPROOFED, EXCEPT THE RET CONNECTORS. USING BUTYL TAPE AFTER INSTALLATION AND FINAL CONNECTIONS ARE MADE. BUTYL TAPE SHALL HAVE A MINIMUM OF ONE-HALF TAPE WIDTH OVERLAP ON EACH TURN AND EACH LAYER SHALL BE WRAPPED THREE TIMES. WEATHERPROOFING SHALL BE SMOOTH WITHOUT BUCKLING. BUTYL BLEEDING IS NOT ALLOWED.
57. IF REQUIRED TO PAINT ANTENNAS AND/OR COAX
 - A. TEMPERATURE SHALL BE ABOVE 50°F
 - B. PAINT COLOR MUST BE APPROVED BY BUILDING OWNER/LANDLORD
 - C. FOR REGULATED TOWERS, FAA/FAA APPROVED PAINT IS REQUIRED.
 - D. DO NOT PAINT OVER COLOR CODING OR ON EQUIPMENT MODEL NUMBERS.
58. ALL CABLES SHALL BE GROUNDING WITH COAXIAL CABLE GROUND KITS FOLLOW THE MANUFACTURER'S RECOMMENDATIONS.
 - E. GROUNDING AT THE ANTENNA LEVEL
 - F. GROUNDING AT BASE OF POLE PRIOR TO TURNING HORIZONTAL.

GROUNDING NOTES

1. THE GENERAL CONTRACTOR SHALL REVIEW AND INSPECT THE EXISTING FACILITY GROUNDING SYSTEM AND LIGHTNING PROTECTION SYSTEM (AS DESIGNED AND INSTALLED) FOR STRICT COMPLIANCE WITH THE NEC (AS ADOPTED BY THE AHJ), THE SITE SPECIFIC (UL, LPLI, OR NFPA) LIGHTNING PROTECTION CODE, AND GENERAL COMPLIANCE WITH EIRISSON AND TIA GROUNDING STANDARDS. THE SUBCONTRACTOR SHALL REPORT ANY VIOLATIONS OR ADVERSE FINDINGS TO THE CONTRACTOR FOR RESOLUTION.
2. ALL GROUND ELECTRODE SYSTEMS (INCLUDING TELECOMMUNICATION, RADIO, LIGHTNING PROTECTION, AND AC POWER (ES)) SHALL BE BONDED TOGETHER, AT OR BELOW GRADE, BY TWO OR MORE COPPER-BONDING CONDUCTORS IN ACCORDANCE WITH THE NEC.
3. THE GENERAL CONTRACTOR SHALL PERFORM IEEE FALL-OF-POTENTIAL RESISTANCE TO EARTH TESTING (PER IEEE 1100 AND 81) FOR NEW GROUND ELECTRODE SYSTEMS. THE GENERAL CONTRACTOR SHALL FURNISH AND INSTALL SUPPLEMENTAL GROUND ELECTRODES AS NEEDED TO ACHIEVE A TEST RESULT OF 5 OHMS OR LESS.
4. METAL RACEWAY SHALL NOT BE USED AS THE NEC REQUIRED EQUIPMENT GROUND CONDUCTOR. 3-STRANDED COPPER CONDUCTORS WITH GREEN INSULATION, SIZED IN ACCORDANCE WITH THE NEC, SHALL BE FURNISHED AND INSTALLED WITH THE POWER CIRCUITS TO BITS EQUIPMENT.
5. EACH BTS CABINET FRAME SHALL BE DIRECTLY CONNECTED TO THE MASTER GROUND BAR WITH GREEN INSULATED SUPPLEMENTAL EQUIPMENT GROUND WIRES, 6 AWG STRANDED COPPER OR LARGER FOR INDOOR BTS. 2 AWG STRANDED COPPER FOR OUTDOOR BTS.
6. EXOTHERMIC WELDS SHALL BE USED FOR ALL GROUNDING CONNECTIONS BELOW GRADE. APPROVED ANTI-OXIDANT COATINGS (E.G. CONDUCTIVE GEL OR PASTE) SHALL BE USED ON ALL COMPRESSION AND BOLTED GROUND CONNECTIONS.
7. ALUMINUM CONDUCTOR OR COPPER CLAD STEEL CONDUCTOR SHALL NOT BE USED FOR GROUNDING CONNECTIONS.
8. MISCELLANEOUS ELECTRICAL AND NON-ELECTRICAL METAL BOXES, FRAMES AND SUPPORTS SHALL BE BONDED TO THE GROUND ROD, IN ACCORDANCE WITH THE NEC. METAL CONDUIT SHALL BE MADE ELECTRICALLY CONTINUOUS WITH LISTED BONDING FITTINGS OR BY BONDING ACROSS THE DISCONTINUITY WITH 6 AWG COPPER WIRE UL APPROVED GROUNDING TYPE CONDUIT CLAMPS.







SUBMITTALS			
DATE	DESCRIPTION	REV	ISSUED BY
06/05/23	PERMITTING	A	LWW
06/05/23	PERMITTING	B	LWW
06/05/23	PERMITTING	C	LWW

DRAWN BY: **AAK**

CHECKED BY: **JDW**

APPROVED BY: **LWW**

NOT FOR CONSTRUCTION

USID
222507

SITE ADDRESS
15 LARY LANE
NORWICH, VT 05055

NODE FA LOCATION
14563618

ATOLL NAME
CRAN_RCTE_00077_10

SHEET TITLE
GENERAL NOTES

SHEET NUMBER
GN-1

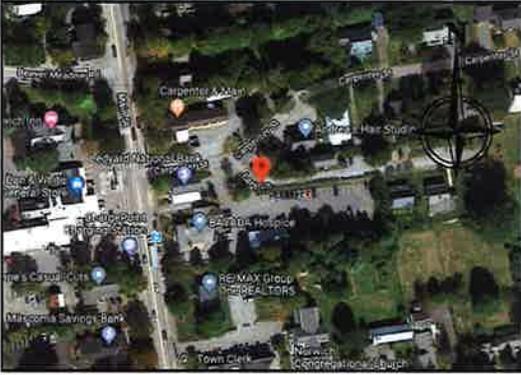
NOTE

SITE PLAN INFORMATION CONTAINED HEREIN IS TAKEN FROM THE DOCUMENTS PROVIDED BY THE CLIENT. NO WARRANTY OR GUARANTEE IS GIVEN BY CARLSON MCCAIN TO THE ACCURACY OR THE COMPLETENESS OF THE COPIED SURVEY/SITE PLAN INFORMATION.

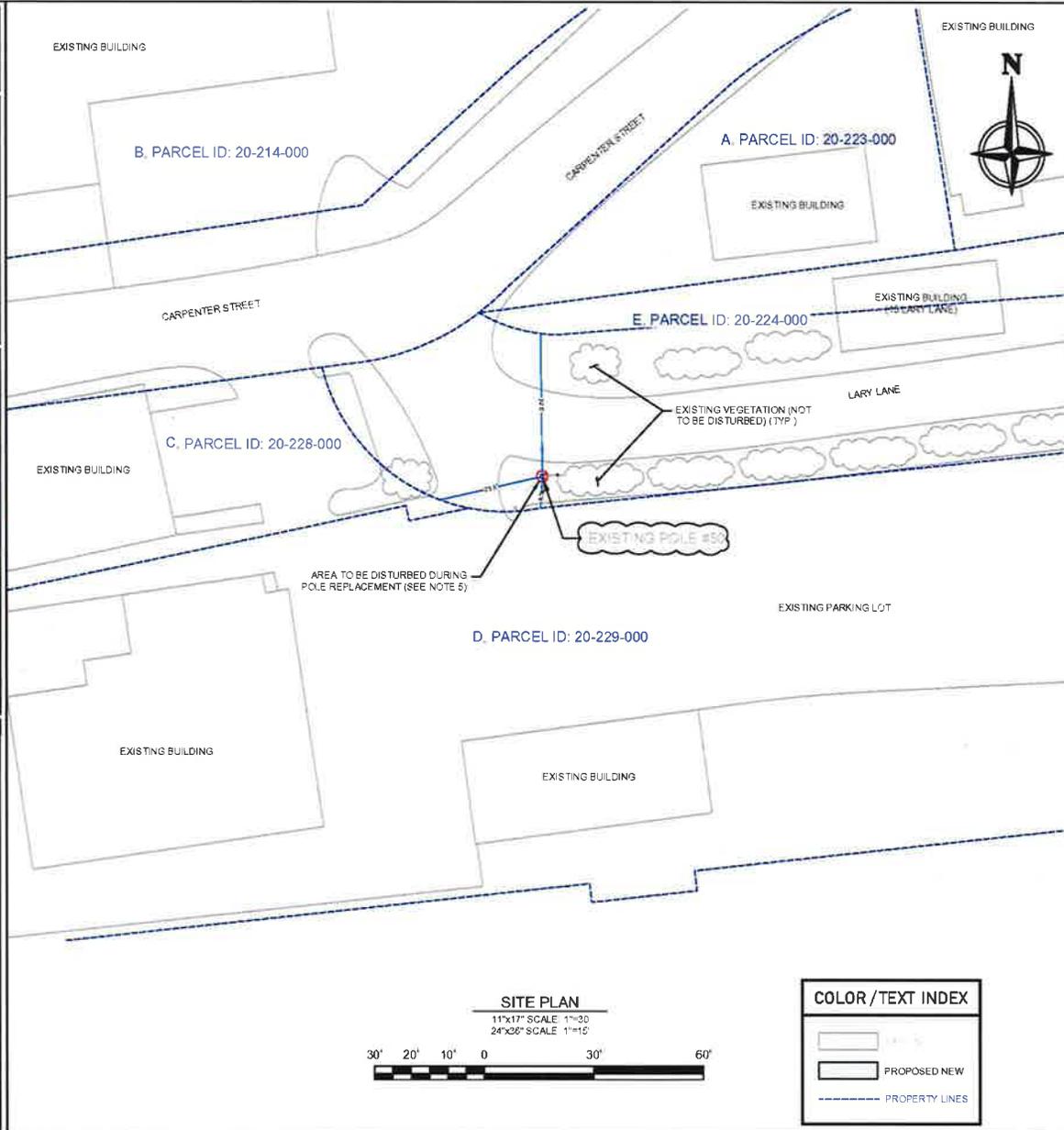
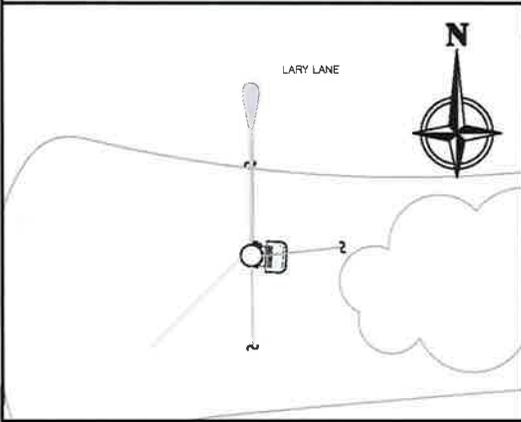
SITE NOTES

- 1 GC RESPONSIBLE FOR SUBMITTING A 48 HOUR NOTICE PRIOR TO MOBILIZATION TO THE SITE
- 2 A PRE-CONSTRUCTION MEETING IS REQUIRED PRIOR TO CONSTRUCTION START TO REVIEW SCOPE OF WORK AND EXPECTATIONS
 - A RIGGING PLAN MAY BE REQUIRED AND SHALL BE SUBMITTED WITH THE 48 HOUR NOTICE
 - GC RESPONSIBLE FOR ENSURING THE SAFETY CLIMB IS NOT TRAPPED OR OTHERWISE OBSTRUCTED WITH REASONABLE BUILDING PRACTICES
- 3 GC RESPONSIBLE FOR COORDINATING DAILY LOG-IN AND LOG-OUT WITH THE POLE OWNER, WHERE APPLICABLE
- 4 GC WILL BE RESPONSIBLE FOR THE POLE OWNER CLOSEOUT PACKAGE WITH REDLINES OF THE CD'S OF THE EQUIPMENT INSTALLED TO BE SUBMITTED WITHIN 7 DAYS OF CONSTRUCTION COMPLETE
- 5 GC WILL TEMPORARILY DISTURB 7 SQUARE FEET OF EARTH TO REPLACE THE POLE
- 6 THE PROJECT WILL COMPLY WITH THE LOW RISK HANDBOOK FOR EROSION PREVENTION AND SEDIMENT CONTROL BY THE VERMONT DEPARTMENT OF ENVIRONMENTAL CONSERVATION
- 7 THERE ARE NO WETLANDS WITHIN THE AREA OF DISTURBANCE

SITE PHOTO



OVERHEAD VIEW







SUBMITTALS				
DATE	DESCRIPTION	REV	ISSUED BY	
04/01/2019	PERMITTING	A	LWW	
05/20/2019	PERMITTING	B	LWW	
05/24/2019	PERMITTING	C	LWW	

DRAWN BY: AAK
 CHECKED BY: JDW
 APPROVED BY: LWW

NOT FOR CONSTRUCTION

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USID: 222507

SITE ADDRESS: 15 LARY LANE, NORWICH, VT 05055

NODE FA LOCATION: 14563618

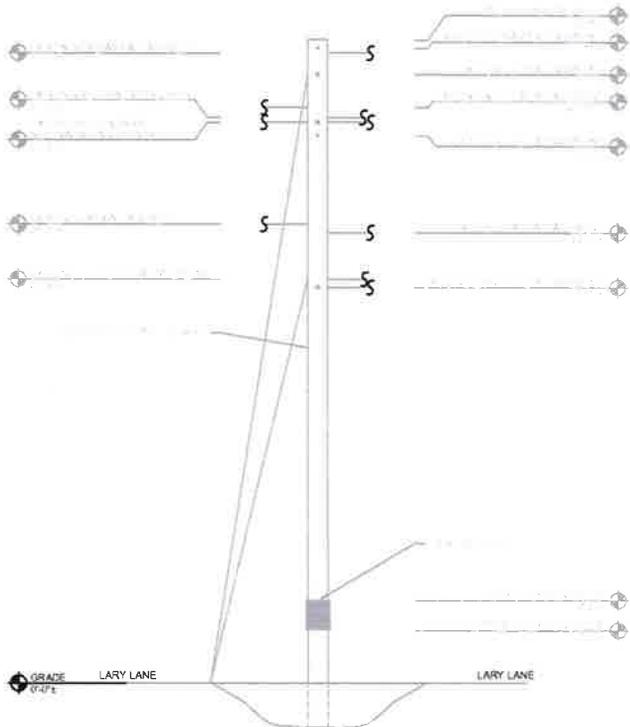
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SHEET TITLE: SITE PLAN

SHEET NUMBER: C-1

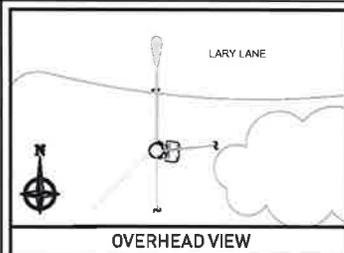
NOTES

1. POLE STRUCTURAL CALCULATIONS ARE PREPARED BY CARLSON MCCAIN. CONTRACTOR TO VERIFY WITH PROJECT MANAGER TO OBTAIN COPY.
2. CONTRACTOR SHALL REFER TO POLE STRUCTURAL CALCULATIONS FOR ADDITIONAL LOADS. NO ERECTION OR MODIFICATION OF POLE SHALL BE MADE WITHOUT APPROVAL OF STRUCTURAL ENGINEER.
3. THERE ARE NO TREES IN THE IMMEDIATE VICINITY OF POLE.

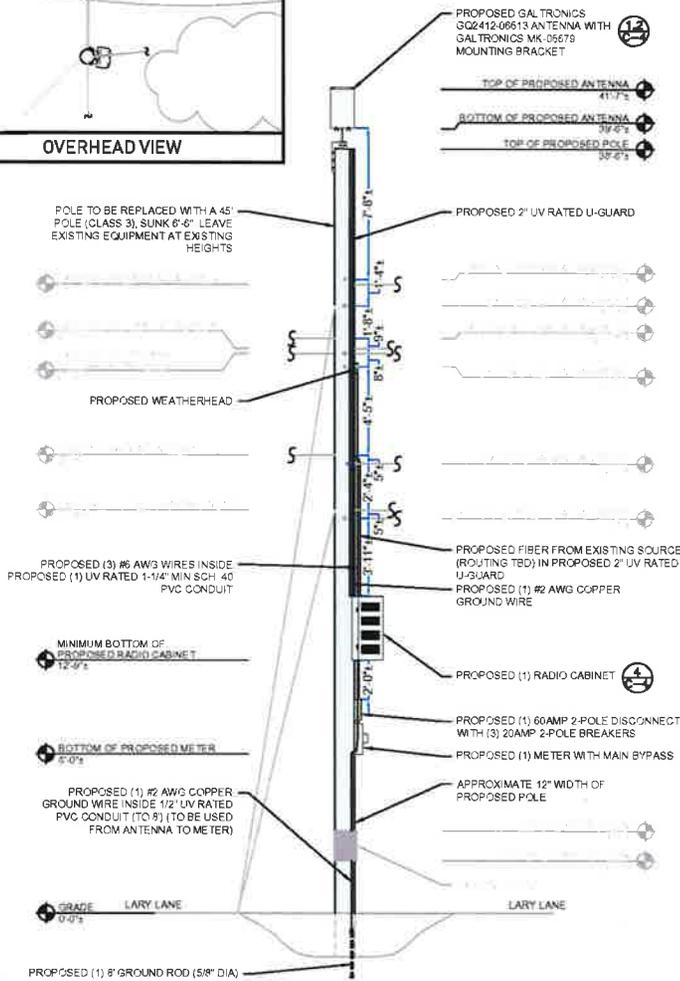


EXISTING POLE ELEVATION - SOUTH

11'x17" SCALE 1"=6'
24'x36" SCALE 1"=3'



OVERHEAD VIEW



PROPOSED POLE ELEVATION - SOUTH

11'x17" SCALE 1"=6'
24'x36" SCALE 1"=3'



COLOR/TEXT INDEX

	EXISTING
	PROPOSED NEW



SUBMITTALS			
DATE	DESCRIPTION	REV	ISSUED BY
04/20/20	PERMITTING	A	LWW
05/05/20	PERMITTING	B	LWW
05/24/20	PERMITTING	C	LWW

DRAWN BY: AAK
CHECKED BY: JDW
APPROVED BY: LWW

NOT FOR CONSTRUCTION

USID
222507

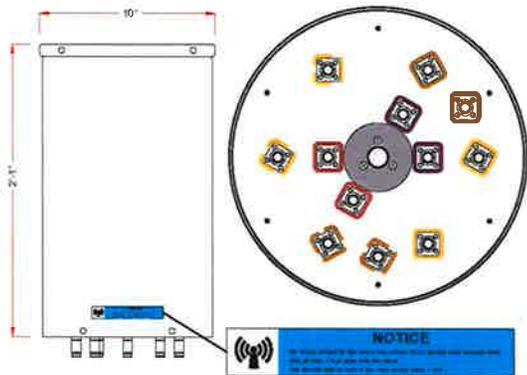
SITE ADDRESS
15 LARY LANE
NORWICH, VT 05055

NODE FA LOCATION
14563618

ATOLL NAME
CRAN_RCTB_00077_10

SHEET TITLE
POLE ELEVATION
PLAN - SOUTH

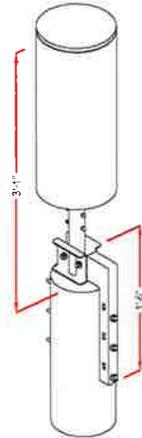
SHEET NUMBER
C-3



WEIGHT 30.9 LBS
VOLUME 2.41 CF

1"x6" NOTICE DECAL
PLACE THREE NOTICE STICKERS EQUALLY SPACED
AROUND THE BOTTOM OF THE ANTENNA RADOME

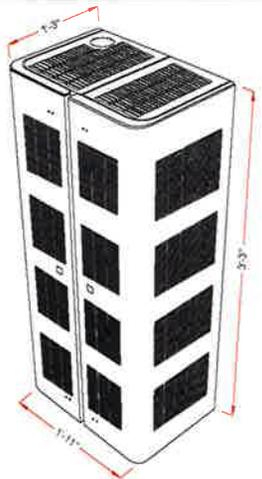
GALTRONICS CANISTER ANTENNA
GQ2412-06613
N.T.S.



WEIGHT 5.14 LBS

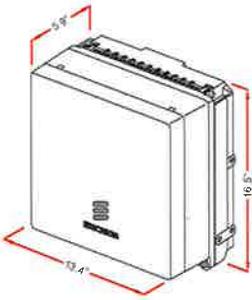
ANTENNA MOUNT DETAIL
N.T.S.

- NOTES**
1. INSTALL ANCHORS/FAS-TENERS A MAXIMUM OF 2'-0" ON CENTERS
 - WOOD STUDS - 1/4"Ø LAG BOLT W/ 1" EMBEDMENT IN W/250
 - CONCRETE - 1/4"Ø H/LT/1 KWIK BOLT III W/ 1-1/2" EMBEDMENT OR EQUIVALENT
 - THROUGH BOLT - 1/4"Ø A35/A307 THREADED ROD W/ NUTS AND WASHERS ANCHORS AND UNISTRUT CHANNEL SHALL HAVE HOT-DIPPED GALVANIZED FINISH
 2. MOUNT RRHS AND DC6 TO UNISTRUT WITH 3/8"Ø UNISTRUT BOLTING HARDWARE AND SPRING NUTS. TYPICAL FOUR PER DEVICE. SUBCONTRACTOR SHALL SUPPLY
 3. SUBCONTRACTOR SHALL SUPPLY ALL OTHER MATERIALS AND INSTALL ALL MOUNTING HARDWARE



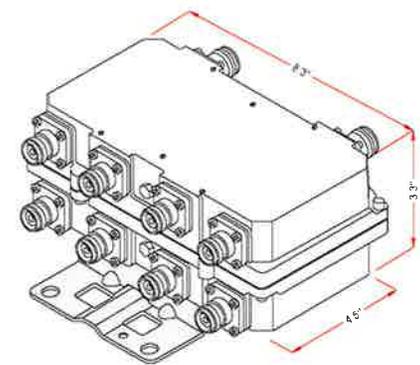
WEIGHT <135.0 LBS (ENCLOSURE ONLY)
EXTERNAL VOLUME 7.8 CF

ERICSSON 39" EQUIPMENT SHROUD
N.T.S.



WEIGHT 46 LBS (EXCLUDING HARDWARE)

ERICSSON
RRUS 4415/26 RADIO
N.T.S.



WEIGHT 7.3 LBS
(WITHOUT BRACKET)

COMMSCOPE QUAD-PACK DIPLEXER
CBC1923Q-43 / E14F55P12
N.T.S.



SUBMITTALS			
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05/20/2013	PERMITTING	C	LWW

DRAWN BY: AAK
CHECKED BY: JDW
APPROVED BY: LWW

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222507

SITE ADDRESS
15 LARY LANE
NORWICH, VT 05055

NODE FA LOCATION
14563618

ATOLL NAME
CRAN_RCTB_00077_10

SHEET TITLE
EQUIPMENT
DETAILS

SHEET NUMBER
C-4

ANTENNA AND CABLING NOTES

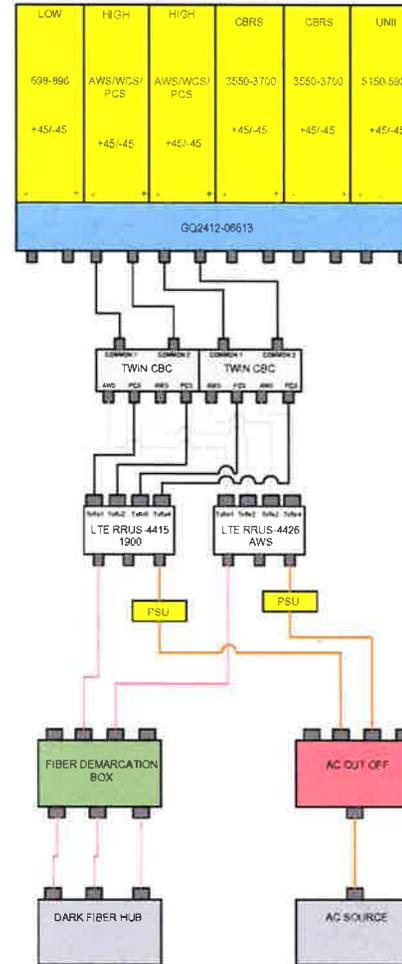
- CONTRACTOR IS TO REFER TO AT&T'S MOST CURRENT RADIO FREQUENCY DATA SHEET (RFDS) PRIOR TO CONSTRUCTION
- THE SIZE, HEIGHT, AND DIRECTION OF THE ANTENNAS SHALL BE ADJUSTED TO ACHIEVE THE AZIMUTHS SPECIFIED AND LIMIT SHADOWING AND TO MEET THE SYSTEM REQUIREMENTS
- CONTRACTOR SHALL VERIFY THE HEIGHT OF THE ANTENNA WITH THE AT&T WIRELESS PROJECT MANAGER
- VERIFY TYPE AND SIZE OF NODE POLE PRIOR TO ORDERING ANY ANTENNA MOUNT
- UNLESS NOTED OTHERWISE THE CONTRACTOR MUST PROVIDE ALL MATERIAL NECESSARY
- ANTENNA AZIMUTHS ARE DEGREES OFF OF TRUE NORTH, BEARING CLOCKWISE, IN WHICH ANTENNA FACE IS DIRECTED. ALL ANTENNAS (AND SUPPORTING STRUCTURES AS PRACTICAL) SHALL BE ACCURATELY ORIENTED IN THE SPECIFIED DIRECTION.
- CONTRACTOR SHALL VERIFY ALL RF INFORMATION PRIOR TO CONSTRUCTION
- SWEEP TEST SHALL BE PERFORMED BY GENERAL CONTRACTOR AND SUBMITTED TO AT&T WIRELESS CONSTRUCTION SPECIALIST. TEST SHALL BE PERFORMED PER AT&T WIRELESS STANDARDS
- CABLE LENGTHS WERE DETERMINED BASED ON THE DESIGN DRAWING. CONTRACTOR TO VERIFY ACTUAL LENGTH DURING PRE-CONSTRUCTION WALK

RF, DC, & COAX CABLE MARKING LOCATIONS TABLE

NO	LOCATIONS
1	EACH TOP JUMPER SHALL BE COLOR CODED WITH (1) SET OF 3" WIDE BANDS
2	EACH MAIN COAX SHALL BE COLOR CODED WITH (1) SET OF 3" WIDE BANDS NEAR THE TOP JUMPER CONNECTION AND WITH (1) SET OF 3/4" WIDE COLOR BANDS JUST PRIOR TO ENTERING THE BTS OR TRANSMITTER BUILDING
3	CABLE ENTRY POINT ON THE INTERIOR OF THE SHELTER
4	ALL BOTTOM JUMPERS SHALL BE COLOR CODED WITH (1) SET OF 3/4" WIDE BANDS ON EACH END OF THE BOTTOM JUMPER
5	ALL BOTTOM JUMPERS SHALL BE COLOR CODED WITH (1) SET OF 3/4" WIDE BANDS ON EACH END OF THE BOTTOM JUMPER

CABLE MARKING NOTES

- THE ANTENNA SYSTEM COAX SHALL BE LABELED WITH VINYL TAPE
- THE STANDARD IS BASED ON EIGHT COLORED TAPES (RED, BLUE, GREEN, YELLOW, ORANGE, BROWN, WHITE, AND VIOLET). THESE TAPES MUST BE 3/4" WIDE & UV RESISTANT SUCH AS SCOTCH 35 VINYL ELECTRICAL COLOR CODING TAPE AND SHOULD BE READILY AVAILABLE TO THE ELECTRICIAN OR CONTRACTOR ON SITE
- USING COLOR BANDS ON THE CABLES, MARK ALL RF CABLE BY SECTOR AND CABLE NUMBER AS SHOWN ON "CABLE COLOR CHART"
- WHEN AN EXISTING COAXIAL LINE THAT IS INTENDED TO BE A SHARED LINE BETWEEN TECHNOLOGIES IS ENCOUNTERED, THE CONTRACTOR SHALL REMOVE THE EXISTING COLOR CODING SCHEME AND REPLACE IT WITH THE COLOR CODING STANDARD. IN THE ABSENCE OF AN EXISTING COLOR CODING AND TAGGING SCHEME, OR WHEN INSTALLING PROPOSED COAXIAL CABLES, THIS GUIDELINE SHALL BE IMPLEMENTED AT THAT SITE REGARDLESS OF TECHNOLOGY
- ALL COLOR CODE TAPE SHALL BE 3M-35 AND SHALL BE INSTALLED USING A MINIMUM OF (3) THREE WRAPS OF TAPE AND SHALL BE NEATLY TRIMMED AND SMOOTHED OUT SO AS TO AVOID UNRAVELING
- ALL COLOR BANDS INSTALLED AT THE TOP OF THE TOWER SHALL BE A MINIMUM OF 3" WIDE, AND SHALL HAVE A MINIMUM OF 3/4" OF SPACE BETWEEN EACH COLOR
- ALL COLOR CODES SHALL BE INSTALLED SO AS TO ALIGN NEATLY WITH ONE ANOTHER FROM SIDE-TO-SIDE
- IF EXISTING CABLES AT THE SITE ALREADY HAVE A COLOR CODING SCHEME AND THEY ARE NOT INTENDED TO BE REUSED OR SHARED WITH THE PROPOSED TECHNOLOGY, THE EXISTING COLOR CODING SCHEME SHALL REMAIN UNTOUCHED



PICO CABLE WIRING DIAGRAM

N T S



SUBMITTALS			
DATE	DESCRIPTION	REV	ISSUED BY
06/05/18	PERMITTING	A	LWW
06/05/18	PERMITTING	B	LWW
06/05/18	PERMITTING	C	LWW

DRAWN BY: AAK
 CHECKED BY: JDW
 APPROVED BY: LWW

NOT FOR CONSTRUCTION

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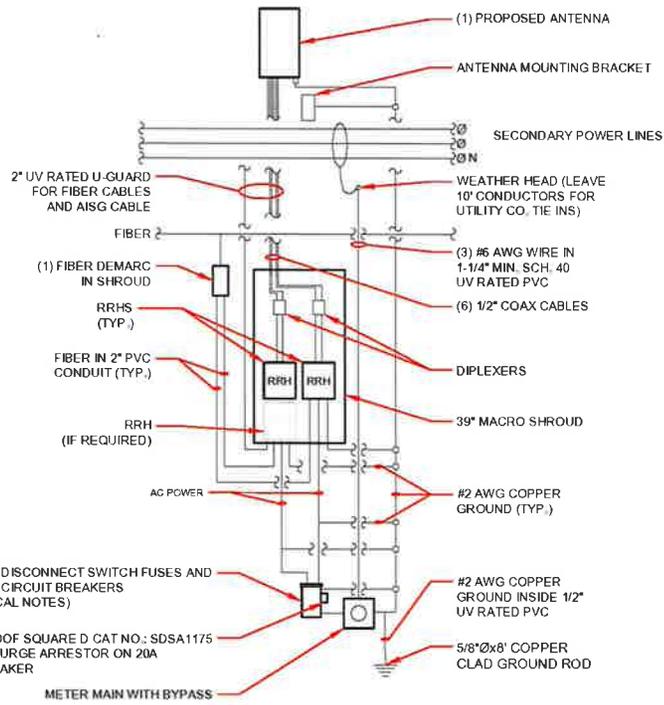
SITE ADDRESS
15 LARY LANE
NORWICH, VT 05055

NODE FA LOCATION
14563618

ATOLL NAME
CRAN_RCTB_00077_10

SHEET TITLE
CABLE NOTES AND
COLOR CODING

SHEET NUMBER
C-5



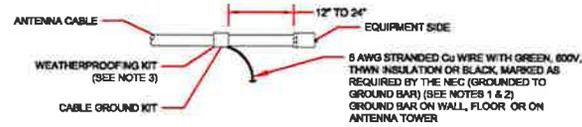
**PICO
GENERAL WIRING DIAGRAM**
N.T.S.

NOTES

BONDING AND GROUNDING TO MEET APPLICABLE NESC REQUIREMENTS

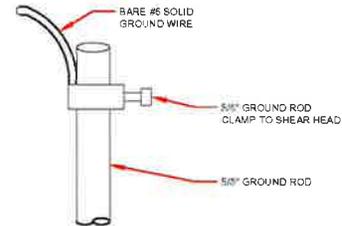
NOTES

1. DO NOT INSTALL CABLE GROUND KIT AT A BEND AND ALWAYS DIRECT GROUND WIRE DOWN TO GROUND BAR
2. GROUNDING KIT SHALL BE TYPE AND PART NUMBER AS SUPPLIED OR RECOMMENDED BY CABLE MANUFACTURER
3. WEATHERPROOFING SHALL BE (TYPE AND PART NUMBER) AS SUPPLIED OR RECOMMENDED BY CABLE MANUFACTURER AND APPROVED BY CONTRACTOR



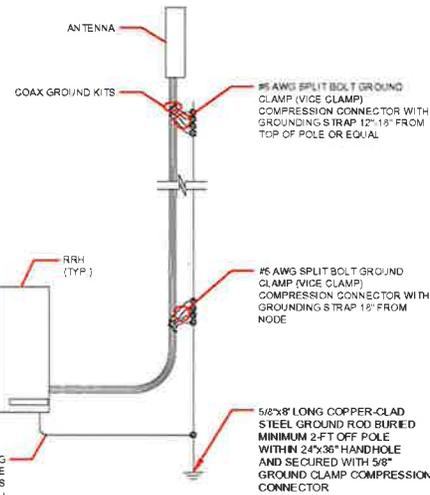
CONNECTION OF CABLE GROUND KIT TO ANTENNA CABLE

11"x17" SCALE N.T.S.
24"x30" SCALE N.T.S.



CONNECTION TO GROUND ROD

N.T.S.



GROUNDING ONE LINE DIAGRAM

N.T.S.



SUBMITTALS			
DATE	DESCRIPTION	REV	TESTED BY
04/01/03	PERMITTING	A	LWW
05/02/03	PERMITTING	B	LWW
07/22/03	PERMITTING	C	LWW

DRAWN BY: AAK
CHECKED BY: JCW
APPROVED BY: LWW

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SITE ADDRESS
15 LARY LANE
NORWICH, VT 05055

NODE FA LOCATION
14563618

ATOLL NAME
CRAN_RCTB_00077_10

SHEET TITLE
ELECTRICAL AND GROUNDING
DETAILS

SHEET NUMBER

G-1

